

AGENDA

WORCESTER COUNTY COMMISSIONERS

Worcester County Government Center, Room 1101, One West Market Street, Snow Hill, Maryland 21863

October 2, 2018

Item #

9:00 AM - Meet in Commissioners' Conference Room - Room 1103 Government Center, One West Market Street, Snow Hill, Maryland - Vote to Meet In Closed Session

9:01 - Closed Session: Discussion regarding hiring a Maintenance Worker II for Recreation and Parks, a Roads Worker II for the Roads Division of Public Works, and an Emergency Management Planner for Emergency Services; and posting to fill vacancies for a Communications Clerk Trainee and a Communications Clerk I for Emergency Services, and a Building Maintenance Mechanic III for the Maintenance Division of Public Works; receiving legal advice from Counsel; and performing administrative functions

PLEASE NOTE - OPEN SESSION TO BEGIN AT 9:30 AM

9:30 - Call to Order, Prayer (Arlene Page), Pledge of Allegiance

9:31 - Report on Closed Session; Review and Approval of Minutes

9:40 - Chief Administrative Officer: Administrative Matters

1-10

(Request for Memorandum of Understanding with Maryland State Police and County Jail for Collection of DNA Samples Under Maryland Law; Purchase of Tractor and Finish Mower for the Jail; Public Performance Licensing Agreement with Swank Motion Pictures for DVD's Broadcast at the Jail; P25 Radio System Status Update; Fiscal Year 2018 (FY18) Year End Budget Transfers; FY18 Year End Reserves for Assigned Encumbrances; Nuisance Abatement of Property at 10646 Bishopville Road; Request for Proposals for Consultant Services - Worcester County Water and Sewerage Plan Update; Award of Bid for Corrugated Metal Pipe for the Roads Division of Public Works; Holly Grove Road Speed Study Results and Recommendation; and potentially other administrative matters)

9:50 -

10:00 - Maryland Department of Transportation/State Highway Administration - Annual Capital Program Tour Meeting for the Draft FY2019-2024 Consolidated Transportation Program (CTP)

11

10:10 -

10:20 -

10:30 -

10:40 -

10:50 -

11:00 -

11:10 -

11:20 -

11:30 - Presentation of Proclamation - Fire Prevention Week/Month in Worcester County

12

11:35 - Public Hearing on Petitions of Worcester County Property Owners to Sell Agricultural Easements to the Maryland Agricultural Land Preservation Foundation (MALPF)

13

11:40 - Chief Administrative Officer: Administrative Matters

1-10, continued

11:50 -

12:00 - Questions from the Press

AGENDAS ARE SUBJECT TO CHANGE UNTIL THE TIME OF CONVENING

Hearing Assistance Units Available - see Kelly Shannahan, Asst. CAO.

Please be thoughtful and considerate of others.

Turn off your cell phones & pagers during the meeting!

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Minutes of the County Commissioners of Worcester County, Maryland

September 18, 2018

Diana Purnell, President
Theodore J. Elder, Vice President
Anthony W. Bertino, Jr.
Madison J. Bunting, Jr.
James C. Church
Merrill W. Lockfaw, Jr.
Joseph M. Mitrecic

Following a motion by Commissioner Lockfaw, seconded by Commissioner Bertino, the Commissioners unanimously voted to meet in closed session at 9:00 a.m. in the Commissioners' Conference Room to discuss legal and personnel matters permitted under the provisions of Section 3-305(b)(1), (7), and (8) of the General Provisions Article of the Annotated Code of Maryland and to perform administrative functions. Also present at the closed session were Harold L. Higgins, Chief Administrative Officer; Kelly Shannahan, Assistant Chief Administrative Officer; Maureen Howarth, County Attorney; and Kim Moses. Topics discussed and actions taken included: transferring Chris Webster from Roads Worker V to Vehicle & Equipment Mechanic II and posting to fill a Roads Worker I position within the Roads Division of Public Works, posting to fill a vacancy for a Correctional Officer Trainee at the Jail; acknowledging the rehiring of former Deputy Corporal Rod Matei as a part-time Security Monitor within the Sheriff's Office; discussing potential and pending litigation and receiving legal advice from counsel; and performing administrative functions.

After the closed session, the Commissioners reconvened in open session. Commissioner Purnell called the meeting to order and announced the topics discussed during the morning closed session.

The Commissioners reviewed and approved the September 4, 2018 closed session minutes as revised and open session minutes as revised.

The Commissioners presented a proclamation recognizing September as Suicide Prevention Month to Local Management Board Director Jessica Sexauer, Health Department staff members, Jesse Klump Memorial Fund Founder Kim Klump and other health partners. Ms. Sexauer invited the Commissioners and those in attendance at the meeting to participate in the annual Out of the Darkness Walk to Prevent Suicide on Saturday, September 22, at Caroline Street and the Boardwalk in Ocean City. Learn the warning signs and risk factors for suicide at <http://worcesterhealth.org>.

The Commissioners, who were joined by current and former Emergency Services Directors Fred Webster and Teresa Owens, respectively, presented a commendation to Bill Sammler, who retired with 24 years of service as a Warning Coordination Meteorologist at the

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National Weather Service (NWS) in Wakefield, Virginia, and they thanked him for the role he played in public safety locally.

The Commissioners presented commendations to members of the Berlin Little League Junior Girls' Softball Team for winning the District 8 Championship and going on to win the 2018 Maryland State Championship for their age division.

The Commissioners presented commendations to members of the Berlin Little League Ages 11-12 All Stars for winning the Maryland District 8 Tournament Play and going on to win the 2018 Maryland State Championship for their age division.

Prior to viewing the Home Box Office (HBO) video clip entitled "Real Sports with Bryant Gumbel - Youth Sports Inc." on youth travel sports, a motion by Commissioner Bertino to postpone viewing the video until after the Commissioners meet in closed session later that morning to discuss the potential acquisition of real property for a public purpose failed 2-5, with Commissioners Bertino and Bunting voting in favor of the motion and Commissioners Church, Elder, Lockfaw, Mitrecic, and Purnell voting in opposition.

The Commissioners viewed the HBO video clip "Real Sports with Bryant Gumbel - Youth Sports Inc." on youth travel sports, also referred to as sports tourism, a reportedly \$9 billion industry at the time it first aired in August 2017, which has grown to a \$15 billion industry today.

Pursuant to the request of Library Director Jennifer Ranck and upon a motion by Commissioner Mitrecic, the Commissioners unanimously authorized the Worcester County Historical Society (WCHS) to erect a memorial for William Julius "Judy" Johnson at the County Library campus in Snow Hill. Mr. Johnson, who was born in Snow Hill in 1899, was elected into the National Baseball Hall of Fame in 1975. Ms. Ranck advised that the WCHS will fund all project costs.

Emergency Services Director Fred Webster and Assistant Director James Hamilton provided the Commissioners with a P25 Radio System status update. Mr. Webster stated that there have been no reports of radio transmission issues since his last update to the Commissioners on September 4, 2018, and all system testing for voice clarity and bit error rate (BER) has been completed, with the findings to be released by Harris and reviewed by the County's consultant, Federal Engineering (FE), in the near future. He concluded that County staff will continue to work with Harris and FE representatives to resolve all prior issues associated with the new system.

Pursuant to the request of Mr. Webster and upon a motion by Commissioner Bertino, the Commissioners unanimously approved the allocation of \$5,042 of the County's annual Homeland Security grant funds to the Maryland Emergency Management Agency (MEMA) to help form, staff, and fund a Maryland Incident Management Team (IMT). Mr. Webster stated that the IMT would be comprised of staff from each of the 23 counties and Baltimore City for the purpose of staffing an Emergency Operations Center and Incident Command Centers during

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Maryland disasters. He advised that during past disasters, including the Ellicott City flooding and Baltimore City riots, the State had to rely on IMT resources from Pennsylvania and Virginia to assist with response and recovery. In response to a question by Commissioner Bertino, Mr. Webster advised that any members of the team from Worcester County would likely include staff from multi-disciplines, such as emergency management, public works, and finance, all of whom would need to undertake additional Incident Command System (IS) training before being eligible to serve.

Sheriff-Elect Matt Crisafulli asked the Commissioners to reconsider their prior decision to deny his requested color choices of black and red for two unmarked Chevy Tahoes for command staff within the Sheriff's Office. At their September 4, 2018 meeting, the Commissioners approved bid specifications in blue only for these two vehicles. Sheriff-Elect Crisafulli stated that command staff's vehicle colors have always differed from those of marked fleet vehicles, making them ideal as they age and have high mileage for use as surveillance vehicles by plain-clothes officers.

Commissioner Church stated that he recently consulted with a retired Drug Enforcement Agency (DEA) officer who advised that the requested colors would stand out in the neighborhoods where surveillance operations often take place. Sheriff-Elect Crisafulli conceded that could be so, but noted that the vehicles are ideal for command staff, and he thanked the Commissioners for the opportunity to meet with them to make his case. The Commissioners took no action on the request.

Pursuant to the request of Environmental Programs Director Bob Mitchell and upon a motion by Commissioner Bertino, the Commissioners voted 6-1, with Commissioner Elder voting in opposition, to approve the application to the Maryland Agricultural Land Preservation Foundation (MALPF) Program for re-certification for Worcester County. Mr. Mitchell advised that Worcester County's certification entitles the County to retain 75% instead of 33% of the Agricultural Land Transfer Tax (ALTT) and also makes the County eligible for 60% additional matching funds from the State when those funds may be available for the purchase of agricultural easements on properties in Worcester County.

Pursuant to the recommendation of Mr. Mitchell and upon a motion by Commissioner Bunting, the Commissioners unanimously agreed to schedule a public hearing on October 2, 2018 to consider nine applications to sell easements to the Maryland Agricultural Land Preservation Foundation (MALPF) in FY19 on the following properties: a 224.1-acre property owned by GCR, LLC located at 934 Bishop Road in Pocomoke and more specifically identified on Tax Map 91 as Parcel 14; a 91.3-acre property owned by Stephen Shockley and located on Davis Branch Road in Snow Hill and more specifically identified on Tax Map 38 as Parcel 26; a 390.2-acre property owned by Patrick and Ralph Shockley, with David and Sandra Shockley as Trustees, located on Shockley Road in Snow Hill and more specifically identified on Tax Map 38 as Parcel 15; a 102-acre property owned by Freddie and Fay Fair and located on the west side of Steel Pond Road in Stockton and more specifically identified on Tax Map 93 as Parcel 41; a 210.13-acre property owned by Mark Gross on Harrison Road in Berlin and more specifically identified on Tax Map 32 as Parcel 217; a 281.2-acre property owned by Harry and Annette

Powell and located on Timmons Road in Snow Hill and more specifically identified on Tax Map 56 as Parcel 25; a 153.9-acre property owned by Samuel (Glenn) and Brenda Shockley at 5910 Laws Road in Snow Hill and more specifically identified on Tax Map 38 as Parcel 4; a 153.9-acre property owned by Glenn and Jean Holland at 1975 Colona Road in Pocomoke and more specifically identified on Tax Map 99 as Parcel 35; and a 176.04-acre property owned by Everett Holland and Mark Holland at 1546 Colona Road in Pocomoke and more specifically identified on Tax Map 99 as Parcel 39.

Mr. Mitchell stated that the nine easement applications were reviewed by both the Worcester County Agricultural Land Preservation Advisory Board and the Worcester County Planning Commission, and all nine applications were recommended for submission to MALPF for consideration. In response to a question by Commissioner Bertino, Mr. Mitchell advised that notifications about the proposed purchases and hearing date will be sent to all neighboring property owners and will be advertised once in the local newspapers prior to the hearing.

The Commissioners met with Assistant Chief Administrative Officer Kelly Shannahan on behalf of the Worcester County Sewer Committee to discuss a request from Hugh Cropper, IV, attorney for Sea Oaks Village, LLC, to allocate 63 equivalent dwelling units (EDUs) of sewer service (59 EDUs for townhomes and four EDUs for commercial development) from the Mystic Harbour Sanitary Service Area (SSA) to the unimproved, 40-acre proposed Sea Oaks Village Residential Planned Community (RPC), which is designated S-3 (sewer service within 6-10 years in the Water and Sewerage Plan), is located on the west side of Stephen Decatur Highway (MD Rt. 611) south of West Ocean City and more specifically identified on Tax Map 26 as Parcel 274, Lot 3A. The front of the property is zoned C-1 Neighborhood Commercial (4.2 acres), and the balance is zoned R-3 Multifamily Residential (35.8 acres). Mr. Shannahan advised that, prior to the replacement and expansion of the Mystic Harbour Wastewater Treatment Plant (WWTP), the subject property was allocated a total of 40 EDUs of sewer service. However, at the request of Mr. Cropper, who also served as the attorney for the former property owner, on March 15, 2016 the Commissioners approved the transfer of those 40 EDUs to other properties in the Mystic Harbour/West OC overlay area, leaving this property with 0 EDUs. Then on September 29, 2017 the County received a request for the allocation of 139 sewer EDUs for the Sea Oaks Village RPC, to serve 135 townhomes and 24,000 square feet of commercial development. The Commissioners denied that application on October 17, 2017. Mr. Shannahan advised that there are 224 available EDUs allocated in Area 1 (north of the airport) for the following uses: Infill and Intensification (87 EDUs); Vacant or Multi-Lot (80 EDUs); Single Family Dwellings (17 EDUs); and Commercial (40 EDUs). He concluded that the "Vacant or Multi-Lot Properties" category (59 EDUs for townhomes) and the "Commercial" category (4 EDUs) would be the appropriate categories from which to assign the EDUs if the Commissioners are inclined to approve Mr. Cropper's request.

Mr. Shannahan advised that the approved expansion of the Mystic Harbour Wastewater Treatment Plant (WWTP) in 2008 and project funding from the United States Department of Agriculture (USDA) in 2009 were predicated on the need for infill and intensification of properties along the U.S. Rt. 50 commercial corridor and vicinity, service to vacant or multi-lot properties, single family dwellings converting from septic systems to public sewer, and commercial properties. Furthermore, the Planning Commission recommended a rating system to

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rank priority allocations of the additional EDUs with highest priority to (1) infill lots, then (2) expansion of existing facilities, then (3) replacement of septic tanks, and last (4) new developments. He stated that the Sewer Committee recommends that any approval of this request be contingent upon the following six conditions: recommendation of approval by the Planning Commission for an amendment to the Water and Sewer Plan to amend the property from S-3 to S-1 (designated for sewer service within 2 years) and to amend the EDU map to incorporate the subject property; approval by the Commissioners of the Plan Amendment referenced in Condition 1; approval by the Maryland Department of the Environment (MDE) of the Plan Amendment referenced in Condition 1; taking the revised RPC Step 1 application back to the Technical Review Committee (TRC) for review and comment; recommendation of approval by the Planning Commission for the revised RPC Step 1 application; and approval by the Commissioners of the revised RPC Step 1 application, with said conditions, exclusive of MDE approval, to be met on or before November 20, 2018.

Upon a motion by Commissioner Mitrecic, the Commissioners voted 5-2, with Commissioners Bertino and Elder voting in opposition, to approve Option 2, approving the request for 63 EDU's of sewer service from Area 1 (North) of the Mystic Harbour SSA to serve the Sea Oaks, LLC property, with EDU's allocated from the "Vacant or Multi-Lot Properties" category (30 EDUs), the "Infill and Intensification" category (29 EDUs), and the "Commercial" category (4 EDUs), and subject to the Conditions of Approval, as recommended by the Sewer Committee.

Commissioner Bertino stated that he is not opposed to granting the requested EDUs; however, he is opposed to the timing of the approval since the County's procedures normally require Water and Sewer Plan compliance and RPC Step 1 approval by the Planning Commission prior to allocating EDUs. Commissioner Elder concurred.

Public Works Director John Tustin reviewed the results of the recent online auction of County surplus property on GovDeals.com and stated that the County earned \$94,723.22 through the auction, with \$23,000 to be refunded to the Water and Wastewater Enterprise Funds and \$1,089 to the Solid Waste Enterprise Fund, with the balance to the County's General Fund.

Pursuant to the recommendation of Mr. Tustin in response to a request by the Sheriff's Office and upon a motion by Commissioner Bertino, the Commissioners unanimously approved an addendum to the five-year contract with Bennet Waste of Fruitland, Maryland for option 2 to exchange the two-cubic-yard refuse container at Animal Control, which is dumped weekly at a cost of \$62.12 per month, for a four-cubic-yard container, to continue to be dumped weekly at a cost of \$87.16 per month to more adequately manage the waste needs of the Animal Control facility.

Pursuant to the recommendation of Mr. Tustin and upon a motion by Commissioner Bertino, the Commissioners unanimously approved bid specifications for a six-space, secured parking facility for Circuit Court Judges at the Worcester County Court House campus. Mr. Tustin advised that Circuit Court grant funds of \$120,000 are available to complete this project.

Pursuant to the recommendation of Mr. Tustin and upon a motion by Commissioner

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Lockfaw, the Commissioners unanimously approved bid specifications for the purchase of one trailer with hydraulic lift gate for the Roads Division of Public Works.

The Commissioners met with Mr. Tustin to review the results of a speed study on Scotts Landing Road, which was conducted from August 20-25, 2018 at the request of an area resident to address safety concerns and speeding vehicles and to consider posting a speed limit sign. Mr. Tustin reviewed the results and noted that, while there were three incidents of excessive speed over 70 miles per hour on Scotts Landing Road during the speed study, the average speed of vehicles traveling this road was 27 miles per hour. Furthermore, roughly two thirds of the traffic traveled at speeds less than 35 miles per hour. Therefore, based on the nature of the geographical area and differences in opinion with other residents on Scotts Landing Road, he recommended making no changes at this time and leaving the road unposted, which allows a maximum speed of 50 miles per hour. Upon a motion by Commissioner Lockfaw, the Commissioners concurred with staff's recommendation and agreed to request that the Sheriff's Office enforce the existing speed limit of 50 miles per hour on this unposted road.

Pursuant to the recommendation of Mr. Tustin and upon a motion by Commissioner Bertino and subject to adding the name of Advisory Committee members and posting the brochure on the County website, the Commissioners unanimously approved the inclusion of an informational brochure to Ocean Pines Water and Wastewater customers in the September 30, 2018 billing and approved black and white reproduction costs of \$678.06 to print the brochures. Mr. Tustin advised that the brochures include information about ongoing system improvements and cost-saving activities in the Ocean Pines Sanitary Service Area (SSA).

Pursuant to the recommendation of Mr. Tustin and upon a motion by Commissioner Mitrecic, the Commissioners unanimously authorized Commission President Purnell to sign the Worcester County Small Project Agreement between the County Commissioners and Steen and Associates (developer) to construct water and sewer lines to serve the Triple Crown Estates subdivision north of Gum Point Road in the Ocean Pines Sanitary Service Area (SSA).

Pursuant to the recommendation of Development Review and Permitting (DRP) Director Ed Tudor and upon a motion by Commissioner Mitrecic, the Commissioners unanimously approved the Request for Proposals (RFP) for the demolition and removal of the structure located at 13412 Madison Avenue in northern Worcester County. Mr. Tudor advised that staff sent letters to the property owner on September 5, 2018, following the Commissioners' declaration of this property as a nuisance; however, the letters were returned as undeliverable on September 12, 2018. Therefore, abatement actions may commence after September 29, 2018.

Pursuant to the recommendation of Mr. Tudor and upon a motion by Commissioner Mitrecic, the Commissioners unanimously adopted the Findings of Fact and Zoning Classification Resolution No. 18-3, for the rezoning of 4.7 acres of land located on the northwesterly side of Market Street (Business U.S. Rt. 113) to the west of U.S. Rt. 113 outside the Snow Hill Corporate limits, and more specifically identified on Tax Map 55 as part of Parcel 22 in the Second Tax District of Worcester County, Maryland, from A-1 Agricultural District to

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C-2 General Commercial District, as approved following the Commissioners' public hearing on this matter at their meeting on September 4, 2018.

The Commissioners met in legislative session.

The Commissioners met with County Attorney Maureen Howarth to review a text amendment application drafted by staff to revise the Public Safety Animal Control section of the County Code to address animals being kept outdoors in unsafe weather conditions, the tethering and restraint of animals, and to clarify what constitutes as appropriate shelter sizes based on the size of the animals.

Following some discussion, Commissioners Bertino, Bunting, Church, Elder, Lockfaw, Mitrecic, and Purnell introduced the aforementioned text amendment as Bill 18-4 (Public Safety - Animal Control) and scheduled a public hearing on the bill for a special legislative session to be held on October 23, 2018.

The Commissioners met with Assistant Chief Administrative Officer Kelly Shannahan to review a text amendment application drafted by staff, as a housekeeping measure, to address standards for reclamation of County-owned surface mining operations. This measure is required in order for the County to qualify for an exemption to the Maryland Surface Mining Law of 1977 by the Maryland Department of the Environment (MDE) for continued use of the County's Langmaid Road Borrow Pit by the Roads Division of Public Works.

Following some discussion, Commissioners Bertino, Bunting, Church, Elder, Lockfaw, Mitrecic, and Purnell introduced the aforementioned text amendment as Bill 18-5 (Zoning - Surface Mining) and scheduled a public hearing on the bill for October 23, 2018.

The Commissioners met with Mr. Shannahan to review a text amendment application drafted by staff, as a housekeeping measure, to increase the competitive bidding threshold from \$10,000 to \$25,000, to reflect changes recently adopted by the County Commissioners in Resolution No. 18-19 Revising and Re-adopting Financial Management Rules and Procedures for County Departments.

Following some discussion, Commissioners Bunting, Church, Elder, Lockfaw, Mitrecic, and Purnell introduced the aforementioned text amendment as Bill 18-6 (County Government - Competitive Bid Threshold) and scheduled a public hearing on the bill for October 23, 2018.

The Commissioners adjourned their legislative session.

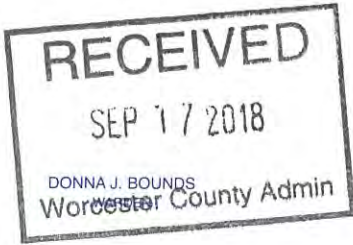
The Commissioners answered questions from the press, after which they adjourned to meet in closed session.

Following a motion by Commissioner Mitrecic, seconded by Commissioner Elder, the Commissioners unanimously voted to meet in closed session at 11:44 a.m. in the Commissioners' Conference Room to discuss matters permitted under the provisions of Section 3-305(b)(3) of the General Provisions Article of the Annotated Code of Maryland and to perform administrative functions. Also present at the closed session were Harold L. Higgins, Chief Administrative Officer; Kelly Shannahan, Assistant Chief Administrative Officer; Maureen Howarth, County Attorney; Kim Moses, Public Information Officer; Recreation and Parks

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Director Tom Perlozzo, Economic Development Deputy Director Kathryn Gordon; Development Review and Permitting Director Ed Tudor; and Finance Officer Phil Thompson. Topics discussed and actions taken included discussing potential acquisition of real property for public purposes.

After the closed session, the Commissioners adjourned to meet again on October 2, 2018.



**Worcester County
Jail**

P.O. BOX 189
SNOW HILL, MARYLAND
21863

TEL: 410-632-1300
FAX: 410-632-3002

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QUINTIN L. DENNIS
SECURITY

FULTON W. HOLLAND JR.
CLASSIFICATION

September 17, 2018

Harold Higgins, Chief Administrative Officer
Worcester County Administration
Room 1103 Government Center
One West Market Street
Snow Hill, MD 21863

RE: Request for Memorandum of Understanding with Maryland State Police Forensic Sciences Division

Dear Mr. Higgins,

I am submitting a request to enter into a Memorandum of Understanding with the Maryland State Police Forensic Sciences Division. The Maryland State Police Forensic Sciences Division is requesting the assistance of the Worcester County Jail to collect and submit DNA samples for specified offenders. Designated officers will be trained in DNA collection procedures by the Director of the Forensic Sciences Division and the officers will be required to complete training annually. The materials for the collection of DNA samples and the training will be provided by the Maryland State Police at no cost to Worcester County. I recommend approval to sign the memorandum of understanding. Please contact me if you have additional questions.

Sincerely,

Donna J. Bounds, Warden
Worcester County Jail

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**MEMORANDUM OF UNDERSTANDING
BETWEEN THE
MARYLAND STATE POLICE FORENSIC SCIENCES DIVISION
AND THE COUNTY COMMISSIONER OF WORCESTER COUNTY, MARYLAND
on behalf of the
WORCESTER COUNTY JAIL
REGARDING
COLLECTION OF DNA SAMPLES REQUIRED UNDER MARYLAND LAW**

This Memorandum of Understanding (MOU) regarding the collection of DNA samples required under Maryland law is made by and between the Maryland Department of State Police Forensic Sciences Division (FSD) and the County Commissioners of Worcester County, Maryland on behalf of the Worcester County Jail [hereinafter “the Parties”] and is effective on the date of the last signature below.

WHEREAS, FSD operates the State of Maryland’s database of DNA samples for the purpose of identifying individuals, *see* Md. Code Ann., Pub. Safety §2-501 *et seq.* and COMAR 29.05.01 *et. seq.*; and

WHEREAS, in order to populate the database with samples of known individuals, Maryland law requires the collection of DNA samples from individuals charged with certain crimes as well as those convicted of certain crimes, *see* Md. Code Ann., Pub. Safety §2-504; and

WHEREAS, the Director of the FSD is tasked with receiving DNA samples from the specified offenders and designating those who are authorized to collect DNA samples. In Worcester County the FSD has been collecting samples for the Worcester County Jail; and

WHEREAS, most jurisdictions have assumed full responsibility for collection of DNA samples from those who are required to submit samples by law and the Parties agree that it is in the interests of public safety to ensure the timely and complete collection of those samples; and

WHEREFORE, the Parties to this MOU agree as follows:

1. **Collection:** When a person who is required to submit a DNA sample by law is brought to the Worcester County Jail upon charging or conviction, the Worcester County Jail will be responsible for collection of a DNA sample.
2. **Designating Collectors:** DNA samples must be collected by a person trained in DNA collection procedures and designated by the Director of FSD. Upon completion of an online DNA collection training video created by FSD, a collector is deemed designated by the Director. The training must be completed annually and the Worcester County Jail will maintain the certification record that is produced at the end of the training to demonstrate compliance.

3. **Collection Kits:** Maryland State Police will supply the Worcester County Jail with DNA collection kits for this purpose upon request.
4. **Liability:** Each party will be responsible for the acts and omissions of its own personnel.
5. **Choice of Law:** This MOU is governed by Maryland law.
6. **Duration:** This MOU shall be deemed in effect until terminated by one or both of the Parties or the law no longer requires the DNA collection.
7. **Termination:** The parties may terminate this MOU at any time, with or without cause. Prior written notice of 30 days of the intent to terminate the MOU will, whenever practicable, be made to the other party. Termination of this MOU does not affect the Parties' respective responsibilities under the statute and regulations governing collection of DNA samples.
8. **Amendments:** This MOU shall not be amended or modified unless in writing signed by both parties.
9. **Authority:** The undersigned represent and affirm that the signors have the power and authority to bind their respective agencies to the terms of this MOU.

THEREFORE, we, the undersigned, hereby agree to the provisions of this Memorandum of Understanding, listed above, regarding the collection of DNA Samples required under Maryland law.

AUTHORIZATIONS:

Maryland State Police:

By: _____ (Seal)
Approved for Form and Legal Sufficiency

Date: _____

By: _____ (Seal)
Colonel William M. Pallozzi, Superintendent

Date: _____

By: _____ (Seal)
Daniel E. Katz, FSD Director

Date: _____

County Commissioners of Worcester County, Maryland:

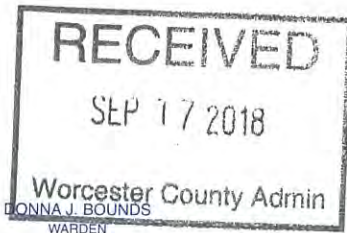
By: _____ (Seal)
Diana Purnell, President

Date: _____

Worcester County Jail:

By: _____ (Seal)
Donna Bounds

Date: _____



Worcester County Jail

P.O. BOX 189
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QUINTIN L. DENNIS
SECURITY

FULTON W. HOLLAND JR.
CLASSIFICATION

September 17, 2018

Harold Higgins, Chief Administrative Officer
Worcester County Administration
Room 1103 Government Center
One West Market Street
Snow Hill, MD 21863

Greetings Mr. Higgins,

As I believe you are aware, the current FY19 Budget for the Jail includes funding in the amount of \$25,103.00 for the purchase of a new tractor and finish mower. In accordance with the recently revised Financial Management Rules and Procedures for County Departments and anticipating a cost of just under \$25,000, I solicited three proposals from known suppliers in the area representing John Deere, Kubota and Kioti for the purchase of this equipment. The following is a summary of the proposals received:

<u>Company Name</u>	<u>Model (mower/groomer)</u>	<u>Total Quote</u>
John Deere (Atlantic Tractor, LLC) Salisbury, MD	John Deere 3038E Mower (\$22,875.21) Frontier GM 1660E Groomer (\$2277.03)	\$25,152.24
Kubota (Burke Equipment Company) Delmar, DE	Kubota (\$22,198.17) (LandPride FDR 1972-13 \$2682.50))	\$24,879.67
Kioti (David A. Banks, Inc.) Georgetown, DE	Kioti Model CK3510H(\$19,000) Woods model PRD7200W (\$3300.00)	\$22,300.00

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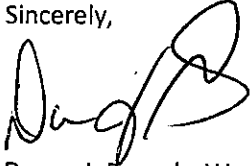
After reviewing the quotes and based upon anticipated reliability, potential service, experience with previous County purchases of John Deere equipment and replacement parts availability from the local John Deere dealer located in Salisbury Maryland, I recommend selecting the proposal from Atlantic Tractor, Inc. for the John Deere model 3038 E tractor with the Frontier GM 1660 E Discharge Grooming Mower. The Kubota dealership is located in Delmar, Delaware and the Kioti dealership is located in Georgetown, Delaware which would require additional travel and down time when service may be required. As you will see, the total cost of the John Deere model is slightly higher than the Kubota; however we believe that the John Deere is the best bid for the County and worth the additional \$272.57. Finally, while we had anticipated that the total cost would

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be under the \$25,000 bid threshold, the actual price is slightly higher. Therefore, I am requesting that the County Commissioners waive the formal bidding requirements and approve the award to Atlantic Tractor, Inc. Salisbury, Maryland at a total cost of \$25,152.24, funding for which is included in the Jail's FY19 budget in account number 9010.070 and account number 6550.020.

Should you have any questions, please feel free to call.

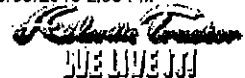
Sincerely,

A handwritten signature in black ink, appearing to read 'Donna J. Bounds', with a large, stylized flourish at the end.

Donna J. Bounds, Warden
Worcester County Jail



JOHN DEERE



Quote Summary

Prepared For:

Worcester County Jail
Donna Bounds
5022 Joyner Rd
Snow Hill, MD 21863
Home: 410-632-1304
dbounds@co.worcester.md.us

Prepared By:

Brian Little
Atlantic Tractor LLC
31415 John Deere Drive
Salisbury, MD 21804-1413
Phone: 410-860-0676
blittle@atjd.net

Quote Id: 17863129
Created On: 24 July 2018
Last Modified On: 06 September 2018
Expiration Date: 28 September 2018

Equipment Summary	Suggested List	Selling Price	Qty	Extended
JOHN DEERE 3038E Compact Utility Tractor	\$ 28,241.00	\$ 22,875.21 X	1 =	\$ 22,875.21
Equipment Total				\$ 22,875.21

Quote Summary

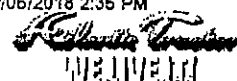
Equipment Total	\$ 22,875.21
SubTotal	\$ 22,875.21
Est. Service Agreement Tax	\$ 0.00
Total	\$ 22,875.21
Down Payment	(0.00)
Rental Applied	(0.00)
Balance Due	\$ 22,875.21

Salesperson : X _____

Accepted By : X _____



JOHN DEERE



Selling Equipment

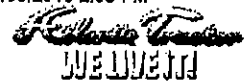
Quote Id: 17863129

Customer: WORCHESTER COUNTY JAIL

JOHN DEERE 3038E Compact Utility Tractor				
Hours:				Suggested List
Stock Number:				\$ 28,241.00
				Selling Price
				\$ 22,875.21
Code	Description	Qty	Unit	Extended
1482LV	3038E Compact Utility Tractor	1	\$ 21,873.00	\$ 21,873.00
Standard Options - Per Unit				
0202	United States	1	\$ 0.00	\$ 0.00
0409	English Operator's Manual and Decal Kit	1	\$ 0.00	\$ 0.00
1701	Factory Installed Loader with Bucket	1	\$ 4,994.00	\$ 4,994.00
4060	iMatch Quick Hitch Category 1	1	\$ 291.00	\$ 291.00
4110	Draft Links - Flat Bar (Standard)	1	\$ 0.00	\$ 0.00
4310	Rear Drawbar	1	\$ 124.00	\$ 124.00
5210	15-19.5 6PR R4 Industrial	1	\$ 148.00	\$ 148.00
6210	25x8.50-14 6PR R4 Industrial	1	\$ 55.00	\$ 55.00
Standard Options Total				\$ 5,612.00
Value Added Services Total				\$ 0.00
Other Charges				
	Freight	1	\$ 406.00	\$ 406.00
	setup 6	1	\$ 350.00	\$ 350.00
Other Charges Total				\$ 756.00
Suggested Price				\$ 28,241.00
Customer Discounts				
Customer Discounts Total			\$ -5,365.79	\$ -5,365.79
Total Selling Price				\$ 22,875.21



JOHN DEERE



Quote Summary

Prepared For:

Worcester County Jail
Donna Bounds
5022 Joyner Rd
Snow Hill, MD 21863
Home: 410-632-1304
dbounds@co.worcester.md.us

Prepared By:

Brian Little
Atlantic Tractor LLC
31415 John Deere Drive
Salisbury, MD 21804-1413
Phone: 410-860-0676
blittle@atjd.net

Quote Id: 18126553
Created On: 06 September 2018
Last Modified On: 06 September 2018
Expiration Date: 13 September 2018

Equipment Summary	Suggested List	Selling Price	Qty	Extended
Frontier GM1060E Economy Rear Discharge Grooming Mower	\$ 2,811.15	\$ 2,277.03 X	1 =	\$ 2,277.03
Equipment Total				\$ 2,277.03

Quote Summary

Equipment Total	\$ 2,277.03
SubTotal	\$ 2,277.03
Est. Service Agreement Tax	\$ 0.00
Total	\$ 2,277.03
Down Payment	(0.00)
Rental Applied	(0.00)
Balance Due	\$ 2,277.03

Salesperson : X _____

Accepted By : X _____

5



JOHN DEERE



Selling Equipment

Quote Id: 18126553

Customer: WORCHESTER COUNTY JAIL

Frontier GM1060E Economy Rear Discharge Grooming Mower				
Hours:				Suggested List
Stock Number:				\$ 2,811.15
				Selling Price
				\$ 2,277.03
Code	Description	Qty	Unit	Extended
0462XF	GM1060E Economy Rear Discharge Grooming Mower	1	\$ 2,540.00	\$ 2,540.00
Other Charges				
	setup 6	1	\$ 200.00	\$ 200.00
	Freight	1	\$ 71.15	\$ 71.15
	Other Charges Total			\$ 271.15
	Suggested Price			\$ 2,811.15
Customer Discounts				
	Customer Discounts Total		\$ -534.12	\$ -534.12
Total Selling Price				\$ 2,277.03

BURKE EQUIPMENT COMPANY

11196 E. SNAKE RD - DELMAR, DE 19940



Quote

DATE: August 3, 2018

Sales Representative: **Dave Bloodsworth**
302-248-7070 Office
410-251-0435 Mobile
302-248-7075 Fax
dbloodsworth@burkeequipment.com

BILL TO NAME 1: Worcester Co.
ADDRESS 1:
ADDRESS 2:
CITY, STATE, ZIP:
COUNTY:
HOME OR OFFICE PH:
MOBILE PH:
FAX:
EMAIL:

CUSTOMER PO#

Make	Model	Serial Number	Description	Amount
Kubota	L3901HST		37hp Compact Tractor w/ R1 Tires Front End Loader, 4x4 and 540 PTO	
LA525			Front End Loader w/ 66" Bucket	\$26,753.00
			SOURCEWELL DISCOUNT	-\$5,885.66
			Dealer Assembly, PDI, Freight	\$1,330.83
LandPride	FDR1972-13		72" Finish Mower	\$2,982.00
			SOURCEWELL DISCOUNT	-\$745.50
			Dealer Assembly, PDI, Freight	\$445.00

TRADE IN INFORMATION

Make	Model	Serial Number	Description	ALLOWANCES

TOTAL TRADE ALLOWANCES: \$0.00

Outstanding Loan Balance:

TRADE OR CONSIGNMENT AGREEMENT:
I AGREE THERE IS NO LIEN ON ANY TRADE OR CONSIGNMENT EQUIPMENT
INITIAL HERE ->
IF THERE IS A LIEN LIST THE LIEN HOLDER HERE:
WHAT INSURANCE COMPANY INSURES THIS EQUIPMENT?
WHAT IS THE PAYOFF?

Invoice Subtotal:	\$24,879.67
Less Trade Allow + Outstanding Loan Bal:	\$0.00
SUBTOTAL:	\$24,879.67
TAX:	\$0.00
TAG & TITLE FEES:	\$0.00
LESS DEPOSIT RECEIVED:	
TOTAL AMOUNT DUE:	\$24,879.67

CUSTOMER SIGNATURE:

DATE:



Tractors > Kubota > L3301

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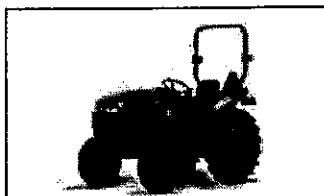


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Kubota L3301



2014 - Standard L Series

Compact Utility tractor

Series next: [Kubota L3901](#)

Series back: [Kubota L2501](#)

- Overview
- Engine
- Transmission
- Dimensions
- Photos
- Attachments

Production:
 Manufacturer: **Kubota**

Kubota L3301 Engine:
 Kubota 1.8L 3-cyl diesel
[full engine details ...](#)

Capacity:
 Fuel: **11.1 gal [42.0 L]**

3-Point Hitch:
 Rear Type: **1**
 Control: **position control**
 Rear lift (at ends): **1985 lbs [900 kg]**
 Rear lift (at 24"/610mm): **1433 lbs [650 kg]**

Power Take-off (PTO):
 Rear PTO: **transmission***
live*

Clutch: **overrunning dry disc**
 Rear RPM: **540 (1.375)**

Dimensions & Tires:
 Wheelbase: **63.3 inches [160 cm]**
 Weight: **2557 to 2778 pounds**
 Front tire: **5.00-15**
 2WD Rear tire: **11.2-24**
[full dimensions and tires ...](#)

Kubota L3301 attachments:
 front-end loader
 backhoe
[all L3301 attachments ...](#)

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Kubota L3301 Power:

Engine (gross):	33 hp [24.6 kW]
Engine (net):	31.4 hp [23.4 kW]
Gear PTO (claimed):	27.7 hp [20.7 kW]
Hydro PTO (claimed):	26.2 hp [19.5 kW]

Mechanical:

Chassis:	4x2 2WD*
	4x4 MFWD 4WD
Steering:	power
Brakes:	mechanical wet disc
Cab:	Two-post folding ROPS.

Hydraulics:

Type:	open center
Pressure:	2347 psi [161.8 bar]
Pump flow:	6.3 gpm [23.8 lpm]
Steering flow:	3.8 gpm [14.4 lpm]

Electrical:

8

L3301 Serial Numbers:

[how to read serial numbers...](#)

Ground:	negative
Charging system:	alternator
Charging amps:	40
Charging volts:	12

Battery:	
Number:	1
Cold-cranking amps:	580
Volts:	12

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Tractors > Kubota > L3301

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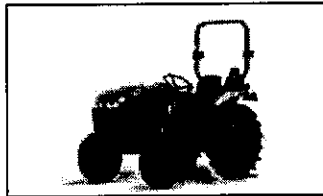


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VIEW

Kubota L3301 - engine



2014 - Standard L Series

Compact Utility tractor

Series next: [Kubota L3901](#)

Series back: [Kubota L2501](#)

[more Kubota L3301 specifications...](#)

- Overview
- Engine
- Transmission
- Dimensions
- Photos
- Attachments

Engine Detail:

Kubota
diesel
3-cylinder
liquid-cooled
111.4 ci [1.8 L]



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Bore/Stroke:	3.43x4.03 inches [87 x 102 mm]
Emissions:	Tier IV
Power (gross):	33 hp [24.6 kW]
Fuel system:	common rail direct injection
Rated RPM:	2700
Starter:	electric
Starter volts:	12

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Tractors > Kubota > L3301

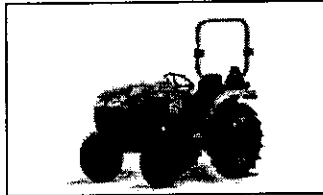
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Kubota L3301 - transmission



2014 - Standard L Series

Compact Utility tractor

Series next: [Kubota L3901](#)

Series back: [Kubota L2501](#)

[more Kubota L3301 specifications...](#)

- Overview
- Engine
- Transmission
- Dimensions
- Photos
- Attachments



Transmission:

Type: synchronized shuttle

Gears: 8 forward and reverse

Clutch: dry disc

Four gears (1-4) in two ranges with mechanical shuttle.

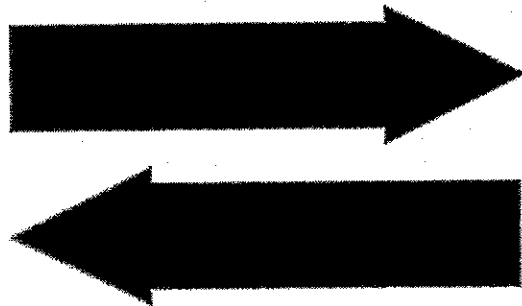
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Speeds:

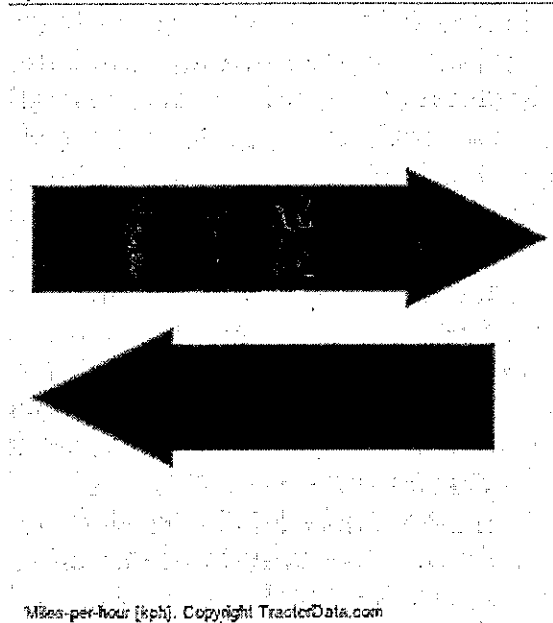


Transmission:

Type: hydrostatic

Gears: infinite (3-range) forward and reverse

Speeds:



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tractors-zero turns - Cub Cadet Lawn Mowers



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Tractors > Kubota > L3301

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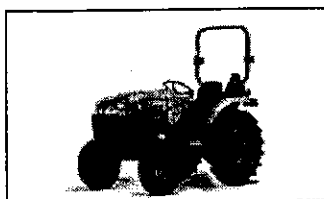


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Kubota L3301 - dimensions



2014 - Standard L Series

Compact Utility tractor

Series next: [Kubota L3901](#)

Series back: [Kubota L2501](#)

[more Kubota L3301 specifications...](#)

- Overview
- Engine
- Transmission
- Dimensions
- Photos
- Attachments

Kubota L3301 Tires:

2WD Standard tires (ag): Front: 5.00-15. Rear: 11.2-24

4WD Standard tires (ag): Front: 7.2-16. Rear: 11.2-24

Lawn/turf front: 23x8.50-12 (2WD)

25x8.50-14 (4WD)

Lawn/turf rear: 13.6-16

Industrial front: 27x8.50-15 (4WD)

Industrial rear: 15-19.5 (4WD)

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Dimensions:

2WD Gear Weight: 2557 lbs [1159 kg]

4WD Gear Weight: 2734 lbs [1240 kg]

4WD Hydro Weight: 2778 lbs [1260 kg]

Wheelbase: 63.3 inches [160 cm]

2WD Length: 110.6 inches [280 cm]

4WD Length: 107.9 inches [274 cm]

Width: 55.1 inches [139 cm]

Height (ROPS): 91.7 inches [232 cm]

2WD Ground clearance: 13.6 inches [34 cm]

4WD Ground clearance: 13.4 inches [34 cm]

Front tread: 41.3 inches [104 cm] (2WD) 43.1 inches [109 cm] (4WD)

Rear tread: 43.8/47.1/50.8 inches [111/119/129 cm]

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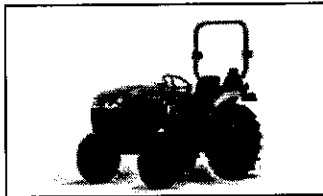


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Kubota L3301 - attachments



2014 - Standard L Series

Compact Utility tractor

Series next: [Kubota L3901](#)

Series back: [Kubota L2501](#)

[more Kubota L3301 specifications...](#)

Overview	Engine	Transmission	Dimensions	Photos	Attachments
--------------------------	------------------------	------------------------------	----------------------------	------------------------	-----------------------------

Loader:

Loader type:	Kubota LA525
Height (to pin):	94.3 inches [239 cm]
Clearance, dumped bucket:	76.2 inches [193 cm]
Dump reach:	25.4 inches [64 cm]
Dump angle:	40
Reach at ground:	63.7 inches [161 cm]
Rollback angle:	31
Breakout force (at pin):	2462 lbs [1116 kg]
Breakout force (at 500mm):	1874 lbs [850 kg]
Lift to full height (at pin):	1131 lbs [513 kg]
Lift to full height (at 500mm):	855 lbs [387 kg]
Lift to 1.5m (at pin):	1490 lbs [675 kg]
Lift to 1.5m (at 500mm):	1182 lbs [536 kg]
Raise time to height:	3.5 s
Bucket dump time:	1.7 s
Lowering time:	2.3 s
Rollback time:	2.2 s

Backhoe:

Backhoe type:	Kubota BH77
2-Foot flat digging depth:	89.8 inches [228 cm]
Reach from pivot:	124.3 inches [315 cm]
Loading height:	65.7 inches [166 cm]
Bucket Rotation:	180
Bucket force:	3421 lbs [1551 kg]
Dipperstick force:	2190 lbs [993 kg]

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Cub Cadet *VI*

WOODS

Drive 1-443-888-7701
Smith

email: d.smith@co.worcester.ma.us

DAVID A. BANKS INC.
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Georgetown, DE 19947

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CUSTOMER ACCT. NO.

NAME *Worcester County Sheriff Office*

ADDRESS

CITY STATE ZIP

CUSTOMER ORDER NO.

DATE *7-19-18*

MERCHANDISE SOLD			MDSE. RETD.		RECEIVED ON		PAID OUT		SOLD BY
CASH	CHARGE	C. O. D.	CASH	CREDIT	ACCOUNT	NOTE			
	<i>Quote-</i>								<i>Mark</i>

QTY.	DESCRIPTION	PRICE	AMOUNT
<i>1</i>	<i>New 2018 Kioti Compact Tractor 35HP Diesel Engine, 4x4 - R-4 Tires Hydro Trans - Model - CK3510H-</i>		<i>19,000.00</i>
<i>1</i>	<i>New 2018 Kioti Front-End Loader - w/66" Quick Attach Bucket - Ball on Cutting Edge - Model - VL4010-</i>		
<i>1</i>	<i>New Woods - 6' Finish Mower - Model - PRD7200W-</i>		<i>3300.00</i>
<i>1</i>	<i>New Woods 5' Finish Mower - Model - PRD6000W-</i>		<i>3150.00</i>

*Thanks
Any Questions
CALL MARK*

168819

SIGNATURE

TAX

TOTAL

\$

15



ISO 9001
NO.954596



ISO 14001
NO.771475



OHSAS 18001
NO.K033008

Specifications



MODEL	CK2610	CK2610H	CK3510	CK3510H	CK4010	CK4010H
ENGINE						
Model	3A165LWM-U	3A165LWM-U	3F-TM4-U	3F-TH4-U	3F-TM4-U	3F-TH4-U
Type	3 Cyl., In-line Vertical, Water-cooled 4-cycle Diesel		3 Cyl., In-line Vertical, Water-cooled 4-cycle Diesel, CRDI			
Engine Gross	HP(KW)	24.5(18.2)	34.9(26)		39.6(29.5)	
PTO @ 540 rpm	HP(KW)	21.7(16.2)	21.5(16.0)	31.8(23.7)	30.8(22.9)	34.8(25.9)
Displacement	cu in.(cc)	100.5 (1,647)		111.4 (1,826)		
Rated Revolution	rpm	2,600				
Fuel Tank Capacity	us gal(ℓ)	9.0 (34)				
DRIVE TRAIN						
Clutch	Dry dual stage	Dry single stage	Dry dual stage	Dry single stage	Dry dual stage	Dry single stage
Transmission	Manual	HST	Manual	HST	Manual	HST
Speeds	9X3	Infinite	9X3	Infinite	9X3	Infinite
Differential Lock	Rear Standard					
Brake	Wet Multi Disc					
PTO	Live					
Rear PTO (standard)	rpm	540				
Mid PTO (optional)	rpm	(2,000)				
4WD type	Mechanical					
HYDRAULIC SYSTEM						
Pump	Gear					
Pump Capacity (Max. flow rate)	gpm(lpm)	11.7 (44.4)				
3-Point Hitch	Category 1					
Maximum lifting capacity (24"alt. of hitch)	lbs(kg)	1,631 (740)				
Hydraulic Lift Control System	Position					
No. of standard remote valves	Opt. (2)					
Steering	Hydrostatic Power Steering					
TIRE SIZE						
Front AG.(TURF, INDUSTRIAL)	7-16 (28X8.5-15/6, 27X8.5-15)					
Rear AG.(TURF, INDUSTRIAL)	11.2-24 (41X14-20/4, 15-19.5)					
TRAVELING SPEEDS						
Forward	mph(km/h)	0.94~14.63 (1.52~23.56)	0~15.53 (0~25)	0.94~14.63 (1.52~23.56)	0~15.53 (0~25)	0.94~14.63 (1.52~23.56)
Reverse	mph(km/h)	1.18~9.56 (1.9~15.39)	0~15.83 (0~25.48)	1.18~9.56 (1.9~15.39)	0~15.83 (0~25.48)	1.18~9.56 (1.9~15.39)
DIMENSIONS						
Overall Length (includes 3-pt hitch)	in.(mm)	120.9 (3,070)				
Overall Width (minimum tread)	in.(mm)	54.5 (1,385)				
Overall Height	in.(mm)	99.2 (2,520)				
Wheelbase	in.(mm)	65.7 (1,670)				
Min. Ground Clearance	in.(mm)	13.4 (340)				
Tread	Front (AG)	44.6 (1,132)				
	Rear (AG)	51.3 (1,302)				
Min. Turning Radius (with brake)	ft(m)	8.0 (2.4)				
Weight with AG, Tires	lbs (kg)	2,646 (1,200)	2,701(1,225)	2,679 (1,215)	2,734 (1,240)	2,679 (1,215)

Standard Equipment

- Parking Brake
- Illuminated Dashboard
- Head Lamp
- Signal Lamp
- Flip Up PTO Cover
- Cup Holder
- Single Lever Joystick

Optional Equipment

- Field Option Cabin
- Mid PTO Kit
- Rear Working Light Kit
- Cruise Control (HST only)
- Link Pedal (HST only)
- Dual Remote Valves (2 Spring Valves)
- Dual Remote Valves (2 Detent Valves)
- Dual Remote Valves (1 Detent Valve & 1 Spring Valve)
- Sunshade
- Clevis Type Drawbar
- Tool Box
- Side View Mirror
- Telescopic Lower Link
- Fuel Heater Kit (CK3510, CK3510H, CK4010, CK4010H only)
- Dual Air Cleaner Element
- Deck Mat
- Lift Rod Kit
- Armrest
- Horn

Attachments

- Front End Loader
- Backhoe
- Contact your local KIOTI dealer for available implements and attachments



KIOTI Tractor Division
DAEDONG - USA, INC.
6300 KIOTI Drive, Wendell, NC 27591
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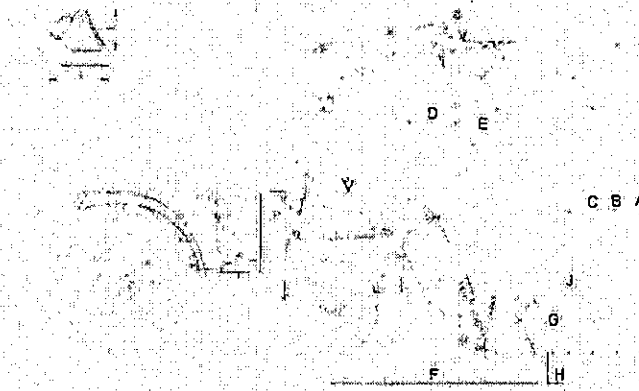
K6

KIOTI Tractor / Daedong Ind. Co., Ltd. / 04.17

*NOTE : Specifications and design are subject to change without notice.

Front End Loader

The KIOTI KL2610 and KL4010 front end loaders are specifically designed for the CK10 Series model tractors, providing efficient performance with single lever joystick control. The KIOTI loader, with a built-in parking stand, is designed to be quickly and easily attached and detached. KL Series loaders utilize the tractor's hydraulic system, feature single-lever control with float and come with a bucket level indicator.



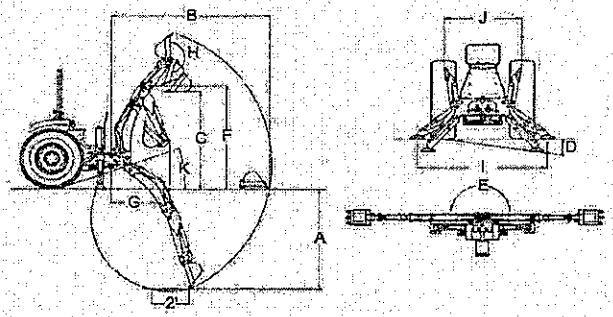
Loader Specifications

Model	KL2610	KL4010
A Maximum Lift Height	98.4 in (2,500mm)	98.4 in (2,500mm)
B Clearance w/ Attachment Level	88.0 in (2,234mm)	93 in (2,360mm)
C Clearance w/ Attachment Dumped (45°)	74.0 in (1,864mm)	79.1 in (2,010mm)
D Reach at Maximum Height	11.3 in (286mm)	27.2 in (690mm)
E Maximum Dump Angle	48°	48°
F Reach w/ Attachment on Ground	63.4 in (1,610mm)	62.2 in (1,580mm)
G Maximum Rollback Angle	28.5°	28°
H Digging Depth Below Grade	4.8 in (112mm)	5.3 in (135mm)
J Overall Height in Carry Position	53.1 in (1,349mm)	49.6 in (1,260mm)
L Depth of Attachment (to back of inner shell)	19.1 in (485mm)	19.9 in (505mm)
M Height of Attachment	19.7 in (501mm)	20.7 in (526mm)
N Depth of Attachment (to pivot pin)	24.3 in (616mm)	26.9 in (682mm)
Lift Capacity to Full Height at Pivot Pins	1,252 lbs (568 kg)	1,835 lbs (832 kg)
Breakout Force at Pivot Pin	2,110 lbs (957 kg)	3,464 lbs (1,571 kg)
Relief valve setting (Loader control valve)	2,567 psi	2,560 psi
Rated flow (Tractor system)	7.1 gpm	7.1 gpm
Bucket Width	60 in / 66 in	60 in / 66 in
Approx. Weight (with Bucket)	915-928 lbs (415-421 kg)	840 lbs (381 kg)
Cycle Times at 2,600 rpm (Full)	KL2610	KL4010
Raise Boom Frame	4.6	3.9
Lower Boom Frame	3.4	2.3
Dump Bucket	3.3	3.2
Retract Bucket	2.7	1.9

*Specifications and design are subject to change without notification.

Backhoe

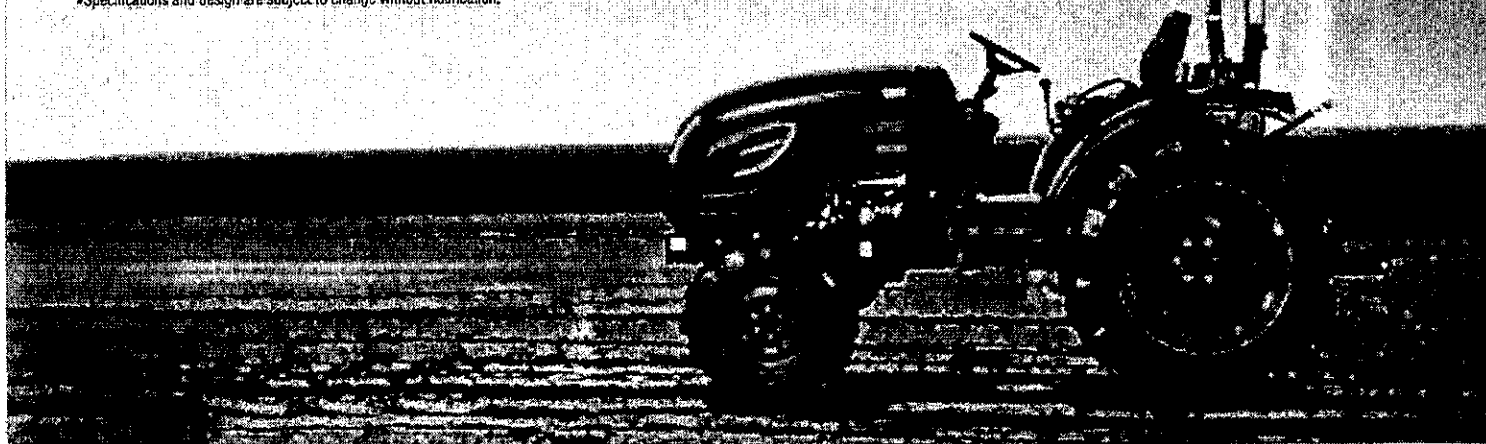
The KIOTI KB2475L backhoe lets you trench up to 89.7" deep with a bucket dig force up to 2,817 pounds. The KB2475L Backhoe features subframe mounting, responsive two-lever controls with exceptional feathering, hydraulic stabilizers, transport lock and a variety of standard and heavy-duty buckets.



Backhoe Specifications

Model	KB2475L			
A Digging Depth (two foot flat bottom)	89.7 in. (2,280 mm)			
B Reach from center line of Swing Pivot	125.1 in. (3,180 mm)			
C Loading Height (bucket at 60°)	72.4 in. (1,840 mm)			
D Maximum Leveling Angle	10°			
E Swing Arc	180°			
F Transport Height (maximum)	83.5 in. (2,120 mm)			
G Transport Overhang	49 in. (1,245 mm)			
H Bucket Rotation	180°			
I Stabilizer Spread (down position)	68.7 in. (1,745 mm)			
J Stabilizer Spread (up position)	46.4 in. (1,180 mm)			
K Angle of Departure	21°			
Shipping Weight (without bucket)	890 lbs (399 Kg)			
Bucket Digging Force	2,817 lbs (1,278 Kg)			
Dipperstick Digging Force	2,105 lbs (955 Kg)			
Operating Pressure	2,466 psi (170 bar)			
Cylinders				
Cylinder	Bore DIA	Retracted Length	Stroke	Rod DIA
Boom	2.75	24.41	15.12	1.57
Bucket	2.36	21.06	11.77	1.38
Dipperstick	2.75	23.5	14.84	1.57
Swing	2.36	15.67	8.78	1.38
Stabilizer	2.36	17.56	10.32	1.38
Buckets				
Bucket	Teeth Quantity	Struck Capacity	Heaped Capacity	Shipping Weight
9" Bucket	3	0.73 cu. ft.	0.87 cu. ft.	46 lbs
12" Bucket	3	1.01 cu. ft.	1.24 cu. ft.	56 lbs
16" Bucket	3	1.38 cu. ft.	1.76 cu. ft.	64 lbs
18" Bucket	4	1.56 cu. ft.	2.02 cu. ft.	70 lbs
24" Bucket	5	2.11 cu. ft.	2.81 cu. ft.	94 lbs
36" Bucket	6	2.78 cu. ft.	4.00 cu. ft.	114 lbs

*Specifications and design are subject to change without notification.





David Banks <info@davidabanks.com>


QUOTE ON KIOTI AND WOODS EQUIPMENT

3 messages

David Banks <info@davidabanks.com>
 To: dsmith@co.worcester.md.us

Thu, Jul 19, 2018 at 12:38 PM

ANY QUESTIONS CALL MARK BANKS

 **Scan_0014.pdf**
 533K

David Smith <dsmith@co.worcester.md.us>
 To: David Banks <info@davidabanks.com>

Thu, Jul 19, 2018 at 12:36 PM

Got it

thanks,

Sent from my iPhone
 Sgt. David M. Smith
 Maintenance Supervisor
 Worcester County Jail
 443-880-7701

> On Jul 19, 2018, at 12:35 PM, David Banks <info@davidabanks.com> wrote:
 >
 > ANY QUESTIONS CALL MARK BANKS
 > <Scan_0014.pdf>

David Smith <dsmith@co.worcester.md.us>
 To: David Banks <info@davidabanks.com>

Mon, Jul 23, 2018 at 10:38 AM

Good Morning,

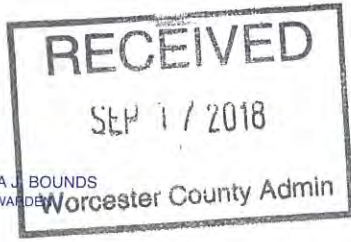
Just a couple questions on the quote.

1. Does the \$19,000 include the quick attach loader bucket?
2. Could you send us a spec sheet of the tractor.

(yes) and bolt cutting
 edge 66"

Thanks,

[Quoted text hidden]



DONNA J. BOUNDS
WARDEN



Worcester County Jail

P.O. BOX 189
SNOW HILL, MARYLAND
21863
TEL: 410-632-1300
FAX: 410-632-3002

3

QUINTIN L. DENNIS
SECURITY

FULTON W. HOLLAND JR.
CLASSIFICATION

September 17, 2018

Harold Higgins, Chief Administrative Officer
Worcester County Administration
Room 1103 Government Center
One West Market Street
Snow Hill, MD 21863

RE: Swank Motion Pictures, Inc. Contract Renewal 11/1/2018-10/31/2019

I am submitting this contract from Swank Motion Pictures, Inc. for renewal to continue to broadcast informational DVD's and entertainment DVD's to the inmate/detainee population. The cost is the same as last year, annually \$1097.80. Informational DVD's are broadcast to all housing units weekly as required by the Immigration Standards, to advise ICE detainees of their legal rights while in custody. The entertainment DVD's broadcast to the inmate/detainee population are part of the Maryland Standards and Immigration Standards as a form of recreational activities. The ability to broadcast is a vital form of communication with the jail population.

Please feel free to contact me if you have any questions.

Sincerely,

Donna J. Bounds, Warden
Worcester County Jail

*Also request authorization for annual renewals at the same cost to be signed by the Chief Administrative Officer

August 17, 2018

Donna Bounds
Warden
WORCESTER COUNTY JAIL
P. O. Box 189
Snow Hill, MD. 21863

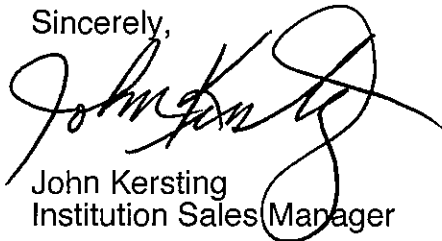
Dear Ms. Bounds:

The Public Performance Agreement between **SWANK MOTION PICTURES, INC.** and **WORCESTER COUNTY JAIL** expiring on October 31, 2018.

Attached is your 2018-2019 agreement from SWANK MOTION PICTURES, INC. If all of the information is correct, please sign both copies of the attached contract and return both to SWANK in the enclosed business reply envelope. A copy for your records will be returned to you in a few days.

Thank you and we look forward to serving your needs in the future. If you have any questions or need additional information, please call me.

Sincerely,



John Kersting
Institution Sales Manager

JK/djc

Enclosure

SWANK MOTION PICTURES, INC. PUBLIC PERFORMANCE LICENSING AGREEMENT FOR CORRECTIONAL INSTITUTIONS

This AGREEMENT is made on this 17th day of August 2018, between WORCESTER COUNTY JAIL ("Licensee" herein) and SWANK MOTION PICTURES, INC. ("Swank" herein), a Missouri corporation.

1. Subject Matter and Term of Agreement

A. Swank is an authorized distributor of copyrighted motion pictures ("DVDs" herein) for non-theatrical public performances. Swank desires to license Licensee for public performance exhibition. Licensee desires to exhibit movies licensed from Swank on the terms and conditions set forth herein.

B. The term of this Agreement shall commence on November 1, 2018 and continue October 31, 2019, after which day it shall expire unless renewed or renegotiated by mutual agreement of the parties.

2. License

Producers listed are those supplying titles at the time of this printing.

During the term of this contract, Swank shall license Licensee for public performance in its facility on the terms and conditions set forth herein. New facilities added during the term of this contract will require an amendment agreed to in writing by both parties. During the contract period, Licensee may exhibit DVD for showings only at its location(s) listed in Exhibit A. Licensee shall be entitled to choose from Swank's current and future list of available movies for public performance purposes which includes titles distributed by Paramount Pictures, Warner Bros, Bleecker Street, Fine Line Features, Lorimar Productions, New Line Cinema, Picturehouse, RKO Films, The Ladd Company, Turner Pictures, Warner Independent Pictures, Warner Premier, A24 Films, STX Entertainment, MGM/UA, American International, Orion, United Artists, Lions Gate Films, Trimark, Sony Pictures, Columbia Pictures, Epic Productions, Tristar Pictures, Triumph Films, Paramount Vantage, NBC Universal Pictures, Polygram, Focus Features, Gramercy, October Films, USA Films, Summit Entertainment, Lantern Entertainment, Buena Vista Distribution, Hollywood Pictures, Miramax, Touchstone Pictures and Walt Disney Pictures. Rentals and or purchases of titles covered by this Agreement are at the expense of the Licensee.

3. Terms

In consideration of the License, WORCESTER COUNTY JAIL shall pay Swank a License Fee of \$1,097.80 payable in one installment upon receipt of invoice.

4. Payment

Invoice is due and payable upon receipt.

5. Advertising

These motion pictures are specifically licensed for non-theatrical showings only. Promotion or advertising outside the Facility is strictly prohibited.

6. Warranties

Swank warrants that:

Swank is authorized by the copyright owners of the titles distributed by Swank to license to others for non-theatrical public performance purposes.

SWANK MOTION PICTURES, INC.
PUBLIC PERFORMANCE LICENSING AGREEMENT
continued

7. Notices

All notices to be given hereunder shall be in writing or delivered personally or mailed by pre-paid certified or registered mail (return receipt requested) as follows:

If to Swank:

SWANK MOTION PICTURES, INC.

10795 Watson Road

St. Louis, Missouri 63127-1012

**Attn: John Kersting
Institution Sales Manager**

If to Licensee:

WORCESTER COUNTY JAIL

P. O. Box 189

Snow Hill, MD. 21863

**Attn: Donna Bounds
Warden**

or to such other addresses as the parties shall specify, or by written notice so given, and shall be deemed to be given as of the date so delivered or mailed.

8. Applicable Law

This Agreement shall be governed by and construed in accordance with the laws of the State of Missouri.

EXECUTED on the day and year first above written.

SWANK MOTION PICTURES, INC.

By _____
**John Kersting
Institution Sales Manager**

WORCESTER COUNTY JAIL

By _____

Name _____

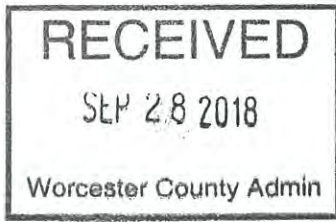
Title _____

EXHIBIT A

LOCATION:

WORCESTER COUNTY JAIL

Snow Hill, MD. 21863



4

FRED E. WEBSTER, JR.
DIRECTOR

EMERGENCY SERVICES
Worcester County
GOVERNMENT CENTER
ONE WEST MARKET STREET, ROOM 1002
SNOW HILL, MARYLAND 21863-1193
TEL: 410-632-1311
FAX: 410-632-4686

DATE: September 27, 2018
TO: Harold Higgins, Chief Administrative Officer
FROM: James Hamilton, Assistant Director *JH*
RE: P25 Radio System

Harris Corporation has provided Worcester County with the results of the execution of the Coverage Acceptance Test Plan as performed in late August and into the first week of September. This testing consisted of representatives of Harris and Worcester County driving prescribed areas of the county making voice transmissions to assess Delivered Audio Quality (DAQ) and using automated equipment provided by Harris to collect Bit Error Rate (BER) measurements. These tests were conducted with different equipment setups to simulate coverage inside of homes, buildings and other structures of construction types that may be reasonably expected in those areas. Additional tests were performed to ensure a mobile radio would receive sufficient coverage along anticipated routes to Peninsula Regional Medical Center and to ensure certain critical buildings had coverage within them.

The summary portion of the document titled "Worcester County Coverage Design Testing Results" produced by Harris Corporation is attached for your review. As presented, this document indicates that Harris Corporation has demonstrated meeting contractual requirements for coverage as defined within the system purchase agreement. County staff continues to evaluate this document with support from Federal Engineering.

While this document requires signature by the county to accept the results, it is being provided for information only at this time. Staff believes that it is in the best interest of the county to continue to evaluate the testing results with Federal Engineering prior to execution.

1a



Worcester County Coverage Design
Testing Results

Prepared By:
Oyetunde Daniel Jolaoye
System Engineer
Harris Corporation

Report Generated:
September 14, 2018

Table of Contents

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1. SUMMARY OF RESULTS- 6 Site Test Result (Recently concluded)

Below are tables summarizing the BER, DAQ and In-building results:

Table 1.1: BER results of various county bounded area

Service Area Definition	Body Loss (dB)	Building Loss (dB)	Required BER	% Guarantee/PASS RATE	Verdict
14dB County Boundary	7 dB	14 dB	2.4%	95.0% / 95.04%	Pass
20dB County Boundary	7 dB	20 dB	2.4%	95% / 97.8%	Pass

Table 1.2: DAQ results of various county bounded area

Service Area Definition	Grid Size	Possible Tested Grids	Inaccessible Grids	Tested Grids	Pass Grids	Fail Grids	Pass Rate
14dB Boundary	0.25mi x 0.25mi	2203	740	1463	1437	26	98.2%

1.1 SUMMARY OF RESULTS- 5 Site Test Result (Previously tested)

Table 1.3: BER results of various county bounded area

Service Area Definition	Body Loss (dB)	Building Loss (dB)	Required BER	% Guarantee/PASS RATE	Verdict
6dB County Boundary	7 dB	6dB	2.4%	95% / 95.20%	Pass
14dB County Boundary	7 dB	14 dB	2.4%	93% / 81.5%	Fail
20dB County Boundary	7 dB	20 dB	2.4%	92% / 92.20%	Pass

Table 1.4: DAQ results of various county bounded area

Service Area Definition	Grid Size	Possible Tested Grids	Inaccessible Grids	Tested Grids	Pass Grids	Fail Grids	Pass Rate
The County + 3-Miles into Neighboring Counties (tested at 6dB),	1mi x 1mi	760	46	714	713	1	99.9%
County's Jurisdictional boundary (tested at 6dB)	1 mi x 1 mi	649	33	616	615	1	99.8%
6dB Boundary (supplied by the County)	1mi x 1mi	424	33	390	389	1	99.7%
14dB Boundary (supplied by the County)	0.25mi x 0.25mi	392	47	345	328	17	95.1%
20dB Boundary (supplied by the County)	0.125mi x 0.125mi	1108	154	954	936	18	98.1%

Table 1.3 In-building Results

Critical Building List for testing:	Number of Tiles tested	Pass	Fail	Pass Rate
1. Worcester County Courthouse Snow Hill	60	58	2	96.67%
2. Worcester County Government Center Snow Hill	60	57	3	95.00%
3. Pocomoke High School	24	24	0	100.00%
4. Worcester Technical High School	41	41	0	100.00%
5. Atlantic General Hospital	41	41	0	100.00%
6. Ocean Pines Fire Department North Station	40	40	0	100.00%
7. Bishopville Fire Department	21	21	0	100.00%
8. Pocomoke Police Department	20	20	0	100.00%
9. Snow Hill Middle School	32	32	0	100.00%
10. Ocean City Elementary School	20	20	0	100.00%

2. BIT ERROR RATE (BER) TEST

Automated BER testing was tested by categorizing the county into three grids; 6dB, 14dB and 20dB. The grids were driven while the BER measurements were automatically recorded and calculated. Table 3.2 below shows the contracted guarantee and service area definition for the BER testing. The BER map for each BER zone is shown in this report as well as tables showing the result.

Table 2.1: Guaranteed Coverage Service Area, Signal Level, and Acceptance Criteria

<i>Service Area Definition</i>	<i>Description</i>	<i>Attenuator Body Loss</i>	<i>Attenuator Building Loss</i>	<i>% Validated CPC Service Area Reliability Acceptance Criteria</i>
6dB Bounded Area (provided by the County)	Portable Indoor	7dB	6dB	95/95%%
14dB Bounded Area (provided by the County)	Portable Indoor	7dB	14dB	95/95%%
20dB Bounded Area (provided by the County)	Portable Indoor	7dB	20dB	95/95%%

*Composite attenuator value to be finalized to account for actual test set-up used.

2.1 Results Presentation

The data recorded are plotted on a map showing the test grids, the areas tested and the test results. Different colors are used to show ranges of measured BER. Table 3.2 below shows the test results. The 6dB boundary is shown with red in figure 3.1 below.

6dB Coverage Boundary:

3201 data points were collected
95.2% have a BER not greater than 2.4%
4.8% have a BER greater than 2.4%

Table 2.2 – BER Testing Results

Service Area Definition	Body Loss (dB)	Building Loss (dB)	Required BER	Measured BER	Verdict
6dB County Boundary	7 dB	6 dB	2.4%	95.2%	PASS

Refer to the map below for the areas that have been driven as part of the automated BER testing.

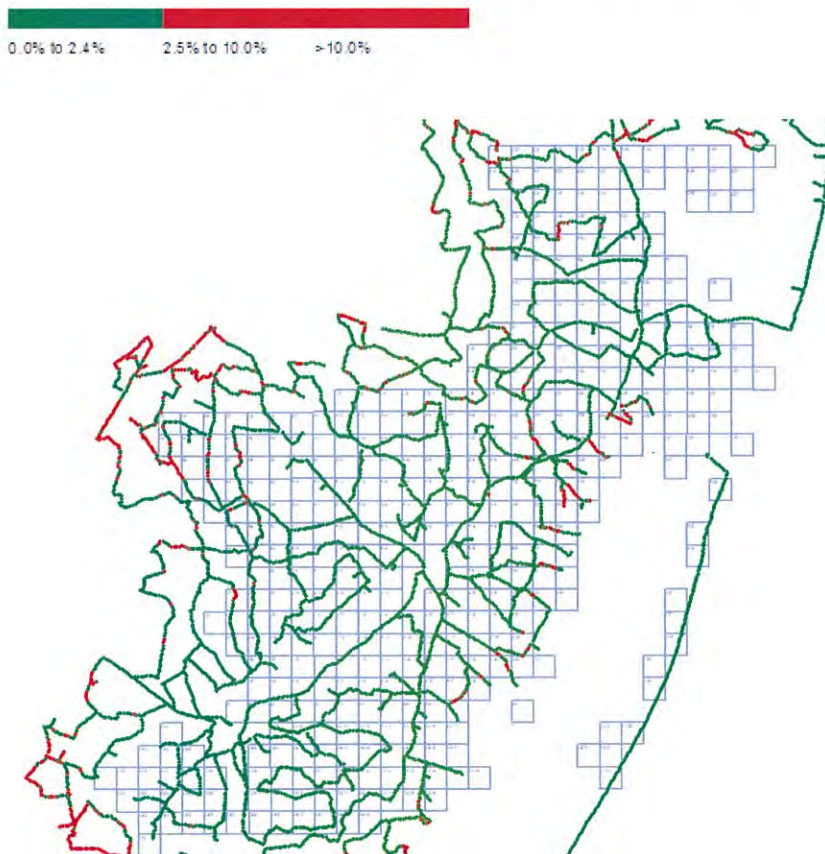


Figure 2.1: 6dB BER drive collected points

14dB Coverage Boundary:

3872 data points were collected
95% have a BER not greater than 2.4%
5% have a BER greater than 2.4%

Table 2.3 – BER Testing Results

Service Area Definition	Body Loss (dB)	Building Loss (dB)	Required BER	Measured BER	Verdict
14dB County Boundary	7 dB	14 dB	2.4%	95.04%	Pass



Figure 2.2: 14dB BER drive collected points

20dB Coverage Boundary:

1023 data points were collected
97.8% have a BER not greater than 2.4%
2.2% have a BER greater than 2.4%

Table 2.4 – BER Testing Results

Service Area Definition	Body Loss (dB)	Building Loss (dB)	Required BER	Measured BER	Verdict
20dB County Boundary	7 dB	20 dB	2.4%	97.8%	PASS



Figure 2.3: 20dB BER drive collected points

3. MANDATORY BUILDING BER TEST, WITH MAXIMUM BUILDING LOSS

The inbuilding BER testing was conducted in 10 critical buildings chosen by the county. Each floor of a building was divided into 20 grids and each grid tested using the automated BER setup.

Table 4.1 below shows the automated in-building BER result for each of the tested building.

3.1 Results Presentation

A test report is provided that includes:

- the name of each mandatory building
- the PASS/FAIL score for each building

Table 3.1 BER results of critical building

Critical Building List for testing:	Number of Tiles tested	Pass	Fail	Pass Rate
1. Worcester County Courthouse Snow Hill	60	58	2	96.67%
2. Worcester County Government Center Snow Hill	60	57	3	95.00%
3. Pocomoke High School	24	24	0	100.00%
4. Worcester Technical High School	41	41	0	100.00%
5. Atlantic General Hospital	41	41	0	100.00%
6. Ocean Pines Fire Department North Station	40	40	0	100.00%
7. Bishopville Fire Department	21	21	0	100.00%
8. Pocomoke Police Department	20	20	0	100.00%
9. Snow Hill Middle School	32	32	0	100.00%
10. Ocean City Elementary School	20	20	0	100.00%

All the selected building passed the required 95% BER rate.

4. INDOOR VOICE QUALITY TEST (DAQ)

The DAQ coverage testing was performed by dividing the county into five zones; County wide + 3 mile, County's jurisdictional boundary, 6dB, 14dB and 20dB. The entire county +3 mile and County's jurisdictional boundary were tested with the 6dB attenuation during the DAQ drive testing. If a grid failed a 6dB DAQ test, the test was repeated with the attenuation changed to 0dB if the grid was outside the 6dB zone. With this setup, only one grid failed the DAQ test using the 6dB attenuation and the failed grid was in a 6dB zone.

Table 5.1 below shows the scale of definition for the DAQ testing. A dispatcher and a Harris representative were stationed at the EOC to grade communication, while a team comprising of a county driver and Harris representative were performing the drive testing and also grading the communication.

Table 5.2 below shows the different grids, size of the grids and attenuation attached to each grid. The number of tested grids at times surpass the number of grids in table 5.2 because grids that were accessible were tested even if they are not labeled on the grid map.

Table 4.1 - Delivered Audio Quality Scale Definitions

Delivered Audio Quality	Subjective Performance Description
DAQ 5.0	Speech easily understood.
DAQ 4.5	Speech easily understood. Infrequent Noise/Distortion.
DAQ 4.0	Speech easily understood. Occasional Noise/Distortion.
DAQ 3.4	Speech understandable with repetition only rarely required. Some Noise/Distortion.
DAQ 3.0	Speech understandable with slight effort. Occasional repetition required due to Noise/Distortion.
DAQ 2.0	Understandable with considerable effort. Frequent repetition due to Noise/Distortion.
DAQ 1.0	Unusable, speech present but unreadable.

Table 4.2: Grid Patterns for Worcester County

Boundary Area:	Grid Size:	Number of Grids:
The County + 3-Miles into Neighboring Counties	1mi x 1mi	258
6dB Boundary (supplied by the County)	1mi x 1mi	424
14dB Boundary (supplied by the County)	0.25mi x 0.25mi	382
20dB Boundary (supplied by the County)	0.125 mi x 0.125	850

Table 4.3 - Coverage Service Area and Acceptance Criteria

<i>Portable Service Area Definition</i>	<i>Description</i>	<i>Attenuator Portable Loss (dB)</i>	<i>Attenuator Building Loss (dB)</i>	<i>% Validated CPC Service Area Reliability Acceptance Criteria 5-site/6-site</i>
The County's jurisdictional boundary + 3-miles into neighboring Counties	Digital Voice	7dB		95/95%
The County's jurisdictional boundary	Digital Voice	7dB		95/95%
6dB indoor boundary (supplied by the County)	Digital Voice	7 dB	6dB	95/95%
14dB indoor boundary (supplied by the County)	Digital Voice	7 dB	14dB	95/95%
20dB indoor boundary (supplied by the County)	Digital Voice	7 dB	20dB	95/95%

*Composite attenuator value to be finalized to account for actual test set-up used.

4.1 Results Presentation

A test report is provided that includes:

- the number of test grids
- the location tested within each grid
- the PASS/FAIL score for each test grid/location for each call direction
- the % PASS calculation for the service area

Green = Passed DAQ 3.4

Blue = Inaccessible grids

Red = Failed grids

The 6dB boundary and County's jurisdictional boundary were all tested together for an efficient drive testing route. The 6dB result shown below is also the result of the county's jurisdictional boundary because the 6dB boundary is county wide.

Table 4.4 – DAQ Testing Results (Previous test)

Refer to the attached results map and test data sheets for the specific grids results.

Service Area Definition	Grid Size	Possible Tested Grids	Inaccessible Grids	Tested Grids	Pass Grids	Fail Grids	Pass Rate
The County + 3-Miles into Neighboring Counties (tested at 6dB)	1mi x 1mi	760	46	714	713	1	99.9%
County's Jurisdictional Boundary (tested at 6dB)	1 mi x 1 mi	649	33	616	615	1	99.8%
6dB Boundary (supplied by the County)	1mi x 1mi	424	33	390	389	1	99.7%
14dB Boundary (supplied by the County)	0.25mi x 0.25mi	392	47	345	328	17	95.1%
20dB Boundary (supplied by the County)	0.125mi x 0.125mi	1108	154	954	936	18	98.1%

Table 4.5 – DAQ Testing Results (recently concluded test)

Service Area Definition	Grid Size	Possible Tested Grids	Inaccessible Grids	Tested Grids	Pass Grids	Fail Grids	Pass Rate
14dB Boundary	0.25mi x 0.25mi	2203	740	1463	1437	26	98.2%

DAQ Grade Report showing the county + 3miles into neighboring county tested at 6dB

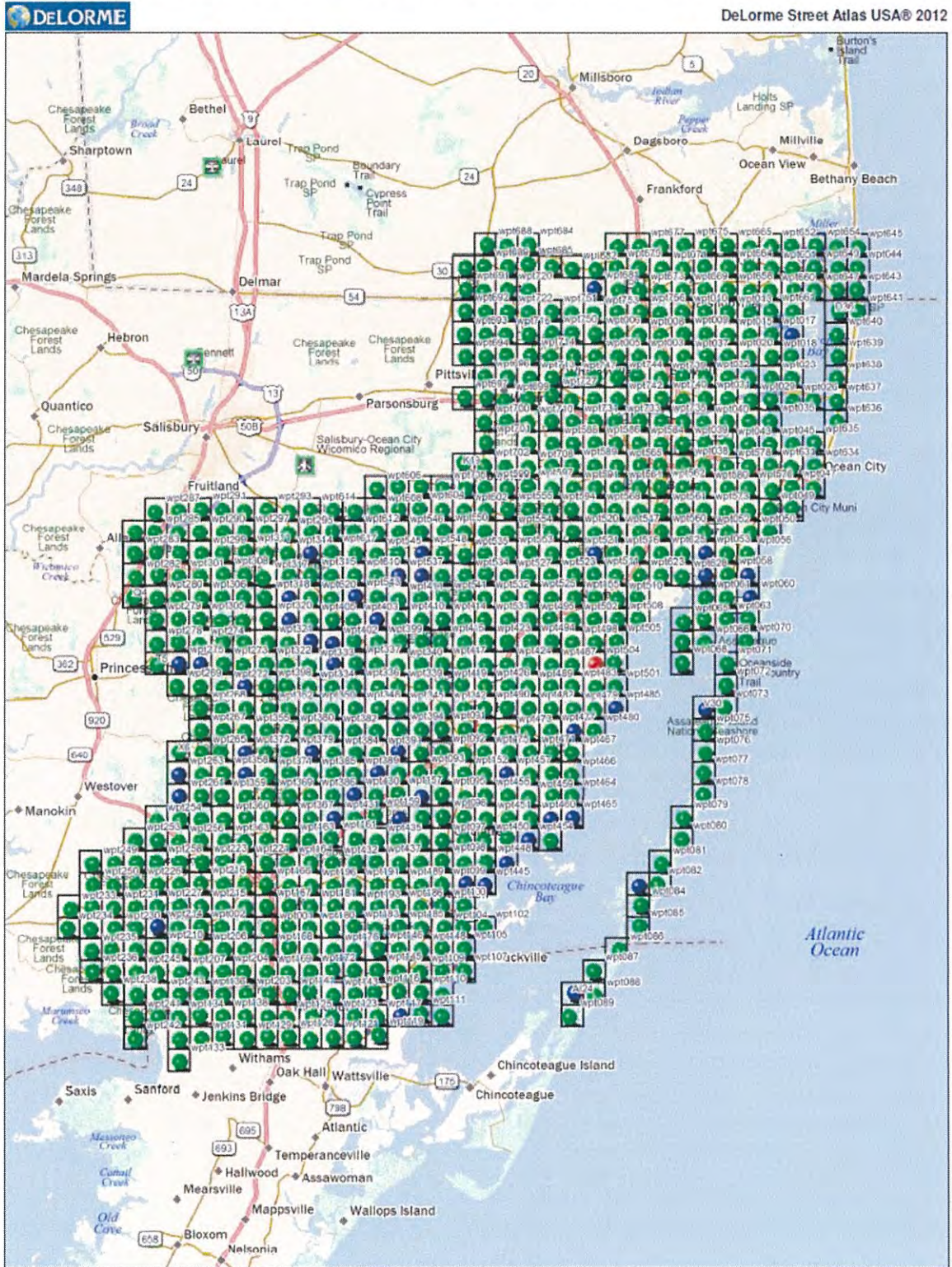


Figure 4.1: County + 3miles into neighboring county DAQ grid test

DAQ Grade Report showing the 6dB county boundary

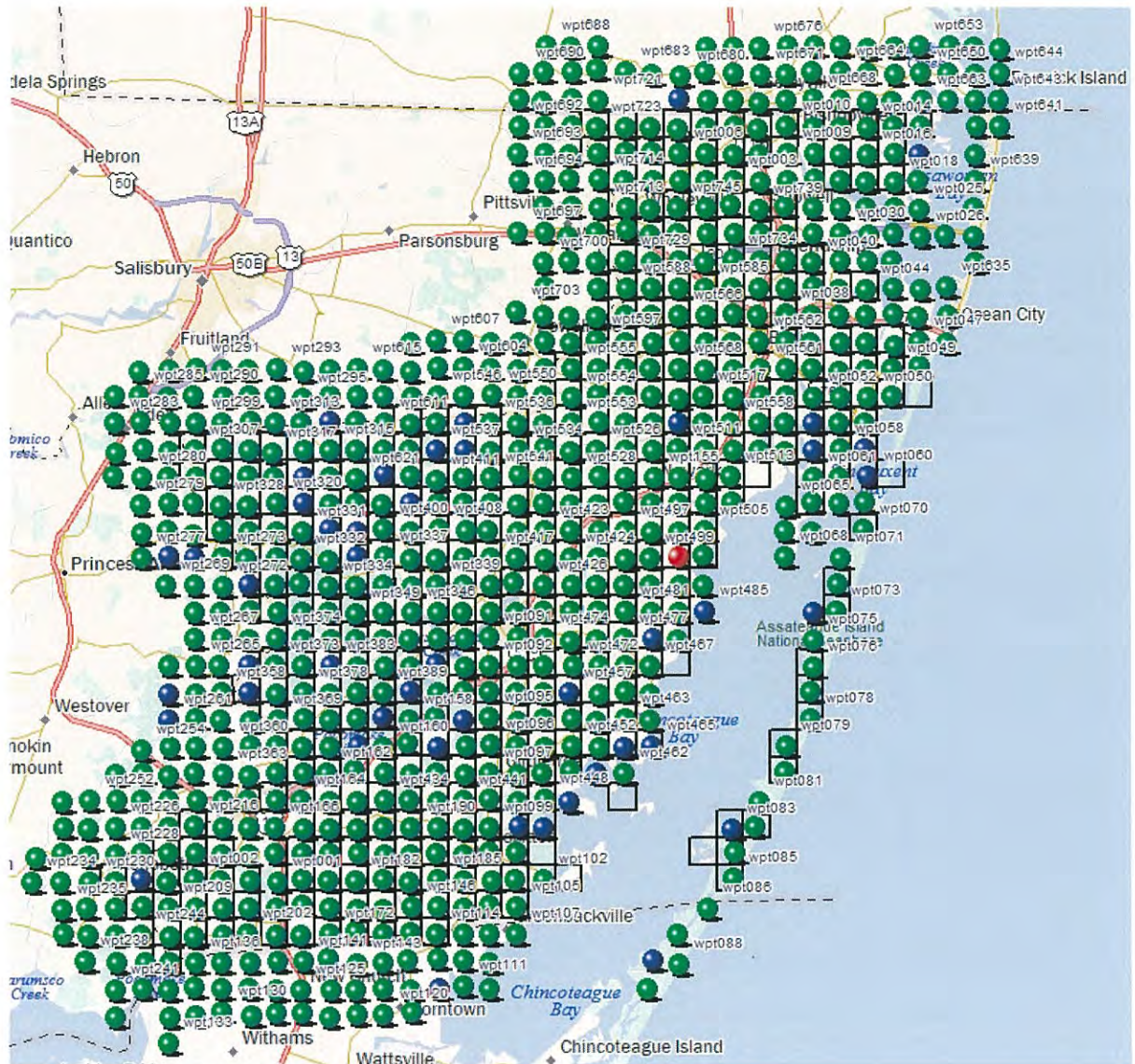


Figure 4.2: 6dB boundary DAQ grid test

14dB DAQ Grade Report



Figure 4.3: 14dB DAQ grid test

Modified 14dB DAQ Report

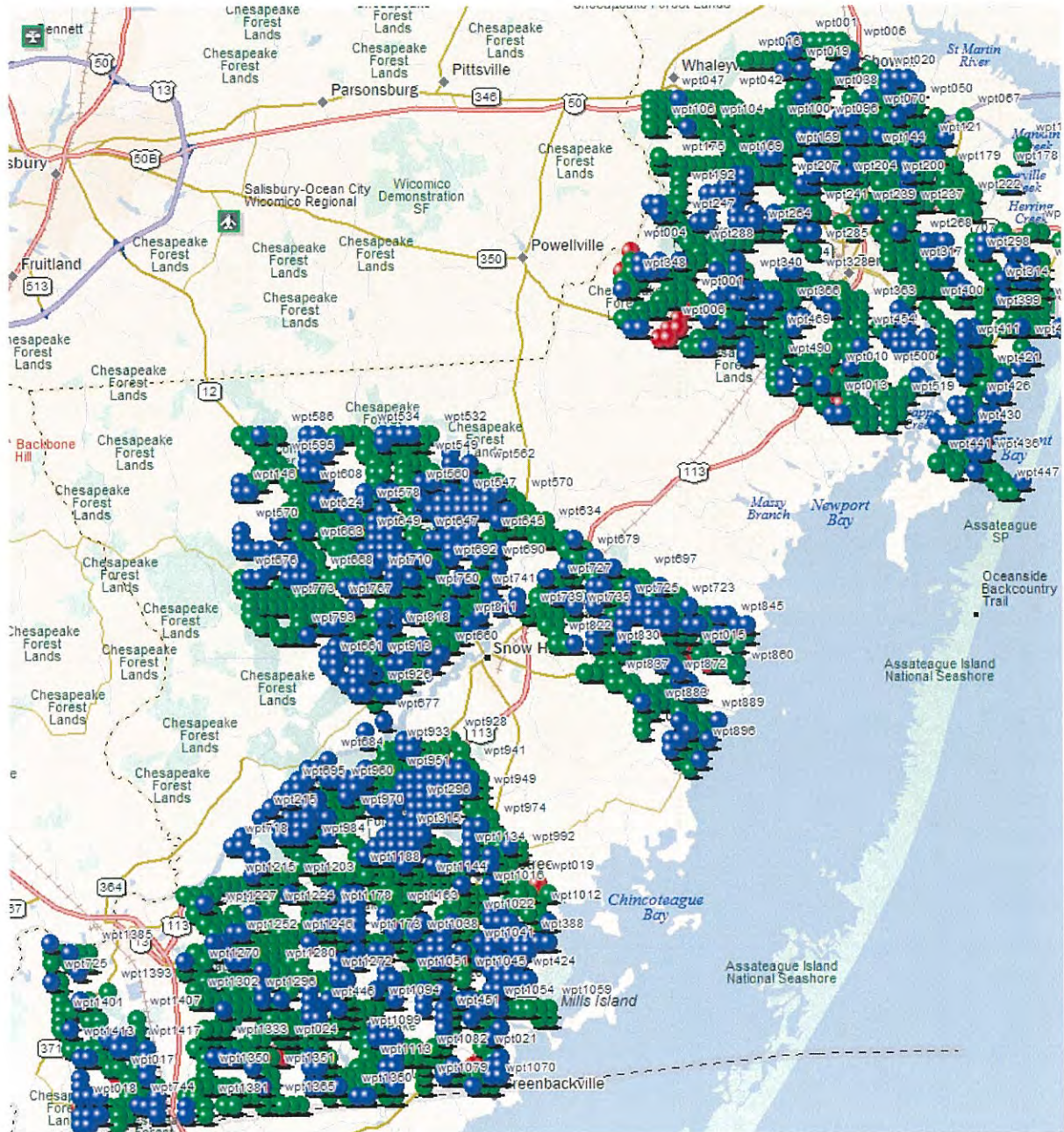


Figure 4.4: 14dB DAQ grid test

20dB DAQ Grade Report



Figure 4.5: 20dB DAQ grid test

5. HOSPITAL ROUTE RSSI TEST

Hospital Route:

A route was mapped from highway 50 to Peninsula Regional Medical Center and then down to Pocomoke. Typhon software was used to collect RSSI values on this route.

The route and results are shown below.

165 total data points were taken of which were no failures, pass rate is 100%

Map type - 1:686,443

-30.0 to -110.0 dBm -111.0 to -140.0 dBm

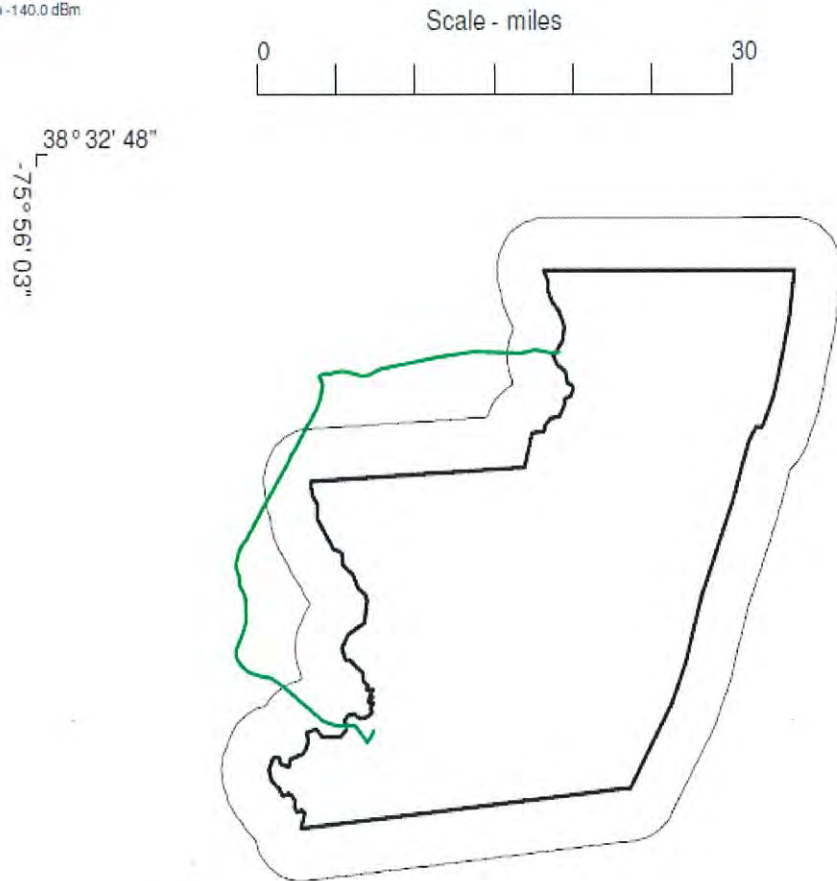


Figure 5.1: Hospital route RSSI result

APPENDIX A. DAQ REPORT SHEET

6dB DAQ Report

GRID	Inbound	Outbound
A20	PASS	PASS
A21	PASS	PASS
A22	PASS	PASS
A26	PASS	PASS
A27	PASS	PASS
A28	PASS	PASS
A29	PASS	PASS
A30	PASS	PASS
A31	PASS	PASS
A32	PASS	PASS
A33	PASS	PASS
A34	PASS	PASS
A35	PASS	PASS
A36	PASS	PASS
A37	PASS	PASS
B19	PASS	PASS
B20	PASS	PASS
B21	PASS	PASS
B22	PASS	PASS
B23	PASS	PASS
B24	PASS	PASS
B25	PASS	PASS
B26	PASS	PASS
B27	PASS	PASS
B28	PASS	PASS
B29	PASS	PASS
B30	PASS	PASS
B31	PASS	PASS
B32	PASS	PASS
B33	PASS	PASS
B34	PASS	PASS
B35	PASS	PASS
B36	PASS	PASS
B37	PASS	PASS
C19	PASS	PASS
C20	PASS	PASS
C21	PASS	PASS
C22	PASS	PASS



5

HAROLD L. HIGGINS, CPA
CHIEF ADMINISTRATIVE OFFICER
MAUREEN F.L. HOWARTH
COUNTY ATTORNEY

COMMISSIONERS
DIANA PURNELL, PRESIDENT
THEODORE J. ELDER, VICE PRESIDENT
ANTHONY W. BERTINO, JR.
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OFFICE OF THE
COUNTY COMMISSIONERS

Worcester County

GOVERNMENT CENTER
ONE WEST MARKET STREET • ROOM 1103
SNOW HILL, MARYLAND
21863-1195

September 26, 2018

TO: Harold Higgins, Chief Administrative Officer
FROM: Kathy Whited, Budget Officer *Kathy*
RE: Fiscal Year 2018 Year End Budget Transfers

The budget transfer request is a component of the General Fund annual audit. We ask the Department and agency heads to wait until the end of the fiscal year to request budget transfers in order to keep the accounting down to one journal entry. The Department heads and Agencies must keep track during the fiscal year of budget accounts that may be over due to unforeseen reasons and then submit their requests for budget transfers to cover these expenditures in their budgets. There is still savings for the County after these transfers have been completed.

I have evaluated the requests and made some adjustments where needed and I concur with the need for all transfers submitted in the attached summary.

I would ask for your review and the County Commissioners approval of the FY2018 Budget Transfer Request worksheets provided which totals \$310,952. Included in this transfer request is \$48,609 (mainly in the Sheriff's office to cover the vehicle bid from 9/4/18) which will be included in the encumbrance request.

Thank you for your consideration and I am available for any questions you may have.

Attachment: FY2018 Budget Transfer Request

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
County Administration					
1	Camera Equipment	100.1001.6110.050	189		camera flash expense greater than budget
2	Mobile Phones	100.1001.6110.245	570		two new phones purchased in FY18
3	Other office equipment	100.1001.6110.290	615		purchase of a new dictation machine
4	Copier Lease	100.1001.6130.010	103		Copy expenses over budget due to use of various color documents
5	Office Supplies	100.1001.6100.190		(1,477)	Office supply budget to cover copy usage, camera equipment, mobile phones and new dictation machine
Elections					
1	Office Rent/Lease	100.1006.6550.170	5,690		New cost of office rent, increased costs of electricity and cleaning office
2	Temporary Clerical Staff	100.1006.6530.150	1,281		Additional contractual staffing needed for June 26, 2018, Primary Election
3	Employee salary & Benefits	100.1006.6000.500	15,525		Increase in health insurance costs
4	Training, Travel & Expense Mileage	100.1006.7000.115	1,011		Costs of mileage, meals and expense related to election training/delivery of materials to polls/state

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FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Elections (continued)					
5	Election Judge Expense	100.1006.6120.060		(23,507)	Election Judge expense savings to cover increase in additional costs for electricity, cleaning and new expense of rent, temporary clerical staff, health insurance mileage
6	Relocation Expenses	100.1006.6700.550	11,734		Costs of relocating office and warehouse
7	Supplies & Equipment Voting machines	100.1006.6110.440		(11,734)	Supplies & Equipment expense savings to cover costs of relocation of office and warehouse
Human Resources					
1	Personnel Advertisements	100.1007.090.6900.040	65		advertising for roads personnel
2	Physical, Shots & Drug Testing	100.1007.090.6530.080		(65)	Move \$65 to cover personnel advertisements

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST				
Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Development Review & Permitting				
1 Copier Supplies	1008.6100.080		(66)	Copier Supply savings to cover Court Reporter costs
2 Court Reporters	1008.6510.050	66		Funds need to Cover Court Reporter charges
3 Paper	1008.6100.210		(615)	Paper savings to cover unexpected legal expenses
4 County Attorney Expenses	1008.6510.020	615		Funds need to cover County Attorney expenses
5 Envelopes	1008.6100.110		(369)	Envelope Savings to cover Vehicle Fuel Expense
6 Vehicle Fuel	1008.6540.020	369		Funds need to cover Fuel Expense
7 Office Supplies	1008.6100.190		(1,194)	Office Supply savings to cover unexpected legal expenses
8 Board Member Allowance	1008.7000.020		(1,832)	Board Member Allowance savings to cover unexpected legal expenses
9 Board/ Commission Attorney	1008.6510.010	3,026		Funds necessary to cover legal expenses

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Development Review & Permitting (continued)					
10	Postage and Freight	1008.6100.230		(80)	Postage and Freight Savings to cover Fire Extinguisher Expense
11	Fire Extinguisher expense	1008.6550.081	80		Funds need to cover Fire Extinguisher Expense
12	Envelopes	1008.6100.110		(680)	Envelope Savings to cover unexpected GIS server replacement
13	Capitol Equipment	1008.9010.060		(6,000)	Capitol Equipment Savings to cover unexpected GIS server replacement
14	Computer Repairs & Supplies	1008.6110.080	6,680		Funds necessary to cover unexpected GIS server replacement
Environmental Programs					
1	Telephone expense	100.1010.6550.270	229		fax line expense for Environmental Programs
2	Other Maint. & Svcs Phone Service	100.1010.6700.050		(229)	remote office internet savings to cover fax line expense

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Sheriff's Office					
1	Personnel Services OT - Kennel	100.1101.030.6000.450	13,300		additional hours over and above 10 hour shift when the handler is not able to incorporate kennel duty within the normal 10 hour shift
2	Uniforms	100.1101.030.6150.050	12,000		funds needed to cover uniforms due to additional replacement hires for Part Time personnel leaving, for Full Time positions and increase in cost of replacement uniforms for normal wear and tear
3	Computers and Printers	100.1101.030.6110.090		(25,300)	Computer and Printer savings to cover Kennel Overtime and Uniforms
4	Vehicle Maintenance expense	100.1101.030.6540.030	52,000		funds needed to cover vehicle maintenance and repairs for replacement engine, transmissions not covered under warranty, body repair for collisions with deer and increased labor costs at fleet maintenance.
5	Law Enforcement equipment	100.1101.030.6110.190		(43,000)	Law enforcement equip savings to cover Vehicle Maintenance
6	Capital New Vehicles	100.1101.030.9010.010	15,174		Transfer savings from Animal Control new vehicles to Sheriff's Office to be encumbered for FY19 purchase of new vehicle approved 9/4/18.

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Sheriff's Office - Animal Control					
7	Supplies and Equip - Spa Neuter	100.1101.040.6110.395		(6,000)	Spa Neuter savings to cover Vehicle Maintenance Sheriff's Office
8	Capital New Vehicles	100.1101.040.9010.010		(15,174)	Transfer savings of new vehicles to Sheriff's Office to be encumbered for FY19 purchase of replacement vehicle approved 9/4/18.
Sheriff's Office - School Safety					
9	Law Enforcement Equipment	100.1101.700.6110.190		(3,000)	Law enforcement Equip savings to cover Vehicle Maintenance for Sheriff's Office
Fire Marshal					
1	Hazmat Supplies & Equipment	100.1104.6110.150	327		Supplies used during Hazmat Incident
2	Printing Expense	100.1104.6100.240		(327)	savings from Printing expenses to cover hazmat supplies
3	Mobile Phone expense	100.1104.6110.245	228		Damaged equipment replacement
5	Radio Supplies	100.1104.6100.320	187		Damaged equipment replacement
4	Dues & Subscriptions	100.1104.6100.100		(415)	Subscription savings to cover mobile phone and radio supply expense
7	Vehicle Fuel	100.1104.6540.020	2,771		Increase in usage and responses

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Fire Marshal (continued)					
9	Vehicle Maintenance	100.1104.6540.030	4,019		Unexpected Vehicle Repairs
11	Vehicle Equipment	100.1104.6540.060	1,122		Equipment not transferable from previous vehicle
8	Travel, Training & Expense Hazmat Training	100.1104.7000.010		(7,912)	Program savings for non-employee Hazmat personnel to cover unexpected vehicle repairs, equipment transfers and fuel needed for FY18 responses.
Maintenance					
1	Dues and Subscriptions	100.1201.6100.100	260		Funds needed for Electrician's & HVAC Licenses
2	Uniforms	100.1201.6150.050		(305)	Uniform savings to cover employee license and drug testing expense
3	Physicals, Shots & Drug Testing	100.1201.6530.080	45		Funds needed for post accident drug testing
4	Computers & Printers	100.1201.6110.090	6,109		Funds to be encumbered to replace tablets with laptop computers to support Building Automation Systems
5	Vehicle Fuel	100.1201.6540.020		(8,109)	Fuel savings to cover laptop purchases and Public Works Building Security System Upgrade
6	Building General Maintenance Repairs	100.1201.6550.090	2,500		Funds to be encumbered for Public Works Building-Security System Upgrade
7	Radio Supplies and repairs	100.1201.6110.320		(500)	Savings in radio repair and supply for Public Works Building-Security System Upgrade

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation	
Roads					
1 Capital Equipment - New Vehicles	100.1202.9010.010		(71,798)	savings from vehicle purchases to help cover cost associated with the Meadowbridge Bridge Replacement	
2 Capital Equipment - Heavy Equipment	100.1202.9010.070		(57,049)	savings from equipment purchases to help cover cost associated with the Meadowbridge Bridge Replacement	
3 Road Maintenance - State Aid Bridges	100.1202.6600.030	128,847		funds needed to help cover cost associated with the Meadowbridge Bridge Replacement	
Public Works - Fleet Division					
1 Fleet Uniforms	100.1203.220.6150.050	122		Funds needed to cover a new employee mid budget year	
2 Fleet Tools and Supplies	100.1203.220.6110.420		(122)	Supplies and equipment tool savings to cover new employee uniforms.	

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Mosquito Control					
1	Tools & Supplies	100.1302.6110.420	683		Funds needed to cover tools needed for Argo amphibious vehicle
3	Copier Lease	100.1302.6130.010	9		Funds needed for replaced copier component
5	Mosquito Control expense	100.1302.6700.350	14		budget to cover expense for repairs to program participant lawn
2	Building Site General Maintenance Repairs	100.1302.6550.090		(706)	Building repair savings to cover tooling expense and repairs
Recreation - Administration					
1	Bank Fees	100.1601.400.6100.052	448		Bank Fees for Credit Card Charges
2	Dues & Subscriptions	100.1601.400.6100.100	2,254		D3 Corp Email Marking System and Beachfest bid fee
3	Office Supplies	100.1601.400.6100.190	684		Office Supplies expenses for additional programs
4	Concession Stand	100.1601.400.6110.100	465		Cover cost of new pizza oven
5	Uniforms	100.1601.400.6150.050	334		Uniform shirts from Ruddos Golf
6	County Attorney Expenses	100.1601.400.6510.020	269		budget needed to cover county attorney legal

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Recreation - Administration (continued)					
7	Fuel	100.1601.400.6540.020		(269)	fuel savings to cover County attorney legal fees
8	Consulting Web Page	100.1601.400.6530.180	23		IT-New Web Page
9	Continuing Education/Certifications	100.1601.400.7000.040		(1,457)	Funds needed to pay for credit card charges, office supplies and uniform shirts
10	Meetings/Conferences/shows	100.1601.400.7000.100		(2,751)	Funds needed for bid fees for Beach Fest Volleyball tournament, and new pizza oven
Recreation - Recreation Programs					
11	Printing Expense	100.1601.410.6100.240	72		Xerox Print Charges above lease contract
12	Advertising	100.1601.410.6900.050	3,005		Advertising exceeded budget due to marketing new recreation programs
13	Amusement Park Tickets	100.1601.410.6175.040		(507)	Program savings to help cover cost of marketing
14	Summer Camps for Youth	100.1601.410.6175.080		(528)	Program savings to help cover cost of marketing
15	Youth Recreational Outdoor Programs	100.1601.410.6175.090		(1,189)	Program savings to help cover cost of marketing
16	Adult Recreational Programs	100.1601.410.6175.201		(853)	Program savings to help cover cost of marketing

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Parks					
1	County attorney	100.1602.500.6510.020	15		county attorney fees
2	Envelopes	100.1602.500.6100.110		(15)	savings in account to cover county attorney
3	Uniforms	100.1602.500.6150.050	36		additional uniform item purchased
4	Office Supplies	100.1602.500.6100.190		(36)	office supply savings to cover uniform
5	Lawn equipment & maintenance	100.1602.500.6110.200	1,840		lawn mower repairs & supplies due to wear and tear
6	Continuing education/certification	100.1602.500.7000.040	110		Additional budget needed to send all Parks staff to Pest Conference
7	Special events	100.1602.500.6700.640		(300)	savings to cover employee Pest Conference and lawn equipment maintenance & repairs
8	Showell Park materials	100.1602.520.6200.020		(636)	Park Materials savings to cover lawn mower & equipment repairs
9	Newtown Park materials	100.1602.530.6200.020		(576)	Park Materials savings to cover lawn mower & equipment repairs
10	NWAC park materials	100.1602.540.6200.020		(275)	Park Materials savings to cover lawn mower & equipment repairs

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation	
Parks (continued)					
11 Satellite Parks materials	100.1602.590.6200.020			(163)	Park materials savings to cover lawn mower & equipment repairs
12 NWAC Park equipment maint/repairs	100.1602.540.6130.045	530			water filtration system upgrade
13 Heavy equipment	100.1602.500.9010.070			(530)	savings in new equipment to cover NWAC park water filtration upgrades
Libraries					
1 Library Shared Computer System	1603.200.6130.030			(6,845)	Eastern Shore Regional Library paid for the first year of the new share catalog system - "Polaris" which helped cover the additional costs of library materials and the youth services vehicle.
2 Library Books	1603.200.6110.230	3,322			Purchased additional materials for the new Berlin branch
3 Library Periodicals	1603.200.6110.240	1,438			Cost of newspaper and magazines continue to increase
4 Fuel expense	1603.200.6540.020	337			Library was fortunate to be given a vehicle previously used by the County Liquor Control Board for the Youth Services Manager, in September 2017 (after budget was submitted)
5 Vehicle Repairs Outside	1603.200.6540.045	1,748			Library was fortunate to a vehicle previously used by the County Liquor Control Board for the Youth Services Manager, in September 2017 (after budget was submitted)

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST				
Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Economic Development				
1 Incentives & Events	100.1801.6100.150	2,843		Funds needed to cover additional events attended by Director and Deputy Director
2 Advertising	100.1801.6900.015	1,642		Funds needed to cover additional advertising expenses for FY18
3 Telephone expenses	100.1801.6550.270	182		Funds needed to cover Verizon telephone expense
4 Consulting Services	100.1801.6530.040		(4,545)	Consulting Services savings to cover Incentives & Events expenses, County Attorney services and advertising
5 County Attorney Expenses	100.1801.6510.020	60		Funds needed to cover County Attorney services
6 Mobile Phones expense	100.1801.6110.245		(182)	Mobile phone savings to cover Verizon telephone expenses

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST				
Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Tourism - Administration				
1 office supplies	1803.200.6110.190		(766)	savings in office supplies budget to cover fuel
2 Vehicle expense fuel	1803.200.6540.020	766		fuel expense greater than budgeted
3 promotional materials	1803.200.6110.310	550		funds needed to cover economic impact study
4 Supplemental advertising	1803.200.6900.052		(550)	transfer budget to cover economic impact study budget shortfall
5 mobile phones expense	1803.200.6110.245	57		funds needed to cover account for new phones
6 Vehicle maintenance	1803.200.6540.030	143		funds needed to cover maintenance on Tourism van
7 telephone expense	1803.200.6550.270		(200)	budget savings to cover phones and vehicle maintenance

FY2018 GENERAL FUND BUDGET TRANSFER REQUEST					
	Account Name	NWS Acct No. (include location if available)	Budget \$ Add	Budget \$ Subtract	Explanation
Tourism - Welcome Center					
9	Signage	1803.420.6110.380	106		budget to cover signage at welcome center
10	Uniforms	1803.420.6150.050	108		to cover actual cost of staff uniforms
11	Admin expense	1803.420.6100.010		(14)	budget savings to cover welcome center expenses
12	travel training expense	1803.420.7000.100		(200)	budget savings to cover welcome center expenses
TOTAL TRANSFER REQUEST			<u>310,952</u>	<u>(310,952)</u>	
Approval Signature					

TEL: 410-632-1194
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E-MAIL: admin@co.worcester.md.us
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HAROLD L. HIGGINS, CPA
CHIEF ADMINISTRATIVE OFFICER
MAUREEN F.L. HOWARTH
COUNTY ATTORNEY

COMMISSIONERS
DIANA PURNELL, PRESIDENT
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SNOW HILL, MARYLAND

21863-1195

September 26, 2018

TO: Harold Higgins, Chief Administrative Officer
FROM: Kathy Whited, Budget Officer *Kathy*
SUBJECT: Fiscal Year End Reserve for Assigned Encumbrances

The Reserve for Encumbrances is required as part of the General Fund annual audit, which would set aside or commit funds for contracts and purchase orders that will be fulfilled in a subsequent fiscal period. I have reviewed the requests from each of the Department and Agency heads for assigned encumbrances and made adjustments where needed.

Attached for your review and the County Commissioners approval is the FY2018 encumbrance request detail. Should you have any questions, do not hesitate to contact me.

The total encumbrance requested is \$1,726,631.

Attachments

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FY2018 BUDGET ENCUMBRANCE REQUEST

As of September 26, 2018

PART A: ENCUMBRANCE REQUEST-CURRENTLY APPROVED PROJECTS (UNCOMPLETED AT 6/30/18):

		General Ledger Account No.	Project Description	Remaining funds to Encumber as of 6/30/18	Meeting Date Approved by Commissioners
1	Circuit Court	100.1002.6160.040	Grant programs- Court security grant	\$ 99,613	6/20/2017
2	Elections	100.1006.6110.440	4th Qtr. Election Equipment Transportation, staffing and auditing - State Board of Elections allocation	13,998	8/21/2018
3	Sheriff's Office	100.1101.030.9010.010	Patrol vehicle totaled 7/25/18 and new vehicle to be purchased with FY18 remaining budget.	40,000	9/4/2018
4	Natural Resources	100.1702.7180	Natural Resources Code Renovations	20,000	8/21/2018
5	Natural Resources	100.1702.7180	MD Agricultural Land Preservation Fund matching to retain certification FY11, FY13, FY16, FY17	27,982	8/21/2018
6	Natural Resources	100.1702.7180	MD Agricultural Land Preservation Fund matching to retain certification FY18	9,242	8/21/2018
7	Library	100.1603.200.9010.220	Phone system project, Bank street office needs new data wiring plus incidentals	10,000	6/25/2018
8	Emergency Services	100.1090.020.9010.180	Skyline Fiber Project	119,188	3/17/2017
9	Emergency Services	100.1102.044.6110.320	Fire Apparatus capital to be completed after radio project	40,000	7/1/2017
Sub-Total Part A:				\$ 380,023	

		General Ledger Account No.	Project Description	Encumbrance \$ Request as of 6/30/18	Agency
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PART B: ENCUMBRANCE REQUEST- GRANT FUNDS APPROPRIATED BUT UNOBLIGATED:

1	Roads	100.1202.6160.145	SHA remaining FY18 Transportation Grant for Bayside Bridge Replacement	\$ 7,767	State Highway Administration
2	Parks	100.1602.510.7180	Grant Program POS - Field Lighting JWS	538,869	Program Open Space
3	Parks	100.1602.517.7180	Trail Development Greys Creek	8,944	CREP Grant Deferred Revenue
4	Boat Landings	100.1204.265.6160.181	Waterway Improvement	23,583	Maryland DNR Waterway Improvement
5	Boat Landings	100.1204.240.7180	Worcester County Boat Landing Facilities	14,515	Maryland DNR Waterway Improvement
6	Boat Landings	100.1204.265.7180	Public Landing Improvements	80,000	Maryland DNR Waterway Improvement
Sub-Total Part B:				\$ 673,678	

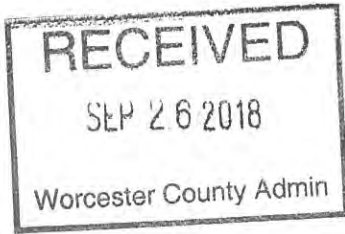
FY2018 BUDGET ENCUMBRANCE REQUEST

As of September 26, 2018

PART C: ENCUMBRANCE REQUEST- OTHER (REQUEST FOR ENCUMBRANCE DOES NOT FIT CATEGORY I OR II ABOVE)

	General Ledger Account No.	Project Description	Encumbrance \$ Request as of 6/30/18	Detailed Description
1	Elections 100.1006.6110.440	Voting units supplies and equipment	\$ 184,853	FY18 invoice due to State
2	Elections 100.1006.6700.550	Cards Technology Wiring 201 Belt street relocation	5,020	relocation invoice received for wiring
3	Public Works Maintenance Division 100.1201.6110.090	Computer upgrade needed for BAS	6,709	Replace tablets with laptops for use with the Building Automation Systems & obsolete phone accessories
4	Public Works Maintenance Division 100.1201.6550.090	Public Works building security system	2,500	Replacement of outdated security system & digital video recorder
5	Public Works Roads Division 100.1202.6110.400	Road striping paint & supplies	22,538	Remaining FY18 striping invoices
6	Public Works Roads Division 100.1202.6540.045	Vehicle operating expense - outside repairs	6,488	Increased volume of vehicles/equipment being sent out for repairs
7	Health Department 100.1201.200.7130.020	Telephone upgrade	45,159	Current Phones are obsolete, out of warranty and failing at significant rates since purchased in 2007 remodel. Project includes 175 new telephones, update 6 network switches and phone licenses.
8	Public Works Mosquito Control 100.1300.9010.050	Replacement office trailer	27,783	FY19 budget allocation exceeds estimated costs
9	Recreation Center 100.1601.400.6550.020	Roof repair project at Recreation Center	7,000	preliminary work done but not completed
10	Natural Resources 100.1702.7180	MALPF match FY08 Rural Legacy program match FY08 funds	160,000	FY08 budget remaining matching funds
11	Natural Resources 100.1702.7180	Environmental Conservation Services - Rural Legacy Program match FY09 funds	200,000	FY09 budget remaining matching funds
12	Boat Landings 100.1204.265.6550.090	Public Landing pier wiring	2,000	County Maintenance to replace wiring for 8 pier lights which illuminate the 300' pier and is in need of replacement
13	Boat Landings 100.1204.280.6550.090	West Ocean City breaker box replacement	2,880	Available budget funds remaining will cover cost to replace 4-5 boxes at estimated \$600-\$750 per box.
Sub-Total Part C:			\$ 672,930	
Total All Parts A-C			\$ 1,726,631	

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DEPARTMENT OF
DEVELOPMENT REVIEW AND PERMITTING

Worcester County

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ZONING DIVISION
BUILDING DIVISION
DATA RESEARCH DIVISION

ADMINISTRATIVE DIVISION
CUSTOMER SERVICE DIVISION
TECHNICAL SERVICE DIVISION

To: Harold Higgins, Chief Administrative Officer
From: Edward A. Tudor, Director, DRP *EAT*
Date: September 25, 2018
Re: Tax Map 9, Parcel 194, 10646 Bishopville Road

This memorandum is in follow-up to our recent conversation regarding the above referenced property which is commonly known as the old Bishopville movie theater. As you know this particular property has been the source of numerous complaints for the last several years. Most recently the complaints center on the presence of rats on the property as a result of the accumulated personal property. Specifically, you asked for my opinion with regard to what options may be available to the County Commissioners to effectuate a more permanent solution to the conditions on this property. I have conferred with County Attorney Maureen Howarth in the preparation of this memorandum.

The area of the property that is not occupied by the remnants of the old theater is entirely filled with a variety of personal property. Section PH 1-101(a)(4) address this issue. The code prohibits the outdoor accumulation of personal property occupying an area of greater than one hundred square feet per lot unless it is visually screened from the public road and adjoining properties or contained wholly within an enclosed structure or building. This property is over 100 sq. ft. While the suitability of the screening the owner has provided has been questioned in the past the owner has generally repaired the screen when requested. Nonetheless, this does nothing to address the alleged presence of rats on the property. Repairing gaps in a fence or raising its height will do nothing to address the vermin issue. For this reason those complaints have been investigated in the past by the Health Department.

Conditions regarding the presence of rats, mice or other vermin are addressed in Section PH1-101(a)(3) of the code. This section prohibits the accumulation of rubbish, household trash or junk threatening to cause a fire hazard, or causing the inhabitation of rats, mice, snakes or vermin or the accumulation of stagnant water causing or threatening to cause the breeding of insects which is or may be dangerous or prejudicial to the public health. To the best of my knowledge

the Health Department has been unable to make a determination in the past that the accumulated materials met this test. Even as I prepared this memo I received the latest inspection report from the Health Department prepared after their inspection this afternoon that states they found no evidence of rats nor could they find a food source for them. I have attached herewith a copy of their latest report and photographs taken just hours ago.

I am also aware that Environmental programs Director Bob Mitchell contacted the property owner regarding the accumulated materials potential impact on the sewerage system. Mr. Mitchell asked for permission to inspect the property to determine impacts on the system but was not granted access and has not heard from the property owner. In his latest email Mr. Mitchell says that he also sent a complaint to the Maryland Department of the Environment's (MDE) compliance division for assistance. I am not aware of any action on the part of MDE. In light of all the aforementioned information and after my discussion with the County Attorney I believe the only alternative for a more permanent solution would be for the County Commissioners to make a determination with regard to the provisions of Section PH1-101(a)(14). This is basically the catch-all provision and states: "Such other similar conditions as the County Commissioners may determine to be prejudicial or dangerous to the health or safety of the people of the County or any of the above or similar conditions as may be determined by the County Commissioners to be prejudicial to property values in the County." I caution however that this determination not be taken lightly. Without some backup from the Health Department making a finding that the property is dangerous to public health could be held to be arbitrary should we be challenged. I have the same concern with regard to being detrimental to property values. While anecdotally this may appear to be true we have no hard evidence to that effect and there are certainly others in the immediate area and all over the county for which the same claim could be made. I will leave the determination of a legally defensible determination in that regard to the County Attorney.

Additionally I fully anticipate that any nuisance abatement order to the property owner from the County Commissioners will result in a request for a hearing. No doubt this issue will be raised with regard to the dilapidated owner occupied house just a few hundred yards south on Saint Martin's Neck Road. In that case are we prepared to remove the family from their home and then raze the structure or have the County act as a general contractor to repair it? I am prepared to carry out whatever order the County Commissioners may determine appropriate in this case but I am concerned however with our ability to manage multiple complex abatements should there be a proliferation of complaints that other structures or properties are detrimental to property values in the County.

As always I will be available to discuss the matter in greater detail with you at your convenience.

Worcester County Complaint Intake Form

Log/Case #: 1,404

Date Received: 09/24/2018

Complaint Type: Rubbish, trash, junk, rats

Received By: BDORSCH

Complaint

The Commissioners have been getting complaints about rats on the property above.

Complainant Information

Complainant: Worcester County Commissioners

Address:

Phone:

Establishment Information

10646 Bishopville Rd
Bishopville, MD 21813

Environmental Health Information

EHS Assigned: Possident, Thomas

Date Assigned: 09/24/2018

Status: Closed

Results: Illness Resulted Valid Complaint Citations Issued

Investigation Record

First Investigation Date: 09/25/2018

09/25/2018 (151) Nuisance Complaint Investigation

1:10 pm Bart Dorsch and Tom Possident conducted a site visit. No household trash was found from a common view inspection of the property. The property was posted with a "No Trespassing" sign and no one answered the door. Overall the property appears to have been cleaned up around the cars compared to the last nuisance complaint investigation completed fall of 2016. We did not see any evidence of rats nor could we find a food source.

Action Taken:

Date Notified: - ,

Completion Date: 09/25/2018

Title PH1

HEALTH-RELATED NUISANCES

SUBTITLE I Environmental Health Hazards

§ PH 1-101. Nuisances.

§ PH 1-102. Abatement of nuisances.

§ PH 1-103. Tattoo establishments.

§ PH 1-104. Junk vehicles.

§ PH 1-105. Smoking in public buildings.

§ PH 1-106. Litter.

§ PH 1-107. Skin penetrating body
adornment.

§ PH 1-108. Nightclubs.

§ PH 1-109. Adult-oriented businesses,
entertainment, and material.

[HISTORY: Adopted by the Board of County Commissioners of Worcester County 8-25-1981 by Bill No. 81-5 as Title 1 of the Public Health Article of the 1981 Code. Amendments noted where applicable.]

SUBTITLE I Environmental Health Hazards

§ PH 1-101. Nuisances. [Amended 11-10-1987 by Bill No. 87-5; 4-25-1989 by Bill No. 89-2]

(a) Certain conditions to be declared nuisances. The existence of any of the following conditions in the County which are found to be dangerous or prejudicial to the maintenance of property values, health, safety or general welfare of the people of the County by the duly designated County department or official are hereby declared to constitute a public nuisance: [Amended 11-16-2004 by Bill No. 04-11]

(1) The uncontrolled growth of grass, weeds or other rank vegetation, including but not limited to ragweed, poison ivy, poison oak, poison sumac, and all other noxious weeds which are generally known to be either allergenic, a skin irritant, or toxic when ingested, to a height exceeding one foot. [Amended 5-18-2010 by Bill No. 10-4]

A. The above requirement shall not apply where the Department has determined, after an investigation which considers the physical characteristics or actual use of the property or other relevant factors, that the property qualifies as one of the following: properties utilized for a bona fide agricultural purpose, natural wooded areas, stream protection areas, habitat protection areas, steep slope and erodible soil protection areas, stormwater management facilities areas, unimproved areas of more than three acres in size, areas publicly owned and maintained as natural areas, and private open space areas covenanted with the County as recreational areas to be maintained in their natural state. In no case shall noxious weeds as described above be allowed to grow within sixty feet of any property line adjoining an occupied property zoned for residential, commercial or industrial purposes. In addition, the

above requirement shall not apply to wetlands, stream protection areas, habitat protection areas, steep slope and erodible soils protection areas, stormwater management facilities areas and nature study areas. Any uncontrolled growth as described in and subject to the provisions of this section shall be cut to a height not exceeding four inches.

- B. Where it is ascertained that the owner, occupant or person in control of any lot or lands within the County has allowed or maintained on such lot or lands any growth of weeds or other rank vegetation to a height over one foot or that noxious weeds, as defined herein, are growing on lands within the County, written notice shall be served upon the owner, lessee, agent, or tenant having charge of any lot or lands within the County that weeds or other rank vegetation have been allowed to grow to a height exceeding one foot and that such weeds or other vegetation must be cut to a height not exceeding three inches. If the owner or other person having charge of such lands is a nonresident, notice shall be sent by regular United States mail to his address as shown on the tax assessment rolls as maintained by the Maryland Department of Assessment and Taxation. Mailing by regular United States mail shall constitute adequate notice. In addition such notice shall be posted on the lot or lands not less than fifteen days prior to taking any further action and shall contain information describing the nature of the violation, the anticipated corrective action, and whom to contact for further information. If the address of any owner or person having charge of such lot or lands cannot be located after diligent search, posting of such notice on the lot or land shall constitute adequate notice.

- (2) Any accumulation of animal or vegetable matter or manure that is offensive by virtue of odors or vapors or by the inhabitation therein of rats, mice, snakes or vermin of any kind which is or may be dangerous or prejudicial to the public health.

- A. The provisions of this section pertaining to manure shall not apply to legitimate agricultural land use unless said use is immediately adjacent to a residential structure on another lot. In such cases, manure cannot be stored within one hundred feet of the residential structure.

- (3) Any placing, leaving, dumping or accumulation of rubbish, household trash or junk causing or threatening to cause a fire hazard, or causing the inhabitation therein of rats, mice, snakes, or vermin of any kind or the accumulation of stagnant water causing or threatening to cause the breeding of insects which is or may be dangerous or prejudicial to the public health.

- (4) Other than as provided in Subsections (a)(4)A and B below, the outdoor storage or accumulation of personal property occupying greater than one hundred square feet of land area per parcel or lot, including but not limited to the following: appliances, appliance parts, furniture, linens, household goods, lawn mowers, auto, truck, boat, recreational vehicle, motorcycle or bicycle parts, scrap metal, glass, scrap paper, bicycles, wire, electrical or plumbing parts and fixtures, tools, building

- supplies and materials not in storage for existing permitted construction activity on the site.
- A. When the storage or accumulation of personal property as described in Subsection (a)(4) above is visually screened from adjoining public road rights-of-way and adjoining properties or contained wholly within a completely enclosed structure, the storage or accumulation of personal property may occupy greater than one hundred square feet of land area per parcel or lot.
- B. The provisions of this subsection shall not apply to properties utilized for bona fide agricultural purposes.
- (5) The deposit or accumulation of any foul, decaying or putrescent substance or garbage, trash, rubbish or other offensive matter upon the ground surface or in or upon any groundwater, abandoned well, sewage system, bathing area, lake, pond, watercourse, ditch, drain, gutter or tidewater, hole or pit.
- (6) The overflow of any foul liquids or sewage or the escape of any sewage or sewage gas from any privy, cesspool, septic tank, subsurface tile field or any other type of sewage system which is not connected to a municipal sewage system; or any open cesspool or unsafe sewage system. **[Amended 7-26-2005 by Bill No. 05-8]**
- (7) A toilet or urinal in any public or quasi-public building which is maintained in an unsanitary condition.
- (8) The accumulation or deposit of manure, human feces, garbage, cannery wastes or by-products, feathers and poultry offal, carcasses of animals or any form of filth.
- (9) A polluted or unsafe water system, well or spring or the pollution of any well or spring. **[Amended 7-26-2005 by Bill No. 05-8]**
- (10) Any premises having an unsafe sewerage system or facility, or that is not provided with a suitable toilet or sanitary privy for all persons gathering, working or living therein. **[Amended 7-26-2005 by Bill No. 05-8]**
- (11) Any dilapidated, burned-out, fallen-down, ramshackled or decayed structure or remnant thereof which is unattended and uninhabitable or unusable for its intended purpose and is beyond reasonable hope of rehabilitation or restoration. The Commissioners, in making a determination of a nuisance condition under this subsection, shall consider the historical significance of the structure and its danger or potential danger to the public.
- (12) Any unattended and unprotected man-made hole, cave, crater, cavity, pit or pool or similar surface condition which constitutes or has the potential of becoming a hazardous area to the public because of potential for cave-in, subsidence or collapse or because of an accumulation of water.
- (13) The disposition of any animal carcass upon the surface of any land, road or highway.

(14) Such other similar conditions as the County Commissioners may determine to be prejudicial or dangerous to the health or safety of the people of the County or any of the above or similar conditions as may be determined by the County Commissioners to be prejudicial to property values in the County.

(b) Procedure for determination of nuisance.

- (1) The County Commissioners shall, by resolution, designate County departments or officials to investigate, determine the existence of and issue citations for nuisances.
- (2) The County Commissioners or any department or official designated to enforce this Subtitle may require that nuisance complaints be in writing, signed by the complainant and contain such information as may be necessary to locate and investigate the condition.
- (3) No complaint shall be necessary to institute the investigation of a nuisance.
- (4) Departments and officials designated to enforce this Subtitle shall cooperate with each other in sharing information and making investigations.
- (5) The investigating department or official shall, after investigation, determine whether or not a nuisance exists.

(c) Violations.

- (1) Anyone permitting or maintaining a nuisance as determined hereunder shall be guilty of a civil infraction.
- (2) Each day that a nuisance is permitted or maintained shall constitute a separate infraction.
- (3) A property owner of property on which a nuisance exists, as well as the person causing the nuisance, shall be guilty of such civil infraction.
- (4) Nothing in this Subtitle shall prohibit the abatement of a nuisance under any other legal procedure or relieve a person charged with a civil infraction hereunder from liability under any other civil or criminal enactment.

- (d) Notice. In addition to the penalties contained in Subsection (c) hereof, where it has been determined pursuant to Subsection (b)(5) hereof that a nuisance exists, the Department or official making such determination shall cause written notice to be sent to the property owner, as well as the occupant or other person in possession of the property in question, said notice to describe the nature of the nuisance and the actions necessary for correction. Such notice shall be sent by registered mail to the owners address as identified on the tax assessment rolls as maintained by the Maryland Department of Assessment and Taxation. If the address of any owner or person having charge of such lot or lands which is the subject of the nuisance cannot be located after diligent search or if the aforementioned notice by registered mail is not accepted or otherwise not deliverable, posting of such notice on the lot or land shall constitute adequate notice. Such notice shall be posted on the lot or lands not less than seven days prior to taking any further action and shall contain information describing the nature of the violation, the required corrective action,

and whom to contact for further information. [Added 11-16-2004 by Bill No. 04-11;¹ amended 5-18-2010 by Bill No. 10-4]

(e) Applicability.

- (1) This Subtitle shall apply only in the unincorporated areas of Worcester County and shall not apply to any disposal site operated by the County Commissioners or the Worcester County Sanitary Commission.²
- (2) This Subtitle shall not apply to any legal, bona fide, recognized agricultural practice, provided that such practice does not constitute a health hazard.

§ PH 1-102. Abatement of nuisances. [Amended 11-10-1987 by Bill No. 87-5; 8-2-1988 by Bill No. 88-6; 4-25-1989 by Bill No. 89-2]

- (a) County Commissioners may abate. The County Commissioners may abate any nuisance so designated under this Subtitle.
- (b) Procedure. Where necessary corrections have not been completed after the notice requirements as contained in § PH 1-101(d) hereof have been fulfilled, any Department or official charged with the enforcement of this subtitle may cause or request abatement of any nuisance condition in accordance with the following: [Amended 11-16-2004 by Bill No. 04-11; 5-18-2010 by Bill No. 10-4]
 - (1) Where the Department has ascertained there to be a violation of the provisions of § PH 1-101(a)(1) hereof and corrective actions have not commenced after notice as provided for in § PH 1-101(d) hereof, the Department may enter upon the premises and cut or otherwise remove the overgrowth of vegetation in accordance with the standards as set forth in § PH 1-101(a) hereof. All costs associated with cutting and or removal of the vegetation, and a service fee, shall be assessed in accordance with a fee schedule established by resolution of the County Commissioners. The Department shall mail a statement of charges promptly upon completion of the corrective action to the owner of the premises. All such statements shall be due and payable within thirty days of mailing and shall bear interest thereafter in the same percentage as a delinquent County tax bill. Any unpaid and delinquent statement shall become a lien upon all real estate and personal property of the subject in the same manner as delinquent taxes and a notation shall be made upon the tax records of the County Finance Officer.
 - (2) For all nuisance conditions which remain uncorrected after notice as provided for in § PH 1-101(d) hereof, other than that described in Subsection (b)(1) hereof, the appropriate Department or official may request abatement of a nuisance.
- (c) Notice, order, hearing. After the receipt of a request as described in Subsection (b)(2) hereof, the County Commissioners shall notify, in writing, the owner of the property on which the nuisance is located, as shown on the tax assessment rolls of the County as

1. Editor's Note: This bill also redesignated former Subsection (d) as Subsection (e).
 2. Editor's Note: The Sanitary Commission was abolished by Bill No. 93-19.

maintained by the Maryland Department of Assessments and Taxation, as well as the occupant or other person in possession of the property in question, of the request for abatement and shall send to such person an order requiring the prompt abatement of such nuisance within a reasonable time, to be set in such order, which is to take into account the nature of the nuisance. The notice shall afford the owner, occupant or other person in possession of the premises the opportunity to be heard by the County Commissioners within a reasonable time, to be set in such order, which time shall take into account the nature of the nuisance. Such notice shall be sent to the owner's address as shown on the tax assessment rolls of the County as maintained by the Maryland Department of Assessments and Taxation by registered United States mail. If the address of any owner or person having charge of such lot or lands cannot be located after diligent search, or if the aforementioned notice by registered mail is not accepted or otherwise not deliverable, it shall be sufficient to post such notice on the lot or land. Such notice shall be posted on the lot or lands not less than fifteen days prior to taking any further action and shall contain information describing the nature of the violation, the anticipated corrective action, and whom to contact for further information. [Amended 11-16-2004 by Bill No. 04-11; 5-18-2010 by Bill No. 10-4]

(d) Abatement by County.

- (1) In the event that such person does not abate any such nuisance as prescribed hereby within the prescribed period of abatement or does not appear before the County Commissioners and have such abatement order rescinded by the County Commissioners within the time prescribed, the County Commissioners may enter upon the premises and cause such condition to be removed or otherwise remedied by such means as the County Commissioners may deem most appropriate and expedient.
- (2) Any person, upon receipt of an abatement notice as prescribed by this section, may, at any time up to the date on which such person might have been heard with regard to an order to abate, request the County, in writing, to abate such condition, provided that such request states an affirmative agreement on the part of the requesting party to pay the costs of such removal or abatement.

- (e) Cost of abatement. Any actual costs incurred by the County in removing, abating or otherwise remedying any nuisance as herein prescribed, including reasonable attorney's fees, shall be charged to the owner of the land on which the nuisance existed as well as all subjects of the civil infraction citation and shall become a lien upon all real estate and personal property of the subject of the civil infraction citation in the same manner as delinquent taxes. In the case of a condominium or cooperative, the lien shall be upon all of the individual units proportionally. It shall be the duty of the County Finance Officer to mail a statement of such charges to the persons responsible at the address shown on the tax assessment rolls of the County or, in the case of no address on the assessment roll, to the last known address. All such statements shall be due and payable within thirty days from the date of receipt thereof and shall bear interest thereafter in the same percentage as a delinquent County tax bill. In the event that any such statement becomes delinquent, a notation of the delinquency shall be made upon the tax records of the County Finance Officer. [Amended 7-26-2005 by Bill No. 05-8]

- (f) Emergency nuisances. If, upon receipt of a report pursuant to Subsection (b) hereof, the Commissioners determine that a nuisance constitutes an emergency situation presenting a clear and present danger to the health or safety of the public, the Commissioners may abate the nuisance pursuant to Subsection (d) hereof without notice or hearing; provided, however, that the Commissioners shall make a good-faith effort to informally contact the property owner or occupant of the premises or person in possession and provide a reasonable opportunity to be heard. The cost of abatement pursuant to Subsection (e) shall not be assessed against the property owner until after the property owner has been given a notice and a reasonable opportunity to be heard.

§ PH 1-103. Tattoo establishments. [Added 12-10-1985 by Bill No. 85-4]

- (a) Legislative intent. The County Commissioners of Worcester County have determined:
- (1) That the coloration of the skin by the aid of needles or any other instruments designed to touch, puncture or penetrate the skin by improperly trained or unsupervised individuals or in unsanitary facilities is dangerous to the health and general welfare of the community.
 - (2) That the puncturing or penetrating of skin area that has rash, pimples, boils, infections or other skin disorders or diseases can cause a spread of the skin disorders or diseases which may result in the communication of such skin disorders or diseases to other persons.
 - (3) That the communication of disease poses a threat to the public health, safety and general welfare.
 - (4) That, in order to protect the public health, safety and general welfare, it is necessary to regulate tattoo establishments.
- (b) Definitions. Unless the particular provision or the context otherwise requires, the definitions and provisions contained in this section shall govern the construction, meaning and application of words and phrases as used in this section.

CERTIFICATE OF INSPECTION — The written approval from the Worcester County Health Department, Department of Health and Mental Hygiene of the State of Maryland or their designated representative that the tattooing establishment has been inspected and meets all the requirements of this section relating to physical facilities, equipment and layout for the operation of a tattoo establishment.

EMPLOYEE — Any person over eighteen years of age, other than an operator, who renders any service in connection with the operation of a tattoo establishment and receives compensation from the operator of the business or its patrons.

HEALTH DEPARTMENT — The Worcester County Health Department, Department of Health and Mental Hygiene of the State of Maryland, or its designated representative.

OPERATOR — Any individual, firm, company, corporation or association that owns or operates an establishment where tattooing is performed and any individual who performs or practices the art of tattooing on the person of another.








Worcester County
Department of Environmental Programs

Memorandum

To: Harold L. Higgins, Chief Administrative Officer

From: Robert J. Mitchell, LEHS 
Director, Environmental Programs

Subject: Request for RFP for Consultant Services
Master Water and Sewerage Plan

Date: September 24, 2018

We are writing to request the County Commissioners' approval to distribute the attached RFP to interested consultants. The new Water and Sewerage Plan will provide a substantial update of the 1994 Plan. The Plan will include updated information on the water and sewerage systems in Worcester County and propose a revised map with classification revisions based on revisions to the classification definitions and reflecting changes in the status of water/sewer service to County properties and municipalities.

The purpose of the Water and Sewerage Plan is to provide an overview of the planning policies, needs, and planned infrastructure related to community and individual water and sewerage systems. The Water and Sewerage Plan is organized into five chapters, appendices, and maps defining the various service classifications. The Consultant will first compile a complete update of the 1994 Water and Sewerage Plan to incorporate all approved amendments since 1994 in order to create an up-to-date base plan. Thereafter, working with the Departments of Environmental Programs and Public Works, the Consultant will prepare a new plan with the relevant tables and mapping in printed and electronic formats. The entire process will be under the direction of County staff and will include work sessions with staff and the Planning Commission, presentation(s) before the County Commissioners and public informational sessions.

Attached is the draft RFP including Notice to Bidders, Proposal Instructions, and Bid Forms. The Department of Environmental Program does have encumbered funds to cover the cost for this consultant in a match program for rural legacy in Account #100.1702.7180. Fortunately, the funds were not utilized the past few budget sessions for their intended use. While we plan to utilize funding from this account, we fully realize there are pressing needs, like the intended use we are

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describing in this memo that would merit redirection of these funds for similar purposes. This use has been including in our most recent encumbrance request to the Budget Officer. I would respectfully request at this time that County Commissioners authorize us to advertise for this RFP and permit the use of funds from Account # 100.1702.7180 to be utilized for this project.

As always, if you have any questions or require additional information, please do not hesitate to contact me.

Attachment

cc: Sewer Committee
Kathy Whited/Budget Officer

**NOTICE TO CONSULTANTS
Request for Proposals**

DRAFT

**Water and Sewerage Plan Update
Worcester County, Maryland**

The Worcester County Commissioners are seeking proposals from qualified consultants for the purpose of updating the Worcester County Water and Sewerage Plan to ensure the provision of safe and adequate drinking water and wastewater systems to meet existing and future demands for Worcester County, Maryland. Requests for Proposals (RFPs) are available from the Office of the County Commissioners, Room 1103 - Worcester County Government Center, One West Market Street, Snow Hill, Maryland 21863, obtained online under the "Bids" drop-down menu in the lower right hand side of the home page at www.co.worcester.md.us, or by calling the Commissioners' Office at 410-632-1194 to request a package by mail. Interested consultants are encouraged to attend a **pre-proposal meeting on Tuesday, October 23, 2018 at 10:00 AM** in Training Room B on the 3rd Floor of the Worcester County Government Center, 1 West Market Street, Snow Hill, Maryland 21863. For directions and details, contact Mr. Robert Mitchell, Environmental Programs Director, at 410-632-1220, extension 1601. **Sealed proposals will be accepted until 1:00 PM, Monday, November 12, 2018** in the Office of the County Commissioners at the above address, at which time they will be opened and publicly read aloud. Envelopes shall be marked "**Proposal for Worcester County Water and Sewerage Plan Update**" in the lower left-hand corner. Email submissions will not be accepted

After opening, proposals will be forwarded to the Department of Environmental Programs for tabulation, review and recommendation to the County Commissioners for their consideration at a future meeting. In awarding the proposal, the Commissioners reserve the right to reject any and all proposals, waive formalities, informalities and technicalities therein, and to take whatever proposal they determine to be in the best interest of the County considering cost, proposal content, qualifications of the consultant, quality of project approach, time of delivery or completion, responsibility of consultants being considered, previous experience of consultants with County contracts, or any other factors they deem appropriate. All inquiries shall be directed to Robert Mitchell, Director of Environmental Programs, 410-632-1220, extension 1601, or by email at bmitchell@co.worcester.md.us.

**Request for Proposals
Worcester County Water and Sewerage Plan Update**



**Due Date: Monday, November 12, 2018
1:00 P.M. (Eastern Standard Time)**

**Request for Proposals
Worcester County Water and Sewerage Plan Update**

1. Proposal Submission Instructions:

- 1.1 The County Commissioners of Worcester County, Maryland, hereinafter referred to as “County”, will accept sealed proposals from qualified consultants, hereinafter referred to as “Consultant”, for a complete update of the Worcester County Water and Sewerage Plan to ensure the provision of safe and adequate drinking water and wastewater systems to meet existing and future demands for Worcester County, Maryland. Instructions are provided in the Notice to Consultants.
- 1.2 Consultants are required to submit an original and four (4) copies of its proposal at the time of submission.
- 1.3 Consultants are responsible for ensuring that their proposal is received by County Administration before the deadline indicated. Proposals received after the announced deadline will not be considered.
- 1.4 Any proposal submitted must be signed by the individual or an individual authorized to bind the Consultant. All proposals submitted without such signature may be deemed non-responsive and may not be considered.
- 1.5 As is more fully explained throughout this Request for Proposals (“RFP”), an award, if made, will be made to a responsible Consultant whose proposal is most advantageous to the County, taking into consideration the evaluation factors.

2. Project Description:

2.1 Background

- 2.1.1 Worcester County is the easternmost county in Maryland, with a large agricultural industry, and concentrated development on the coast that supports a significant tourism industry. In addition to the year-round residents, the northeastern portion of the County serves as a second or retirement home for many. Approximately 52% of the County’s population resides in the northeastern quarter of the County, including Berlin, West Ocean City, Ocean Pines, and Ocean City.
- 2.1.2 Worcester County’s year-round population grew significantly from 1990 to 2000 (32.9%) and has continued to expand from 2000 to present (11%). Most of the growth, and most of the resident population has located in the northeastern part of the County. According to the Maryland Department of Planning’s (MDP) most recent projections (July 1, 2016), the current year round population of the County is 51,444.

- 2.1.3** It is important to note that population in Ocean City swells to 70,000, or more, on off-season weekends and up to 300,000 on summer weekends. Assateague State Park and National Seashore and other campgrounds also attract another 100,000 visitors in the summer months. This seasonal population places significant demand on County resources, including recreation and parks facilities and programs, particularly in the northeastern portion of the County.
- 2.1.4** A hard copy of the 1994 revision of the Water and Sewerage Plan with amendments is available for review in the Environmental Programs Department between the hours of 8:30 am and 4:30 pm Monday through Friday. A PDF copy of the 1994 Plan and amendments, as well as the 2006 Worcester County Comprehensive Plan and the 2011 Water Resources Element amendment to the Comprehensive Plan will be provided to each bidder as well.

3. Plan Description:

- 3.1** The new Water and Sewerage Plan will provide a substantial update of the 1994 Plan. The Plan will include updated information on the water and sewerage systems in Worcester County and propose a revised map with classification revisions based on revisions to the classification definitions and reflecting changes in the status of water/sewer service to County properties and municipalities. The Plan is required by the Maryland Department of the Environment (MDE) to include a procedure for a Triannual revision, and additional direction for the Plan to be updated at that time, if necessary.

The purpose of the Water and Sewerage Plan is to provide an overview of the planning policies, needs, and planned infrastructure related to community and individual water and sewerage systems. The Water and Sewerage Plan is organized into five chapters, appendices, and maps defining the various service classifications. The Consultant will first compile a complete update of the 1994 Water and Sewerage Plan to incorporate all approved amendments since 1994 in order to create an up-to-date base plan. Thereafter, working with the Departments of Environmental Programs and Public Works, the Consultant will prepare a new plan consisting of the following sections, at a minimum:

- 3.1.1** Introduction: A statement that the Plan has officially been adopted by the County; a statement that the Plan has been submitted to MDE and meets the requirements of the Code of Maryland Regulations (COMAR); a statement certifying that the engineering aspects of the Plan have been prepared and reviewed for adequacy by a Professional Engineer (P.E.) licensed in the State of Maryland; and MDE's approval letter.
- 3.1.2** Chapter 1: This chapter describes the planning process and procedures for the adoption of amendments to the Plan text or maps. The chapter includes both general and special policies that have been adopted for the designation of community water and sewer service areas, and which regulate water and sewerage system extensions, connections, and their staging. There would be two main items as follows:
- (a) Discussion and Statement that the goals and polices listed in the chapter are consistent with the County Comprehensive Plan; and

- (b) A summary and chart of County organization relating to management of water and sewerage facilities.

A draft of this section of the Plan will be provided by the County.

- 3.1.3** Chapter 2: This chapter provides an overview of the natural features of the County, the demographic and development patterns, and other factors related to the physical environment. This information provides the background for resource protection as it relates to water supply and sewerage system planning in Worcester County. General background information relevant to water and sewerage planning shall include: maps, charts and tables reflecting aquifers, soil drainage, topography, ground and surface patterns, and water quality; population projections; maps reflecting existing land use, zoning and comprehensive development; table reflecting existing and zoned land use in acres; map showing existing/proposed major public institutions (such as schools, hospitals, corrections facilities, government complexes); and a table reflecting populations of each.
- 3.1.4** Chapter 3 Water: Description of existing, planned and future demand for water service areas including tables, maps, charts, graphs, descriptive information and all other matter regarding these systems including groundwater resources and quality; existing/projected water demands and sources of pollution/contamination; and alternatives and rationale of providing future water supplies and/or interconnections of existing supplies.

For each existing water services area, the following shall be reflected: operating agency, rated and actual productions, type of treatment, location, Operation and Maintenance (O&M) costs, and proposed means of financial improvements. For any proposed new water supply, a summary of the environmental impacts shall be outlined.

Tables are also required reflecting:

- Population projections, projected water supply demands and planned capacity of each water service area through 2030.
 - Inventory of community systems, supplies, ground water withdrawals, existing treatment facilities, well problem areas, and immediate, as well as 5- and 10-year priorities for water development.
- 3.1.5** Chapter 4: Sewerage: Description of existing, planned and community and multi-use sewerage systems including tables, maps, charts, graphs, descriptive information and all other matters regarding these systems including locations of points of discharges, evaluation of discharge points and protected water uses; and conformance with effluent limitations, rationale for planned alternatives for treatment facilities, pump stations or interceptors.

For each existing sewer service area and community system, the following shall be reflected: operating agency, design average and peak flows, whether combined or

separate collection systems, level and type of treatment, sludge disposal plans, condition of treatment and transmission facilities, O&M costs, and proposed means of financing improvements.

Detailed information regarding the County onsite sewage systems, innovative systems, and prior planning efforts, including the Groundwater Protection Report, will be supplied to the successful consultant by the Department of Environmental Programs. This will also include septic problem areas that are to be included in the new plan.

Tables are also required reflecting:

- Population projections, present and expected demands and capacities, inventory of existing wastewater treatment plants (WWTP), problem areas, water quality problems due to storm drain outfalls and to non-point sources, immediate, as well as 5- and 10-year priorities for sewerage system development.

3.1.6 Chapter 5 Financial Implementation Plan, including:

- Worcester County's Existing Financing Programs.
- Alternative Funding Strategies.
- Capital Improvements, Programming, Planning and Budgeting Process.
- Project Prioritization and Coordination.

A working draft of this section of the Plan will be provided by the County.

3.1.7 Mapping files for the current areas, the shapefiles and associated data, as available, will be provided to the Consultant for their use and insertion into the plan. Draft maps for the new plan, as available, will be provided to the successful Consultant upon award in the delivery of additional materials. It will be up to the successful Consultant to prepare, edit, and finalize those maps for presentation to the County for review along with the Plan documents.

3.2 The completed plan shall be in accordance with the requirements of COMAR, Title 26, Department of the Environment, Subtitle 03, Chapter 01, Planning Water Supply and Sewage Systems, and Title 9, Subtitle 5, County Water and Sewage Plans of the Environment Article of the Annotated Code of Maryland. The Plan shall be consistent with the Water Resources and Land Use elements of the Worcester County Comprehensive Plan dated March 7, 2006 and the Comprehensive Plans of all municipalities located in Worcester County. Those plans are generally available to the Consultant online. However where such plans are available to the County, they will be made available to the successful Consultant in PDF format.

3.3 The Plan shall include all required statements, descriptive information, charts, mapping, graphics, tables, statistics, projections and all other matters required by law and/or regulation in order to accurately identify all existing, planned and future water and sewerage facilities in Worcester County, Maryland.

3.4 Textual materials, tables, charts, graphs, and all other illustrations required shall be prepared on 8 ½ x 11 inch stock paper or any other size which can be folded for inclusion in a 3-ring

binder. Margins on the left-hand edge shall be formatted to enable 3-hole punching so as not to obstruct the printed information on each page. The format of the Plan shall be prepared so that any new or additional material may be inserted in the 3-ring binder in the appropriate places to provide a continuously updated and current Plan. The County requires that a complete copy of the plan be provided in digital format and be prepared in Microsoft Word (for word processing portions) and Excel (for spreadsheet portions). Mapping is a critical element of the plan, and preliminary work, including available electronic files, done by County staff will be delivered to the Consultant for their use.

3.5 Deviations from the format guidelines described above are unacceptable.

4. Submittal Requirements and Deliverable Schedule:

- 4.1 Consultant shall provide one (1) original hard copy and one original electronic copy of the up-to-date base plan which includes the 1994 Water and Sewerage Plan and incorporates all approved amendments since 1994.
- 4.2 Consultant shall provide twenty (20) copies of all written draft reports and chapters specified in Section 3 (Plan Description) of this RFP. Draft documents shall be submitted to the Environmental Programs Department, who will distribute to other agencies, and others as designated by the County.
- 4.3 Consultant shall attend all necessary meetings with staff to draft the Plan and all public meetings and hearings associated with the Plan review and approval process. For budgeting purposes, the Consultant should plan to attend at least fifteen (15) onsite meetings with the staff, County agencies as directed by the County, and County Commissioners.
- 4.4 Consultant shall update the draft documents based on comments received from reviewing departments of the County and State agencies.
- 4.5 Consultant shall present the final draft plan to a group representing the Worcester County Sewer Committee
- 4.6 Consultant shall prepare and deliver thirty-two (32) bound copies of the final approved Plan in 3-Ring Heavy Duty binders.
- 4.7 **Milestones** – The County anticipates that this project will require approximately 14 months to complete from Notice of Award to Consultant. Deadlines will be specified in the Contract between the County and the Consultant. In their proposal, Consultant shall specify their proposed schedule for submittal of the following deliverables:

Proposed Deliverables and Schedule

RFP Awarded – (Anticipated on or about December 18, 2018)
Meeting with County Staff and Receipt of Data Items – (within 14-days of award)
Chapter 1 and Chapter 2 Drafts
Review of drafts with County Staff – (allow 2 weeks)
Chapter 3, and Chapter 4 Drafts
Review of drafts with County Staff – (allow 2 weeks)
Chapter 5 and Assembly of Appendices
Review Chapter 5 and Appendices with County Staff – (allow 2 weeks)
Assemble Draft Plan
Meeting with County staff on all work products, including all draft maps
Public Listening Session(s)
Work Session with County staff and Planning Commission
Presentation to County Commissioners to schedule Public Hearing
Public Hearing by County Commissioners – (allow 30-days for advertising by County)
Final Edits/Changes to County Staff
Final Approval by County Commissioners
Submission of Final Plan to County
Meet with MDE to present Final Plan

5. Project Payments:

5.1 Monthly itemized invoices from the successful Consultant shall be approved by the County based on a percentage of completion of the approved delivery schedule, less 10% Retainage to be paid at the completion of the project upon receipt of all deliverables.

6. Time for Completion:

6.1 Notice to Proceed: The successful Consultant shall proceed with furnishing the services as described promptly upon receiving the Notice to Proceed. All services shall be performed simultaneously and in accordance with the schedule provided in the contract between the County and Consultant.

7. Projected Award Timetable:

The following projected timetable is included as a guide for planning purposes. The County reserves the right to adjust this timetable as required during the course of the RFP process:

<u>Event</u>	<u>Date</u>
RFP Approved and Issued	October 5, 2018
Proposals Due	November 12, 2018
Complete Evaluation of Proposals and Interviews	November 27, 2018
Award Contract	December 18, 2018

8. County's Responsibilities:

The Department of Environmental Programs will provide the successful Consultant with the following information:

- 8.1** Provide all information in the possession of the County as referenced in Section 2 (Project Description) and Section 3 (Plan Description) of this RFP.
- 8.2** Examine all information submitted and presented
- 8.3** Advertise any public notices, at County's expenses, associated with public review of the draft Plan.
- 8.4** Robert Mitchell, Director of Environmental Programs (telephone 410-632-1200, x1601) shall be the primary point of contact for the County once the contract has been awarded for this project.

9. RFP Process:

- 9.1** Consultants are to submit written proposals, which present its qualifications and understanding of the work to be performed. Consultants are required to address each evaluation criteria in the order listed and to be specific in presenting its qualifications. Each Consultant's proposal shall provide all information, which it considers pertinent to its qualifications for the project and which responds to the Plan Description, Submittal Requirements and Evaluation Criteria described herein. Please limit the body of your proposal to no more than thirty (30) pages. Attachments and Appendices will not count towards the 30-page limit.

10. Proposal Content:

- 10.1** Consultant's proposal shall be prepared in accordance with, and in the order of the format guidelines below:
- 10.2** **The maximum number of pages for your response is thirty (30) typed pages. Supplemental information may be submitted but must be submitted separately from the specific responses required below and elsewhere in the RFP.**
 - 10.2.1** Provide information, which documents the Consultant's qualifications and experience, including its ability, capacity, and number of years of experience in providing environmental planning and consulting services. Consultant should specify if it has updated other county water and sewerage plans in Maryland within the last five (5) years.
 - 10.2.2** Description of the Consultant's approach to the project and a proposed work plan and schedule for the project that addresses the milestones as presented within the 14 month schedule.

10.2.3 Cost proposal on the form provided herein.

10.2.4 A list of key staff to be assigned to the project including resumes of staff that will be responsible for the work. At a minimum, staff shall include a planner or environmental planner as key staff assigned to this project. The list of key staff shall not be revised without the written consent of the County.

10.2.5 Expand on previous projects where expertise for environmental planning or on projects of similar scope meeting the requirements of this solicitation have been demonstrated. Such information shall include:

1. Description and location of the projects, including a detailed description of Consultant's responsibilities.
2. Name, phone number and email address of the contact person for each project.

10.2.6 Acknowledgement of Any Past or Present Lawsuits - Include a statement concerning whether lawsuits have been filed against the Consultant, its principals or any joint venture partner for misfeasance or malfeasance with respect to professional services and, if so, a detailed listing of the adverse action, cause, number, jurisdiction in which filed and current status.

11. Evaluation Criteria:

A Review Committee established by the County will read, review and evaluate each proposal for technical merit and selection will be made primarily on the basis of the criteria listed below. Varying weights to illustrate their importance have been assigned to each criteria listed below in the form of points, with a maximum possible score of 100 points. During their review of each proposal each member of the committee will assign a final score based on the criteria. The evaluation criteria are as follows:

- 11.1** Scope and quality of services provided and the demonstrated ability of the Consultant to provide all services identified in this RFP. This includes the Consultant's ability to accurately produce Plan documents in accordance with State and County requirements.
(15 points)
- 11.2** Demonstration of the Consultant's ability to successfully complete the project and deliver a Plan acceptable to the County and State approval bodies as reflected in projects of a similar nature to the requirements identified in this RFP. This includes professional staff credentials (i.e. planner/environmental planner on staff) and division of project responsibilities
(35 points)
- 11.3** Past experience updating water and sewerage plans for other Maryland counties.
(30 points)
- 11.4** Consultant's current total workload and capacity to complete the work in a timely manner.
(10 points)

- 11.5** The responsiveness of the firm to the submittal format instructions contained in Section 9 (RFP Process) and Section 10 (Proposal Content). This also includes submission of the correct number of copies of the proposal; Signature Sheet; Cost Proposal, and acknowledgement of addendums, as may be applicable.
(10 points)

Total cost and other details provided in the Cost Proposal Page will also be considered in selecting the Consultants to interview. Once each member of the Review Committee has rated each proposal, a composite rating shall be developed which indicates the Committee's collective ranking of the Consultants that are qualified to perform this project.

Consultants will then be selected for interview during the week of November 26, 2018. Following interviews, the Review Committee will forward their recommendation for selection to County Administration for review and approval by the County Commissioners at a future meeting.

12. Contract Information:

- 12.1** Following award of the proposal by the County, an award letter and two original copies of the contract will be mailed to the successful Consultant for signature and return to the County. The County will sign both copies of the contract and return one fully executed copy to the Consultant and retain the second fully executed copy for the County.

13. Indemnification:

- 13.1** The Consultant shall indemnify and hold harmless the County Commissioners of Worcester County, Maryland, and its elected and appointed officials, agents and employees from and against any and all liabilities, judgments, settlements, losses, costs or charges, including attorney fees, as a result of any claim, demand, action, or suit relating to any bodily injury, sickness or disease, including death, loss or property damage or destruction caused by, arising out of, related to or associated with this work by the Consultant and its members, officers, agents, employees, subcontractors, or invitees.

14. Insurance:

- 14.1** The successful Consultant will be required to provide a copy of its Certificate of Insurance including Professional Liability Insurance in the amount outlined in the Insurance Table. This Certificate should name the County Commissioners of Worcester County, Maryland as certificate holder and as additionally insured. This certificate shall be mailed to:

Mr. Kelly Shannahan
Assistant Chief Administrative Officer
County Administration
Government Center – Room 1103
1 West Market Street
Snow Hill, MD 21863

15. Proposals Binding 90 Days:

15.1 All proposals submitted shall be binding for ninety (90) calendar days following the due date, unless the Consultant, upon written request from the County, agrees to an extension.

16. No Compensation to the Consultant for RFP Preparation:

16.1 The Consultant will in no way be compensated for the preparation of this RFP.

17. Further Information:

17.1 Questions regarding this RFP shall be emailed to Robert Mitchell at bmitchell@co.worcester.md.us . The County will formally respond to questions via Addendums to the RFP. Please be advised that questions will be entertained until 2:00 P.M. on Thursday, November 8, 2018. Questions submitted after this time will not be answered.

**Request for Proposals
Worcester County Water and Sewerage Plan Update**

Signature Sheet

My signature certifies that the proposal as submitted complies with all Terms and Conditions set forth in this RFP.

My signature also certifies that the accompanying proposal is not the result of, or affected by, any unlawful act of collusion with another person or company engaged in the same line of business or commerce.

My signature also certifies that this Consultant has no business or personal relationships with any other companies or persons that could be considered as a conflict of interest to the County, and that there are no principals, officers, agents, employees, or representatives of this Consultant that have any business or personal relationships with any other companies or persons that could be considered as a conflict of interest or a potential conflict of interest to the County, pertaining to any and all work or services to be performed as a result of this request and any resulting contract with the County.

PROPOSAL MUST BE SIGNED TO BE VALID

I hereby certify that I am authorized to sign as a Representative for the Consultant:

Date: _____ Signature: _____
Typed Name: _____
Title: _____
Firm: _____
Address: _____

Phone: _____

**Cost Proposal Page
Request for Proposals
Worcester County Water and Sewerage Plan Update**

Total lump sum cost for the services outlined in our proposal in compliance with all criteria of the RFP by the County for successful completion and adoption of the Worcester County Water and Sewerage Plan Update project is as follows:

_____ Dollars \$ _____
(Written) (Figures)

Estimated number of hours to complete the plan: _____ **

**Attach an itemized breakdown of the lump sum cost

Breakdown shall include the following:

Labor Category Description	Hours	Direct Hourly Rate	Total Labor Cost
		Direct Labor Cost Subtotal	
		Multiplier	
		Total Labor Cost	
Subcontractor Expenses Description	Direct Cost	Mark-up(%)	Total Cost
		Total Subcontractor Expenses	
		TOTAL PRICE	

COST PROPOSAL MUST BE SIGNED TO BE VALID

I hereby certify that I am authorized to sign as a Representative for the Consultant:

Date: _____ Signature: _____
 Typed Name: _____
 Title: _____
 Firm: _____
 Address: _____

 Phone: _____

Consultant acknowledges receipt of Addendum(s), if any, by initialing the following:

Addendum #1 _____ Addendum #3 _____
 Addendum #2 _____ Addendum #4 _____

Insurance Requirements for
County Commissioners of Worcester County, Maryland

Automobile Liability Insurance: Coverage sufficient to cover owned, hired and non-owned coverage, including bodily injury, per person and occurrence and property damage per occurrence.*

Minimum Limits

\$1,000,000 Combined Single Limit

**Required for all contracts EXCEPT architectural design, review and/or engineering services and planning, research and/or policy projects.*

Statutory Workers Compensation and Employer's Liability Insurance: Workers Compensation Coverage shall meet statutory limits as required by the State of Maryland or other applicable laws and Employers' Liability Insurance as follows.*

Minimum Limits

\$500,000 Each accident for bodily injury by accident

\$500,000 Policy limit for bodily injury by disease and

\$500,000 Each employee for bodily injury by disease

**Workers' Compensation and Employer's Liability Insurance is required for all contractors who have employees or subcontractors.*

Professional Liability Insurance: Coverage for errors, omissions, and negligent acts per claim and aggregate, with one year discovery period.*

Minimum Limits

\$1,000,000 Each Occurrence

\$5,000 Deductible

**Required for all Professional Service Contracts ONLY including but not limited to architectural design, review and/or engineering services.*

Bidders List

McCormick and Taylor
106 Milford Street, Suite 105
Salisbury, MD 21804

Davis Bowen & Friedel
601 E Main St #100
Salisbury, MD 21804

GMB
206 West Main Street
Salisbury MD 21801

ERM
180 Admiral Cochrane Drive #400
Annapolis, MD 21401

Tetra Tech, Inc.
240 Continental Drive, Ste. 200
Newark, DE 19713

O'Brien & Gere
11350 McCormick Road #304
Hunt Valley, MD 21031

Whitman, Requardt & Associates, LLP
801 South Caroline St.
Baltimore, MD 21231

EA Engineering, Science, and Technology, Inc.
225 Schilling Circle, Suite 400
Hunt Valley, MD 21031

Gannett Fleming
Rutherford Plaza Building, Suite 300
7133 Rutherford Road
Baltimore, MD 21244-2718



9

Worcester County
DEPARTMENT OF PUBLIC WORKS
6113 TIMMONS ROAD
SNOW HILL, MARYLAND 21863

JOHN H. TUSTIN, P.E.
DIRECTOR

JOHN S. ROSS, P.E.
DEPUTY DIRECTOR

TEL: 410-632-5623
FAX: 410-632-1753

MEMORANDUM

TO: Harold L. Higgins, Chief Administrative Officer
FROM: John H. Tustin, P.E., Director *JHT*
DATE: September 25, 2018
SUBJECT: Bid Recommendation – Corrugated Metal Pipe
Department of Public Works – Roads Division

DIVISIONS

MAINTENANCE
TEL: 410-632-3766
FAX: 410-632-1753

ROADS
TEL: 410-632-2244
FAX: 410-632-0020

SOLID WASTE
TEL: 410-632-3177
FAX: 410-632-3000

FLEET
MANAGEMENT
TEL: 410-632-5675
FAX: 410-632-1753

WATER AND
WASTEWATER
TEL: 410-641-5251
FAX: 410-641-5185

Monday, September 24, 2018, bids were received and opened for the purchase of corrugated metal pipe to be utilized by the Department of Public Works, Roads Division. Below is a summary of the four bids received.

Vendor	Total Bid Price	Page
Chemung Supply Corp, Elmira, NY	\$34,660.00	3
Lane Enterprises, Inc., Bealton, VA	\$36,265.80	5
Tri-Supply and Equipment, New Castle, DE	\$37,482.91	7
Ferguson Waterworks, Salisbury, MD	\$37,624.65	9

Upon review by the Department of Public Works, Roads Division, it was determined that all four bids received did meet the required specifications; therefore, it is recommended that Chemung Supply Corp. be awarded the contract to supply pipe for their quoted price of \$34,660.00. Funding in the amount of \$40,000.00 was approved in the current FY19 operating budget in account 100.1202.6140.040.

Should you have any questions, please don't hesitate to call me.

Attachments

cc: Frank J. Adkins

Competitive Bid Worksheet

Item: Purchase of Corrugated Metal Pipe and Plastic Pipe

Bid Deadline/Opening Date: 1:00 P.M., Monday, September 24, 2018

Bids Received by deadline = 4

Vendor's Submitting Bids

Total Bid Price

Lane Enterprises, Inc.
6369 Schoolhouse Road
Bealeton, VA 22712

8 36,265.80

Chemung Supply Corp.
Rt 14 Miracle Mile - (P.O. Box 527)
Elmira, NY 14903

4 34,660

Ferguson Waterworks
28596 Naylor Mill Road
Salisbury, MD 21801

7 37,634.65 * 37,624.65

Tri-Supply & Equipment
1685 River Road
New Castle, DE 19720

4 37,482.91

BID FORM

**Worcester County Department of Public Works – Roads Division
“FY19 – Pipe Bid”**

I/We have reviewed the specifications and provisions for furnishing **Riveted Galvanized Steel Corrugated Metal Pipe** to the Worcester County Department of Public Works, Roads Division and understand said requirements. I/We hereby propose to furnish pipe and bands for:

TOTAL BID PRICE (including freight): \$ 34,660.00

Pipe and bands to be delivered within 10-14 calendar days from receipt of written order.

Worcester County reserves the right to adjust the amount of pipe depending on varying circumstances.

BID MUST BE SIGNED TO BE VALID.

Date: 9/20/18

Signature: 

Typed Name: Carl H Perine

Title: Vice-President

Firm: Chemung Supply Corp.

Address: PO Box 527

Elmira, NY 14902

Phone: 607-733-5506

Bid Specifications

The Worcester County Commissioners are accepting bids on the following Riveted Galvanized Steel Corrugated Metal Pipe or Helically Corrugated Pipe with Two Annular Corrugations on each end of pipe delivered to Worcester County Department of Public Works, Roads Division, Snow Hill Shop, 5764 Worcester Highway, Snow Hill, MD 21863. NO DIMPLE BANDS, NO INDIVIDUAL LUGS, ONLY BANDS WITH ANGLES ACROSS WIDTH OF BAND OR APPROVED EQUAL WILL BE ACCEPTED FOR PIPE SIZES LARGER THAN 18". ALL PIPES CUT TO MEET SIZE REQUIREMENTS MUST BE RE ROLLED. All Federal and State Taxes are exempt.

<u>PIECES</u>	<u>GAUGE</u>	<u>DIAMETER</u>	<u>PIPE</u>	<u>LENGTH</u>	<u>PRICE/FT</u>	<u>TOTAL</u>
6	12	48"	GCCMP	20'	\$ 60.00	\$7200.00
3	12	48"	GCCMP	14'	\$ 60.00	\$2520.00
5		48"	BANDS		\$ 90.00	\$ 450.00
4	12	36"	GCCMP	20'	\$ 45.00	\$3600.00
10		36"	BANDS		\$ 68.00	\$ 680.00
3	14	30"	GCCMP	16'	\$ 28.00	\$1344.00
4	14	30"	GCCMP	14'	\$ 28.00	\$1568.00
2	14	30"	GCCMP	12'	\$ 28.00	\$ 672.00
2	14	24"	GCCMP	16'	\$ 22.00	\$ 704.00
1	14	24"	GCCMP	14'	\$ 22.00	\$ 308.00
5	16	18"	GCCMP	20'	\$ 14.00	\$1400.00
2	16	18"	GCCMP	16'	\$ 14.00	\$ 448.00
5	16	18"	GCCMP	14'	\$ 14.00	\$ 980.00
5	16	15"	GCCMP	20'	\$ 11.00	\$1100.00
4	16	15"	GCCMP	16'	\$ 11.00	\$ 704.00
5	16	15"	GCCMP	14'	\$ 11.00	\$ 770.00
15	16	12"	GCCMP	20'	\$ 9.00	\$2700.00
25	16	12"	GCCMP	16'	\$ 9.00	\$3600.00
20	16	12"	GCCMP	14'	\$ 9.00	\$2520.00
9	16	12"	GCCMP	12'	\$ 9.00	\$ 972.00
30		12"	BANDS		\$ 14.00	\$ 420.00

BID FORM

**Worcester County Department of Public Works – Roads Division
“FY19 – Pipe Bid”**

I/We have reviewed the specifications and provisions for furnishing **Riveted Galvanized Steel Corrugated Metal Pipe** to the Worcester County Department of Public Works, Roads Division and understand said requirements. I/We hereby propose to furnish pipe and bands for:

TOTAL BID PRICE (including freight): \$ 36,265.80

Pipe and bands to be delivered within 30-45 calendar days from receipt of written order.

Worcester County reserves the right to adjust the amount of pipe depending on varying circumstances.

BID MUST BE SIGNED TO BE VALID.

Date: 9/18/18

Signature: Annette M Bliss, VP

Typed Name: Annette M. Bliss

Title: Vice President

Firm: Lane Enterprises, Inc.

Address: P.O. Box 67
6369 Schoolhouse Road
Bealeton, VA 22712

Phone: (540) 439-3201

Bid Specifications

The Worcester County Commissioners are accepting bids on the following Riveted Galvanized Steel Corrugated Metal Pipe or Helically Corrugated Pipe with Two Annular Corrugations on each end of pipe delivered to Worcester County Department of Public Works, Roads Division, Snow Hill Shop, 5764 Worcester Highway, Snow Hill, MD 21863. NO DIMPLE BANDS, NO INDIVIDUAL LUGS, ONLY BANDS WITH ANGLES ACROSS WIDTH OF BAND OR APPROVED EQUAL WILL BE ACCEPTED FOR PIPE SIZES LARGER THAN 18". ALL PIPES CUT TO MEET SIZE REQUIREMENTS MUST BE RE ROLLED. All Federal and State Taxes are exempt.

<u>PIECES</u>	<u>GAUGE</u>	<u>DIAMETER</u>	<u>PIPE</u>	<u>LENGTH</u>	<u>PRICE/FT</u>	<u>TOTAL</u>
6	12	48"	GCCMP	20'	\$ <u>63.20</u>	\$ <u>7584.00</u>
3	12	48"	GCCMP	14'	\$ <u>63.20</u>	\$ <u>2654.40</u>
5		48"	BANDS		\$ <u>80.20</u>	\$ <u>401.00</u>
4	12	36"	GCCMP	20'	\$ <u>47.75</u>	\$ <u>3820.00</u>
10		36"	BANDS		\$ <u>64.75</u>	\$ <u>647.50</u>
3	14	30"	GCCMP	16'	\$ <u>28.70</u>	\$ <u>1377.60</u>
4	14	30"	GCCMP	14'	\$ <u>28.70</u>	\$ <u>1607.20</u>
2	14	30"	GCCMP	12'	\$ <u>28.70</u>	\$ <u>688.80</u> (P18)
2	14	24"	GCCMP	16'	\$ <u>23.50</u>	\$ <u>752.00</u>
1	14	24"	GCCMP	14'	\$ <u>23.50</u>	\$ <u>329.00</u>
5	16	18"	GCCMP	20'	\$ <u>14.10</u>	\$ <u>1410.00</u>
2	16	18"	GCCMP	16'	\$ <u>14.10</u>	\$ <u>451.20</u>
5	16	18"	GCCMP	14'	\$ <u>14.10</u>	\$ <u>987.00</u>
5	16	15"	GCCMP	20'	\$ <u>11.95</u>	\$ <u>1195.00</u>
4	16	15"	GCCMP	16'	\$ <u>11.95</u>	\$ <u>764.80</u>
5	16	15"	GCCMP	14'	\$ <u>11.95</u>	\$ <u>836.50</u>
15	16	12"	GCCMP	20'	\$ <u>9.60</u>	\$ <u>2880.00</u>
25	16	12"	GCCMP	16'	\$ <u>9.60</u>	\$ <u>3840.00</u>
20	16	12"	GCCMP	14'	\$ <u>9.60</u>	\$ <u>2688.00</u>
9	16	12"	GCCMP	12'	\$ <u>9.60</u>	\$ <u>1036.80</u>
30		12"	BANDS		\$ <u>10.50</u>	\$ <u>315.00</u>

BID FORM

**Worcester County Department of Public Works – Roads Division
“FY19 – Pipe Bid”**

I/We have reviewed the specifications and provisions for furnishing **Riveted Galvanized Steel Corrugated Metal Pipe** to the Worcester County Department of Public Works, Roads Division and understand said requirements. I/We hereby propose to furnish pipe and bands for:

TOTAL BID PRICE (including freight): \$ # 37,482.91

Pipe and bands to be delivered within 30 calendar days from receipt of written order.

Worcester County reserves the right to adjust the amount of pipe depending on varying circumstances.

BID MUST BE SIGNED TO BE VALID.

Date: 9/19/18 Signature: *Neal G. Messick*

Typed Name: NEAL G. MESSICK

Title: OWNER / PARTNER

Firm: TAI SUPPLY & EQUIPMENT

Address: 110 COLUMBIA RD
SALISBURY, MD 21801

Phone: 410-546-2900

Bid Specifications

The Worcester County Commissioners are accepting bids on the following Riveted Galvanized Steel Corrugated Metal Pipe or Helically Corrugated Pipe with Two Annular Corrugations on each end of pipe delivered to Worcester County Department of Public Works, Roads Division, Snow Hill Shop, 5764 Worcester Highway, Snow Hill, MD 21863. NO DIMPLE BANDS, NO INDIVIDUAL LUGS, ONLY BANDS WITH ANGLES ACROSS WIDTH OF BAND OR APPROVED EQUAL WILL BE ACCEPTED FOR PIPE SIZES LARGER THAN 18". ALL PIPES CUT TO MEET SIZE REQUIREMENTS MUST BE RE ROLLED. All Federal and State Taxes are exempt.

<u>PIECES</u>	<u>GAUGE</u>	<u>DIAMETER</u>	<u>PIPE</u>	<u>LENGTH</u>	<u>PRICE/FT</u>	<u>TOTAL</u>
6	12	48"	GCCMP	20'	\$ <u>65.36</u>	\$ <u>7,843.20</u>
3	12	48"	GCCMP	14'	\$ <u>65.36</u>	\$ <u>2,745.12</u>
5		48"	BANDS		\$ <u>82.95</u>	\$ <u>414.75</u>
4	12	36"	GCCMP	20'	\$ <u>49.40</u>	\$ <u>3,952.00</u>
10		36"	BANDS		\$ <u>66.97</u>	\$ <u>669.70</u>
3	14	30"	GCCMP	16'	\$ <u>29.66</u>	\$ <u>1,423.68</u>
4	14	30"	GCCMP	14'	\$ <u>29.66</u>	\$ <u>1,660.96</u>
2	14	30"	GCCMP	12'	\$ <u>29.66</u>	\$ <u>711.84</u>
2	14	24"	GCCMP	16'	\$ <u>24.31</u>	\$ <u>777.92</u>
1	14	24"	GCCMP	14'	\$ <u>24.31</u>	\$ <u>340.34</u>
5	16	18"	GCCMP	20'	\$ <u>14.54</u>	\$ <u>1,454.00</u>
2	16	18"	GCCMP	16'	\$ <u>14.54</u>	\$ <u>465.28</u>
5	16	18"	GCCMP	14'	\$ <u>14.54</u>	\$ <u>1,017.80</u>
5	16	15"	GCCMP	20'	\$ <u>12.34</u>	\$ <u>1,234.00</u>
4	16	15"	GCCMP	16'	\$ <u>12.34</u>	\$ <u>789.76</u>
5	16	15"	GCCMP	14'	\$ <u>12.34</u>	\$ <u>863.80</u>
15	16	12"	GCCMP	20'	\$ <u>9.92</u>	\$ <u>2,976.00</u>
25	16	12"	GCCMP	16'	\$ <u>9.92</u>	\$ <u>3,968.00</u>
20	16	12"	GCCMP	14'	\$ <u>9.92</u>	\$ <u>2,776.00</u>
9	16	12"	GCCMP	12'	\$ <u>9.92</u>	\$ <u>1,071.36</u>
30		12"	BANDS		\$ <u>10.86</u>	\$ <u>325.80</u>

BID FORM

**Worcester County Department of Public Works – Roads Division
“FY19 – Pipe Bid”**

I/We have reviewed the specifications and provisions for furnishing **Riveted Galvanized Steel Corrugated Metal Pipe** to the Worcester County Department of Public Works, Roads Division and understand said requirements. I/We hereby propose to furnish pipe and bands for:

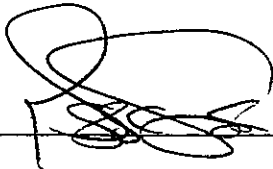
TOTAL BID PRICE (including freight): \$ 37,634.65 *

Pipe and bands to be delivered within 21 calendar days from receipt of written order.

Worcester County reserves the right to adjust the amount of pipe depending on varying circumstances.

BID MUST BE SIGNED TO BE VALID.

Date: September 24, 2018

Signature: 

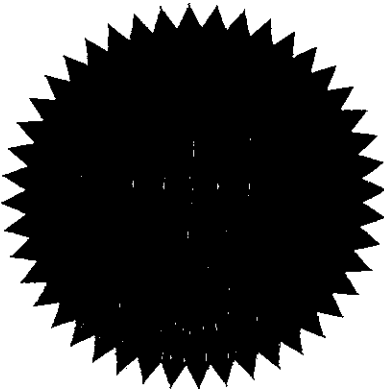
Typed Name: Gregg Thomas

Title: Sales Representative

Firm: Ferguson Waterworks

Address: 28596 Naylor Mill Road
Salisbury, MD 21801

Phone: 410-677-6793

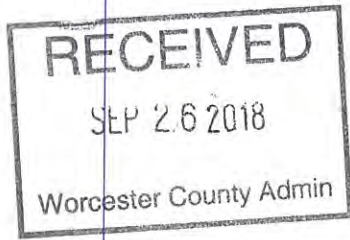


*Prices include freight for orders meeting or exceeding \$5000.00 total.

Bid Specifications

The Worcester County Commissioners are accepting bids on the following Riveted Galvanized Steel Corrugated Metal Pipe or Helically Corrugated Pipe with Two Annular Corrugations on each end of pipe delivered to Worcester County Department of Public Works, Roads Division, Snow Hill Shop, 5764 Worcester Highway, Snow Hill, MD 21863. NO DIMPLE BANDS, NO INDIVIDUAL LUGS, ONLY BANDS WITH ANGLES ACROSS WIDTH OF BAND OR APPROVED EQUAL WILL BE ACCEPTED FOR PIPE SIZES LARGER THAN 18". ALL PIPES CUT TO MEET SIZE REQUIREMENTS MUST BE RE ROLLED. All Federal and State Taxes are exempt.

<u>PIECES</u>	<u>GAUGE</u>	<u>DIAMETER</u>	<u>PIPE</u>	<u>LENGTH</u>	<u>PRICE/FT.</u>	<u>TOTAL</u>
6	12	48"	GCCMP	20'	\$ 65.00	\$ 7,800.00
3	12	48"	GCCMP	14'	\$ 65.00	\$ 2,730.00
5		48"	BANDS		\$ 95.51	\$ 487.55
4	12	36"	GCCMP	20'	\$ 49.00	\$ 3,920.00
10		36"	BANDS		\$ 73.51	\$ 735.10
3	14	30"	GCCMP	16'	\$ 30.00	\$ 1,440.00
4	14	30"	GCCMP	14'	\$ 30.00	\$ 1,680.00
2	14	30"	GCCMP	12'	\$ 30.00	\$ 720.00
2	14	24"	GCCMP	16'	\$ 24.00	\$ 768.00
1	14	24"	GCCMP	14'	\$ 24.00	\$ 336.00
5	16	18"	GCCMP	20'	\$ 15.00	\$ 1,500.00
2	16	18"	GCCMP	16'	\$ 15.00	\$ 480.00
5	16	18"	GCCMP	14'	\$ 15.00	\$ 1,050.00
5	16	15"	GCCMP	20'	\$ 12.00	\$ 1,200.00
4	16	15"	GCCMP	16'	\$ 12.00	\$ 768.00
5	16	15"	GCCMP	14'	\$ 12.00	\$ 840.00
15	16	12"	GCCMP	20'	\$ 10.00	\$ 3,000.00
25	16	12"	GCCMP	16'	\$ 10.00	\$ 4,000.00
20	16	12"	GCCMP	14'	\$ 10.00	\$ 2,800.00
9	16	12"	GCCMP	12'	\$ 10.00	\$ 1,080.00
30		12"	BANDS		\$ 10.00	\$ 300.00



10

Worcester County
DEPARTMENT OF PUBLIC WORKS
6113 TIMMONS ROAD
SNOW HILL, MARYLAND 21863

JOHN H. TUSTIN, P.E.
DIRECTOR

JOHN S. ROSS, P.E.
DEPUTY DIRECTOR

TEL: 410-632-5623
FAX: 410-632-1753

MEMORANDUM

TO: Harold L. Higgins, Chief Administrative Officer
FROM: John H. Tustin, P.E., Director *JHT*
DATE: September 25, 2018
SUBJECT: Holly Grove Road – Speed Study

DIVISIONS

MAINTENANCE
TEL: 410-632-3766
FAX: 410-632-1753

ROADS
TEL: 410-632-2244
FAX: 410-632-0020

SOLID WASTE
TEL: 410-632-3177
FAX: 410-632-3000

FLEET MANAGEMENT
TEL: 410-632-5675
FAX: 410-632-1753

WATER AND WASTEWATER
TEL: 410-641-5251
FAX: 410-641-5185

.....
The Department conducted a speed study on Holly Grove Road from Thursday, June 14, 2018 thru Thursday, June 21, 2018 at the request of an area resident to address safety concerns/speeding vehicles and to request for a speed limit sign to be posted. Additional complaints have been received from time to time as Holly Grove Road has now become a cut through to MD 611 resulting in a huge increase of summer traffic given the use of GPS technology. A copy of the study is attached.

The results of the study combined both north bound and south bound traffic and are as follows:

- Number of Vehicles: 14,472
- Average Speed: 37 mph
- 85th Percentile: 42 mph
- Vehicles 36 mph and greater: 8,632 or 59.7%
- Vehicles >1 mph to 35 mph: 5,840 or 40.3%

Currently Holly Grove Road is not posted; therefore, a speed limit of 50 mph governs. Based on the study, 40.3% of all traffic traveled less than 36 mph and 59.7% of all traffic traveled more than 35 mph. It is therefore recommended that Holly Grove Road be posted at 40 mph. As a side note, Samuel Bowen Boulevard on the north end of HGR is posted at 35 mph and Sinexent to the south is 40 mph. This posting would provide a more uniform speed limit throughout this geographical area. Enforcement activity by the Sheriff's Department should be encouraged.

Should you have any questions regarding this study I will be happy to discuss them with you.

Attachment

cc: Frank Adkins

Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Thursday

North, South

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	85th Percent	95th Percent
06/14/18	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	0	0	0	6	31	29	15	5	3	0	0	0	0	0	89	43	48
07:00	1	0	1	3	45	33	22	2	1	0	0	0	0	0	108	41	44
08:00	0	0	2	21	39	33	16	3	0	0	0	0	0	0	114	40	44
09:00	1	0	3	19	35	41	20	3	1	0	0	0	0	0	123	41	44
10:00	3	0	2	16	32	43	20	7	0	0	0	0	0	0	123	42	45
11:00	1	0	0	6	38	41	30	7	2	1	0	0	0	0	126	43	47
12 PM	1	0	1	13	34	42	34	5	1	0	0	0	0	0	131	42	44
13:00	0	1	2	6	45	48	17	9	3	0	0	0	0	0	131	42	48
14:00	0	0	0	3	37	43	27	3	4	0	0	0	0	0	117	43	46
15:00	0	0	1	9	24	33	18	3	1	0	1	0	0	0	90	42	45
16:00	0	0	0	11	17	30	14	2	0	0	0	0	0	0	74	41	44
17:00	1	0	1	6	22	20	6	2	1	0	0	0	0	0	59	40	45
18:00	0	0	2	0	9	7	5	3	0	0	0	0	0	0	26	44	47
19:00	0	0	1	4	2	6	1	0	1	1	0	0	0	0	16	42	56
20:00	0	0	0	3	3	0	1	0	0	0	0	0	0	0	7	34	43
21:00	0	0	0	2	1	6	0	0	1	0	0	0	0	0	10	39	52
22:00	0	0	1	0	1	0	1	0	0	0	0	0	0	0	3	42	44
23:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	29	29
Total	8	1	17	130	415	455	247	54	19	2	1	0	0	0	1349		
Percent	0.6%	0.1%	1.3%	9.6%	30.8%	33.7%	18.3%	4.0%	1.4%	0.1%	0.1%	0.0%	0.0%	0.0%			
AM Peak	10:00		09:00	08:00	07:00	10:00	11:00	10:00	06:00	11:00							11:00
Vol.	3		3	21	45	43	30	7	3	1							126
PM Peak	12:00	13:00	13:00	12:00	13:00	13:00	12:00	13:00	14:00	19:00	15:00						12:00
Vol.	1	1	2	13	45	48	34	9	4	1	1						131

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Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Friday

North, South

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	999	Total	85th Percent	95th Percent
06/15/18	0	0	1	0	1	0	0	1	0	0	0	0	0	0	0	3	47	49
01:00	0	0	1	0	3	1	2	1	1	0	0	0	0	0	0	9	48	52
02:00	0	0	0	1	1	9	2	1	1	0	0	0	0	0	0	15	44	51
03:00	1	0	0	4	11	16	14	6	2	1	0	0	0	0	0	55	45	50
04:00	0	0	0	4	17	25	29	14	6	2	1	0	0	0	0	98	47	53
05:00	0	0	2	5	24	45	16	7	2	0	0	0	0	0	0	101	43	47
06:00	3	0	2	19	43	51	22	3	1	0	0	0	0	0	0	144	41	44
07:00	0	0	0	10	36	54	21	9	1	0	0	0	0	0	0	131	42	46
08:00	2	0	1	15	51	67	22	2	0	0	0	0	0	0	0	160	39	43
09:00	0	0	2	16	44	51	21	4	0	1	0	0	0	0	0	139	41	44
10:00	1	0	0	16	50	44	28	11	2	1	0	0	0	0	0	153	43	47
11:00	1	0	0	12	46	48	30	10	1	0	0	0	0	0	0	148	43	46
12 PM	2	0	0	5	59	53	35	10	3	0	0	0	0	0	0	167	43	47
13:00	3	3	1	4	46	64	39	9	0	0	0	0	0	0	0	169	42	45
14:00	0	0	0	6	39	39	27	6	3	0	0	0	0	0	0	120	43	47
15:00	2	0	0	4	25	40	23	9	3	0	0	0	0	0	0	106	44	48
16:00	1	0	1	11	22	28	18	2	2	1	1	0	0	0	0	87	43	49
17:00	0	0	0	10	21	23	9	2	1	1	0	0	0	0	0	67	41	46
18:00	0	0	3	4	12	16	2	2	1	0	0	0	0	0	0	40	39	47
19:00	0	0	0	2	12	13	4	1	0	0	1	0	0	0	0	33	41	46
20:00	0	0	0	2	7	3	9	1	0	0	0	0	0	0	0	22	43	44
21:00	0	0	1	0	2	2	2	1	0	0	0	0	0	0	0	8	44	47
22:00	1	1	1	3	2	5	0	1	0	0	0	0	0	0	0	14	38	46
23:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	34	34
Total	17	4	16	153	575	697	375	113	30	7	3	0	0	0	0	1990		
Percent	0.9%	0.2%	0.8%	7.7%	28.9%	35.0%	18.8%	5.7%	1.5%	0.4%	0.2%	0.0%	0.0%	0.0%				
AM Peak	06:00		05:00	06:00	08:00	08:00	11:00	04:00	04:00	04:00	04:00					08:00		
Vol.	3		2	19	51	67	30	14	6	2	1					160		
PM Peak	13:00	13:00	18:00	16:00	12:00	13:00	13:00	12:00	12:00	16:00	16:00					13:00		
Vol.	3	3	3	11	59	64	39	10	3	1	1					169		

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Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Saturday
North, South

Start Time	15	20	25	30	35	40	45	50	55	60	65	70	75	999	Total	85th Percent	95th Percent
06/16/18	0	0	0	0	1	4	1	0	0	0	0	0	0	0	6	40	43
01:00	1	0	0	1	0	3	0	1	0	0	0	0	0	0	6	45	48
02:00	0	0	1	3	8	8	8	1	0	0	0	0	0	0	30	43	47
03:00	1	0	0	3	5	22	21	8	1	0	2	0	0	0	63	45	49
04:00	0	0	0	7	14	22	25	10	3	3	0	0	0	0	84	46	52
05:00	3	0	0	9	46	48	25	14	1	0	1	0	0	0	147	43	48
06:00	6	1	1	15	91	141	46	7	2	0	0	0	0	0	310	40	44
07:00	31	19	20	74	144	234	76	7	1	0	0	0	0	0	606	39	43
08:00	26	4	6	69	227	249	64	5	1	0	0	0	0	0	651	39	42
09:00	7	0	1	51	144	142	63	11	2	1	0	0	0	0	422	41	44
10:00	8	0	1	20	54	119	71	21	3	0	0	0	0	0	297	43	47
11:00	1	0	0	8	53	69	43	17	1	0	0	0	0	0	192	43	47
12 PM	2	0	2	10	42	69	34	3	4	1	0	0	0	0	167	42	44
13:00	1	0	0	15	62	59	26	3	5	0	0	0	0	0	171	41	44
14:00	2	0	2	14	49	48	28	10	2	0	0	0	0	0	155	42	47
15:00	0	0	1	5	35	32	24	10	0	3	0	1	0	0	111	44	49
16:00	0	0	0	7	27	35	22	8	0	1	0	1	0	0	101	43	48
17:00	0	0	2	5	29	33	10	3	0	1	0	0	0	0	83	40	44
18:00	2	0	1	10	10	13	4	7	0	0	0	0	0	0	47	44	48
19:00	0	0	2	1	5	9	7	2	4	0	0	0	0	0	30	48	53
20:00	0	0	0	1	5	5	4	1	1	0	0	0	0	0	17	44	50
21:00	0	0	1	0	0	3	0	0	0	0	0	0	0	0	4	39	39
22:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	3	42	44
23:00	0	0	0	1	2	0	0	1	0	0	0	0	0	0	4	47	49
Total	91	24	41	329	1054	1368	603	150	32	10	3	2	0	0	3707		
Percent	2.5%	0.6%	1.1%	8.9%	28.4%	36.9%	16.3%	4.0%	0.9%	0.3%	0.1%	0.1%	0.0%	0.0%			
AM Peak	07:00	07:00	07:00	07:00	08:00	08:00	07:00	10:00	04:00	04:00	03:00				08:00		
Vol.	31	19	20	74	227	249	76	21	3	3	2				651		
PM Peak	12:00		12:00	13:00	13:00	12:00	12:00	14:00	13:00	15:00		15:00			13:00		
Vol.	2		2	15	62	69	34	10	5	3		1			171		

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Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road

Latitude: 0' 0.0000 Undefined

Sunday

North, South

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	999	Total	85th Percent	95th Percent
	15	20	25	30	35	40	45	50	55	60	65	70	75					
06/17/18	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	37	39
01:00	0	0	0	1	0	1	2	1	0	0	0	0	0	0	0	5	46	48
02:00	0	0	0	2	4	5	5	4	2	0	0	0	0	0	0	22	48	52
03:00	1	0	2	2	14	10	8	4	1	0	1	0	0	0	0	43	44	49
04:00	2	0	0	3	16	24	14	5	1	1	0	0	0	0	0	66	43	48
05:00	1	0	1	6	24	49	31	10	2	1	0	0	0	0	0	125	44	48
06:00	4	0	2	17	48	65	26	4	3	0	0	0	0	0	0	169	41	44
07:00	3	0	0	29	65	75	35	8	0	1	0	0	0	0	0	216	41	44
08:00	0	0	2	24	82	71	24	1	1	0	0	0	0	0	0	205	39	43
09:00	1	0	0	18	54	50	18	12	1	0	0	0	0	0	0	154	42	47
10:00	2	0	2	13	50	44	31	13	1	0	0	0	0	0	0	156	43	47
11:00	0	0	1	10	45	60	24	13	1	0	0	0	0	0	0	154	43	47
12 PM	1	1	2	0	51	46	20	4	4	0	0	0	0	0	0	129	42	46
13:00	0	0	2	4	46	44	22	5	0	0	0	0	1	0	0	124	42	44
14:00	1	0	0	7	32	40	17	7	1	0	0	0	0	0	0	105	42	46
15:00	0	0	1	7	26	34	11	5	1	0	0	1	0	0	0	86	42	47
16:00	0	0	0	3	33	25	16	3	1	0	0	1	0	0	0	82	42	46
17:00	0	0	1	4	19	16	9	2	1	0	0	0	0	0	0	52	42	46
18:00	0	0	0	3	5	11	3	0	0	0	0	0	0	0	0	22	39	43
19:00	0	0	1	0	6	8	1	1	0	0	0	0	0	0	0	17	39	45
20:00	0	0	0	2	5	4	3	2	0	0	0	0	0	0	0	16	44	47
21:00	0	0	0	1	1	2	3	0	0	0	0	0	0	0	0	7	43	44
22:00	0	0	0	1	1	0	1	1	0	0	0	0	0	0	0	4	47	49
23:00	0	0	0	0	0	1	0	1	0	0	0	1	0	0	0	3	67	69
Total	16	1	17	157	629	686	324	106	21	3	1	3	1	0		1965		
Percent	0.8%	0.1%	0.9%	8.0%	32.0%	34.9%	16.5%	5.4%	1.1%	0.2%	0.1%	0.2%	0.1%	0.0%				
AM Peak	06:00		03:00	07:00	08:00	07:00	07:00	10:00	06:00	04:00	03:00					07:00		
Vol.	4		2	29	82	75	35	13	3	1	1					216		
PM Peak	12:00	12:00	12:00	14:00	12:00	12:00	13:00	14:00	12:00			15:00	13:00			12:00		
Vol.	1	1	2	7	51	46	22	7	4			1	1		129			

Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Monday
North, South

Start Time	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	999	Total	85th Percent	95th Percent
06/18/18	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	
01:00	0	0	0	0	1	0	1	1	1	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	47	49	
02:00	0	0	2	4	5	8	3	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	24	42	49		
03:00	1	0	0	0	13	17	15	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	52	44	48		
04:00	0	0	0	5	18	22	15	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	71	45	51		
05:00	1	0	1	1	19	26	16	11	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	78	46	49		
06:00	1	0	0	11	45	49	22	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	134	41	44		
07:00	1	0	1	15	41	44	30	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	137	42	44		
08:00	2	1	1	16	68	53	26	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	174	41	44		
09:00	1	0	2	18	36	52	17	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	131	40	44		
10:00	0	0	1	17	39	34	19	5	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	117	42	46		
11:00	1	0	3	15	27	49	20	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	126	43	48		
12 PM	2	0	2	17	43	50	29	11	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	155	43	46		
13:00	1	0	1	16	40	49	20	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	134	41	45		
14:00	1	0	0	7	37	35	32	8	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	123	43	48		
15:00	1	0	1	7	30	44	16	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	108	42	47		
16:00	0	0	0	7	26	24	8	6	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	74	43	49		
17:00	0	0	0	6	17	18	13	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	58	43	46		
18:00	0	0	0	2	17	12	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	39	41	44		
19:00	1	0	0	2	5	9	3	3	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24	45	49		
20:00	0	0	0	1	2	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	43	52		
21:00	0	0	0	0	3	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	42	44		
22:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*	
23:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34		
Total	14	1	15	168	533	602	315	98	25	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1781				
Percent	0.8%	0.1%	0.8%	9.4%	29.9%	33.8%	17.7%	5.5%	1.4%	0.4%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					
AM Peak	08:00	08:00	11:00	09:00	08:00	08:00	07:00	05:00	04:00	02:00																	08:00			
Vol.	2	1	3	18	68	53	30	11	4	1																	174			
PM Peak	12:00		12:00	12:00	12:00	12:00	14:00	12:00	15:00	14:00	14:00	14:00															12:00			
Vol.	2		2	17	43	50	32	11	3	2	1																155			

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Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Tuesday

North, South

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	85th Percent	95th Percent
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
06/19/18	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*
01:00	0	0	0	2	2	3	4	1	0	0	0	0	0	0	12	43	46
02:00	0	0	1	0	8	5	8	2	0	0	0	0	0	0	24	43	46
03:00	1	0	3	5	12	17	7	7	4	1	0	0	0	0	57	47	52
04:00	1	0	0	2	21	17	24	5	2	0	0	0	0	0	72	44	48
05:00	0	0	2	4	34	36	29	5	3	0	0	0	0	0	113	43	47
06:00	0	0	1	11	33	42	27	3	0	0	0	0	0	0	117	42	44
07:00	0	0	1	17	44	30	17	5	0	0	0	0	0	0	114	41	44
08:00	1	0	2	14	52	49	20	4	0	0	0	0	0	0	142	40	44
09:00	3	0	2	19	45	41	17	4	0	0	0	0	0	0	131	40	44
10:00	2	0	0	13	39	49	20	6	2	0	0	0	0	0	131	42	46
11:00	0	0	2	14	28	49	23	3	1	0	0	0	0	0	120	41	44
12 PM	2	0	0	9	45	45	30	11	4	0	0	0	0	0	146	43	48
13:00	0	0	1	6	30	41	23	6	4	0	1	0	0	0	112	43	49
14:00	3	0	0	5	29	52	22	7	3	0	0	0	0	0	121	43	47
15:00	0	0	1	4	26	34	12	6	0	0	0	0	0	0	83	42	46
16:00	0	0	0	4	18	25	13	4	1	0	0	0	0	0	65	43	47
17:00	0	0	1	3	23	19	9	4	0	0	0	0	0	0	59	42	46
18:00	0	1	2	7	7	12	3	1	0	0	0	0	0	0	33	39	43
19:00	0	0	0	3	2	7	5	0	0	0	0	0	0	0	17	42	44
20:00	0	0	0	0	0	6	3	1	1	0	0	0	0	0	11	46	52
21:00	0	0	0	1	0	1	3	3	0	0	0	0	0	0	8	48	49
22:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	39	39
23:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	34	34
Total	13	1	19	143	499	581	319	88	25	1	1	0	0	0	1690		
Percent	0.8%	0.1%	1.1%	8.5%	29.5%	34.4%	18.9%	5.2%	1.5%	0.1%	0.1%	0.0%	0.0%	0.0%			
AM Peak	09:00		03:00	09:00	08:00	08:00	05:00	03:00	03:00	03:00					08:00		
Vol.	3		3	19	52	49	29	7	4	1					142		
PM Peak	14:00	18:00	18:00	12:00	12:00	14:00	12:00	12:00	12:00		13:00				12:00		
Vol.	3	1	2	9	45	52	30	11	4		1			146			

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Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
410-632-2244

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
Latitude: 0' 0.0000 Undefined

Wednesday

North, South	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	85th Percent	95th Percent
Start Time	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
06/20/18	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	34	34
01:00	0	0	0	1	1	1	2	2	1	0	0	0	0	0	8	49	52
02:00	0	0	0	0	3	9	3	3	1	0	0	0	0	0	19	46	50
03:00	1	0	1	3	13	22	20	9	4	0	0	0	0	0	73	46	50
04:00	0	0	0	2	19	35	19	5	1	1	0	0	0	0	82	43	47
05:00	1	0	1	8	28	35	26	7	2	1	0	0	0	0	109	43	48
06:00	2	0	1	9	35	43	21	9	0	0	0	0	0	0	120	42	46
07:00	0	0	0	6	41	56	21	2	0	0	0	0	0	0	126	40	43
08:00	1	0	0	14	43	52	22	3	0	0	0	0	0	0	135	41	44
09:00	0	1	7	25	59	49	10	8	0	0	0	0	0	0	159	39	45
10:00	1	0	1	8	41	46	23	12	2	0	0	0	0	0	134	43	48
11:00	0	1	3	9	41	42	29	10	0	1	0	0	0	0	136	43	47
12 PM	1	0	1	6	39	59	16	4	2	1	0	0	0	0	129	41	45
13:00	1	0	0	4	36	48	21	9	5	0	1	0	0	0	125	44	49
14:00	0	0	1	14	48	45	26	8	2	0	0	0	0	0	144	42	46
15:00	1	0	1	9	27	41	17	9	0	1	0	0	0	0	106	43	47
16:00	0	0	1	10	22	24	14	6	5	0	1	0	0	0	83	44	51
17:00	0	0	0	3	15	8	8	4	1	0	0	0	0	1	40	44	48
18:00	0	0	0	1	5	20	7	1	0	0	0	0	0	0	34	42	44
19:00	0	0	0	1	6	7	4	2	0	1	0	0	0	0	21	44	49
20:00	0	0	0	0	1	2	1	1	0	0	0	0	0	0	5	46	48
21:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	3	42	44
22:00	0	0	0	0	0	2	2	0	0	0	0	0	0	0	4	43	44
23:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	3	37	39
Total	9	2	19	134	526	648	313	114	26	6	2	0	0	1	1800		
Percent	0.5%	0.1%	1.1%	7.4%	29.2%	36.0%	17.4%	6.3%	1.4%	0.3%	0.1%	0.0%	0.0%	0.1%			
AM Peak	06:00	09:00	09:00	09:00	09:00	07:00	11:00	10:00	03:00	04:00					09:00		
Vol.	2	1	7	25	59	56	29	12	4	1					159		
PM Peak	12:00		12:00	14:00	14:00	12:00	14:00	13:00	13:00	12:00	13:00			17:00	14:00		
Vol.	1		1	14	48	59	26	9	5	1	1			1	144		

Worcester County DPW - Roads Division

5764 Worcester Highway
Snow Hill, MD 21863
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Thursday

Site Code:
Station ID:
Holly Grove Road
Holly Grove Road
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North, South

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	85th Percent	95th Percent
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
06/21/18	0	0	0	0	2	0	1	0	0	0	0	0	0	0	3	42	44
01:00	0	0	0	1	1	2	1	1	1	0	0	0	0	0	7	49	53
02:00	0	0	0	1	5	10	4	4	0	0	0	0	0	0	24	45	48
03:00	0	0	1	1	17	20	15	9	1	1	0	0	0	0	65	45	49
04:00	0	0	3	2	15	36	18	14	3	0	0	0	0	0	91	46	49
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	0	0	4	5	40	68	39	28	5	1	0	0	0	0	190		
Percent	0.0%	0.0%	2.1%	2.6%	21.1%	35.8%	20.5%	14.7%	2.6%	0.5%	0.0%	0.0%	0.0%	0.0%			
AM Peak			04:00	04:00	03:00	04:00	04:00	04:00	04:00	03:00					04:00		
Vol.			3	2	17	36	18	14	3	1					91		
PM Peak																	
Vol.																	
Total	168	34	148	1219	4271	5105	2535	751	183	38	13	5	1	1	14472		
Percent	1.2%	0.2%	1.0%	8.4%	29.5%	35.3%	17.5%	5.2%	1.3%	0.3%	0.1%	0.0%	0.0%	0.0%			

15th Percentile : 30 MPH
50th Percentile : 36 MPH
85th Percentile : 42 MPH
95th Percentile : 46 MPH

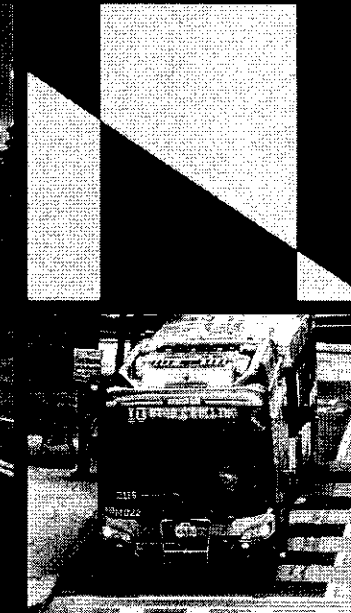
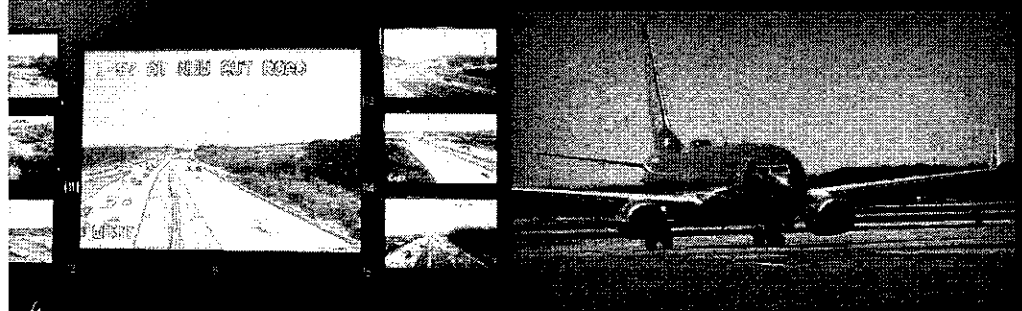
Stats
10 MPH Pace Speed : 31-40 MPH
Number in Pace : 9376
Percent in Pace : 64.8%
Number of Vehicles > 55 MPH : 58
Percent of Vehicles > 55 MPH : 0.4%
Mean Speed(Average) : 37 MPH

FY2019 to FY2024

Consolidated Transportation Program

DRAFT

Larry Hogan, **Governor** | Boyd K. Rutherford, **Lt. Governor** | Pete K. Rahn, **Secretary**



MARYLAND'S CONSOLIDATED TRANSPORTATION PROGRAM

The Consolidated Transportation Program (CTP) is Maryland's six-year capital budget for transportation projects. The Capital Program includes major and minor projects for the Maryland Department of Transportation (MDOT) business units; the Transportation Secretary's Office (MDOT TSO), the Maryland Aviation Administration (MDOT MAA), Maryland Port Administration (MDOT MPA), Motor Vehicle Administration (MDOT MVA), State Highway Administration (MDOT SHA), Maryland Transit Administration (MDOT MTA) – and related authorities to the MDOT, including the Maryland Transportation Authority (MDTA) and the Washington Metropolitan Area Transit Authority (WMATA).

In this document, you will find a Project Information Form (PIF) for every major project, which includes project details, financial information and construction status as well as a list of minor capital projects. MDOT works together with residents, local jurisdictions, and local and State elected officials to include projects in the CTP that preserve investments, enhance transportation services, and improve accessibility throughout the State. In order to help Maryland's citizens review this document, the CTP includes a summary of MDOT's financing and budgeting process and instructions for reading PIFs.

MDOT ensures nondiscrimination and equal employment in all programs and activities in accordance with Title VI and Title VII of the Civil Rights Act of 1964. If you need more information or special assistance for persons with disabilities or limited English proficiency, contact MDOT's Office of Diversity and Equity at 410-865-1397.

For the hearing impaired, Maryland Relay 711.

For further information about this document or to order a hard copy, please contact Ms. Dawn Thomason at the Maryland Department of Transportation, Office of Planning and Capital Programming toll free at 1-888-713-1414, or locally at 410-865-1288. This document also is available online at: www.ctp.maryland.gov.

For more information on Maryland transportation, please visit us on the web at www.mdot.maryland.gov.

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Highlights of the 2019 State Report on Transportation

Larry Hogan, **Governor** | Boyd K. Rutherford, **Lt. Governor** | Pete K. Rahn, **Secretary**

A Welcome Message from Governor Larry Hogan

**"Maryland is open
for business!"**

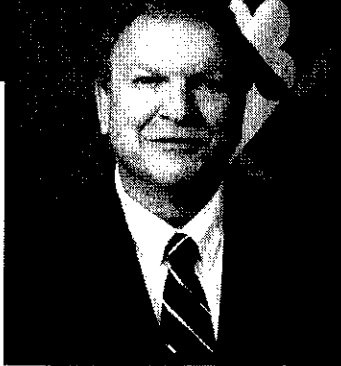
Maryland's transportation system is an economic driver for our State and our Administration has made the commitment to ensure this system is the best in the nation. It is critical that Maryland develop and maintain a transportation system that gets millions of users to their destinations efficiently and safely. Through a commitment of \$16 billion in transportation projects across the State, Maryland is improving the travel experience whether you drive, walk, ride, cruise, or fly. Through transportation investments from the Eastern Shore, such as MD 404, to western Maryland, such as I-81 and US 219, we are creating a safer, more efficient transportation network for all riders.

I am excited about the Traffic Relief Plan that would add new express lanes to I-270, I-495 (Capital Beltway), and MD 295 (Baltimore-Washington Parkway) in the Washington area, leaving the existing lanes untolled. These managed lanes will provide drivers with the choice to pay for a quicker trip, simultaneously reducing delays for those who choose to stay in the existing free lanes. The Traffic Relief Plan in the Baltimore area will extend the Express Toll Lanes (ETLs) on I-95, reconfigure the interchange between I-695 (Baltimore Beltway) and I-70, and improve I-695 through innovative congestion relief efforts. These improvements to our most congested roadways are critical to spur increased economic development and restore quality of life for countless Marylanders who have been negatively affected by years of traffic congestion. Our Administration continues to pursue a balanced approach to address transportation demands, including the construction of the Purple Line transitway project in Montgomery and Prince George's Counties and Baltimore's recent completion of BaltimoreLink. We have also dedicated funding for the Washington Metropolitan Area Transit Authority (WMATA) along with Virginia and Washington, D.C.

Maryland is generating increased interest from national and international transportation users. This past year, the Helen Delich Bentley Port of Baltimore experienced record growth in cargo due to our ability to now handle larger container vessels as well as other improvements. The Baltimore Washington Thurgood Marshall Airport also continues to serve an ever expanding group of passengers who are drawn to the enhanced amenities at the Airport and the ease passengers experience flying in and out of Maryland.

MDOT continues to ensure the transportation system operates efficiently through participation in our Administration's Customer Service initiative that includes improvements in the way MDOT provides the highest quality customer service. Making E-Z Pass transponders free will put \$46 million back in Marylander's pockets. We also reached an incredible milestone for our State when the Coordinated Highway Action Response Team (CHART) assisted its 1 millionth driver on Maryland highways in January 2018.

Our Administration has gained momentum through the achievements we have made in roadway, transit, airport and port with 816 MDOT projects underway at \$8.8 billion. We are making it easier for our citizens to do business in Maryland and benefiting their daily commutes as well as affecting the State's economy. We will continue to be a place where people want to live and do business because we have a first-rate transportation system.



A Message from Secretary of Transportation Pete K. Rahn

Over the past three years, the Maryland Department of Transportation (MDOT) has delivered substantial improvements to our transportation system due to the commitment and support of Governor Hogan. As Maryland continues to grow and prosper, we are committed to actively address future transportation challenges. We are now in the second year of the largest construction program in MDOT's history and making great strides with new roads and

bridges that are reducing congestion and improving safety. Most notably, over the past three years, Maryland has rehabilitated all 69 bridges that were identified as structurally deficient in 2015.

At MDOT, we provide a balanced approach to transportation. This includes the implementation of the Governor's Traffic Relief Plan, the BaltimoreLink improvements and the implementation of the Purple Line. Of course, we continue to promote the Commuter Choice Maryland program, helping you find opportunities to work with your employers to promote rideshare, telework, flexible work hours, etc. Every day we make progress toward our goal of a transportation system that not only meets, but exceeds the expectations of the traveling public due to the hard work and dedication of the MDOT employees.

Safety is an important focus for MDOT. After several years of decline, we are now seeing increases in the number of traffic related fatalities on our roadways. We all must commit to traveling more safely on Maryland's roadways.

MDOT strives to make our transportation system the best in the nation, as this also improves our economy. Technological advances will play a key role in shaping our transportation system in the future, whether it is through drone technology, connected and automated vehicles, electric vehicle infrastructure, or new ways to move through the transportation system.

At the heart of all that we do is the customer - those millions of people who use our transportation system every day. We want to continue to make that a positive experience. The Consolidated Transportation Program (CTP) outlines how we hope to meet the demands on the transportation system. We hope you take the time to review the CTP and provide your input.

Pete K. Rahn



MISSION STATEMENT

"The Maryland Department of Transportation is a customer-driven leader that delivers safe, sustainable, intelligent, and exceptional transportation solutions in order to connect our customers to life's opportunities."

4

Who is MDOT?

The Maryland Department of Transportation (MDOT) is unique among state DOTs in its ability to deliver a comprehensive transportation system and experience to the people and businesses it serves. MDOT houses all of the State's transportation agencies in one organization, ensuring the interconnectivity of State highways, toll facilities, transit, aviation, ports, and motor vehicle and driver services. Implementation of MDOT One Stop Shop (mdotonestopshop.maryland.gov), a website that puts transportation network information in one central location, enhanced this interconnectivity. The website serves to improve user experience in all modes of transportation and to make MDOT information more accessible to the public.

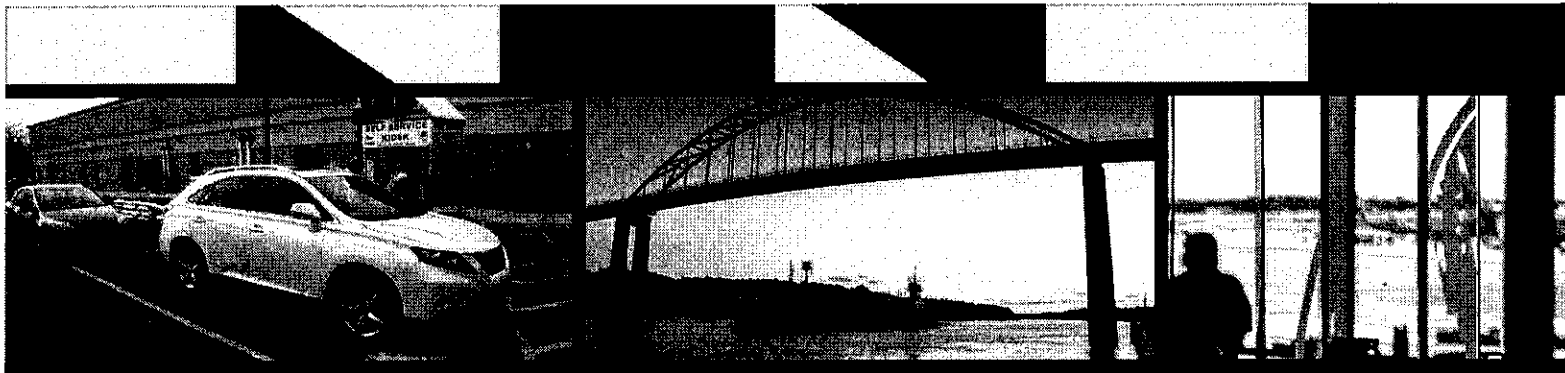
This organization is ONE MDOT instead of six separate entities; one Department with everyone working together towards the mission of ensuring that the Maryland Department of Transportation is a customer-driven leader that delivers safe, sustainable, intelligent, and exceptional transportation solutions in order to connect our customers to life's opportunities. The MDOT Secretary serves as Chairman of the Maryland Transportation Authority (MDTA), which owns, operates and maintains the State's eight toll facilities. While the Washington Metropolitan Area Transit Authority (WMATA) is not part of MDOT, the Governor appoints two Maryland WMATA Board members and MDOT staff work closely with those appointees and the other Board members to ensure efficient and effective transit services in the metropolitan Washington region.

The MDOT manages the State Transportation Trust Fund, which gives Maryland the flexibility to fund priority transportation needs that support a balanced statewide multimodal transportation system. This capability provides economic benefits to the entire State, creating jobs, expanding opportunities, and advancing transportation plans, programs, and projects that will improve our transportation system and the quality of life for Marylanders. Toll revenues fund MDTA and not the Transportation Trust Fund or General Funds.

MDOT – Continuing A Tradition of Quality

MDOT was recognized by the Maryland Quality Initiative (MdQI) in 2018 for its commitment to quality as the recipient of 14 Awards of Excellence given to transportation professionals for planning studies, programs and construction projects completed in 2017. For the past 25 years, MdQI has served as a forum to bring public and private sector partners together with a shared vision of improving Maryland's transportation system. The MDOT Maryland Aviation Administration (MDOT MAA) received two awards for a connector project between Concourses C and D and a new security checkpoint at BWI Thurgood Marshall Airport. The MDOT State Highway Administration (MDOT SHA) received an award for its project to replace the deck (riding surface) on the MD 546 Bridge over I-68 in Garrett County.





Safety is First at MDOT

MDOT Employees Sign Safety Pledge

Secretary Pete Rahn issued a challenge to the more than 10,000 MDOT employees to take the traffic safety pledge and agree to follow the State's highway safety guidelines when they drive, ride, or walk. MDOT's mission specifies the agency is "a customer-driven leader that delivers safe, sustainable, intelligent, and exceptional transportation solutions." Recent increases in traffic-related deaths prompted the Secretary to announce the pledge and to make sure that traffic safety starts at home among the individuals working for MDOT. That is why he urged every employee to lead by example and demonstrate safety behavior by following some simple, common sense rules:

- Have a safe and sober ride ... a designated driver, cab, ride share, or public transportation.
- Park the phone before you drive. Distractions lead to more than 28,000 injuries per year in Maryland.
- Always buckle up! It's the single most important way to save your life in a crash.
- Adapt your driving. Leave a little early. You won't feel the need to speed or drive aggressively.
- Look twice for pedestrians, bicyclists, and motorcyclists, especially when changing lanes or approaching an intersection.
- Use crosswalks and bike lanes. Be visible on the road.
- Move over when approaching an emergency vehicle or tow truck using visual signals. If you are unable to move over, slow down.

The reason for the initiative is the increase in traffic fatalities in Maryland from 522 in 2016 to 547 in 2017. MDOT would like each and every Maryland citizen to accept this challenge as well, and follow the rules above when traveling on Maryland roadways. For more information, see the safety pledge at <https://www.surveymonkey.com/r/MDOTsafetypledge>.

Local Road Plans Address Traffic Crashes Where They Happen

MDOT promotes traffic safety through the many infrastructure and behavioral programs and projects implemented by the MDOT SHA and MDOT Motor Vehicle Administration's (MVA) Highway Safety Office, which is the lead agency for the State's Strategic Highway Safety Plan (SHSP). The SHSP is a statewide, comprehensive safety plan that provides a coordinated framework for reducing deaths and severe injuries on all public roads. Through extensive outreach and involvement of federal, state, local, and private sector safety stakeholders, the plan establishes statewide goals and critical emphasis areas. As an outgrowth of this initiative, many local communities are developing local road safety plans using the SHSP as a guide. The Federal Highway Administration (FHWA) has designated this program as a proven countermeasure because it tailors solutions to local issues and needs. Secretary Rahn sent a letter to all county officials in the State urging them to take on this effort and help build "a culture of safety" in Maryland. So far, two counties (Prince George's and Washington) have fully completed plans with two more nearing completion by the end of 2018 (Harford and Cecil). Thirteen other jurisdictions have expressed interest. Montgomery County has a Vision Zero Action Plan.

6

Maryland's Transportation Long Range Plan and Project Selection



The Maryland Transportation Plan (MTP) establishes a 20-year vision for transportation in the State and provides policy direction through statewide multimodal goals and objectives. The MTP outlines the State's overarching transportation priorities and helps define transportation decision-making within this larger context. In turn, the MTP guides the development of the CTP, which identifies specific road, bridge, transit, aviation, port, pedestrian and bikeway projects that will be funded over the next six years, following specific goals, objectives, and strategies. Maryland's transportation business units use the MTP to guide plans, policies and priorities throughout the State.

The MTP is also used to develop performance measures to assess how effectively the Department is performing its mission. The 2040 MTP will include a system performance report that will describe MDOT's targets for safety, freight, bridges, and transit asset management. MDOT updates the MTP every five years to address current and future transportation challenges and conditions. The MTP is updated through extensive consultation and coordination with State, regional and local government stakeholders and outreach to Maryland's citizens. Between August 2017 and August 2018, the Department conducted outreach to Maryland's citizens, agencies, and interested organizations to update this long-range vision through surveys, web updates, presentations, videos and email blasts. The MTP goals are to:

- Ensure a Safe, Secure, and Resilient Transportation System – Enhance the safety and security of Maryland's multimodal transportation system and provide a transportation system that is resilient to natural or man-made hazards;
- Maintain a High Standard and Modernize Maryland's Multimodal Transportation System – Preserve, maintain, and modernize the State's existing transportation infrastructure and assets;
- Improve the Quality and Efficiency of the Transportation System to enhance the customer experience – Increase the use of technologies and operational improvements to enhance transportation services and communication to satisfy our customers;
- Provide Better Transportation Choices and Connections – Improve transportation connections to support alternative transportation options for the movement of people and goods;
- Facilitate Economic Opportunity and Reduce Congestion in Maryland through Strategic System Expansion - Invest in and pursue opportunities to promote system improvements that support economic development, reduce congestion, and improve the movement of people and goods;
- Ensure Environmental Protection and Sensitivity – Deliver sustainable transportation infrastructure improvements that protect and reduce impacts to Maryland's natural, historic, and cultural resources; and
- Promote Fiscal Responsibility – Ensure responsible investment and management of taxpayer resources to add value and deliver quality transportation improvements through performance based decision-making and innovative funding mechanisms and partnerships.

Your input into this long-range plan update has been critical in shaping the State's transportation vision and priorities. Please visit the MTP web site at: mdot.maryland.gov/MTP to find out more about the Maryland Transportation Plan. A draft plan will be available in the Fall of 2018 with a final plan completed by January 2019. Inquiries may be directed to 2019MTP@mdot.state.md.us.



Maryland's Bicycle and Pedestrian Master Plan (BPMP)

Maryland develops and regularly updates the Bicycle Pedestrian Master Plan (BPMP) in guiding the future growth of non-motorized transportation throughout the State. The BPMP establishes a 20-year vision for bicycling and walking as an integral part of Maryland's transportation system. Along with the Maryland Transportation Plan (MTP), the BPMP is updated every five years and shares similar goals—transportation choice, multimodal connectivity, safety, equitable and innovative planning and analysis, and economic development. The plan also serves as a guide for local and regional partners on issues related to bicycle and pedestrian policy, accessibility, planning, and investment strategies. Following a year-long process involving data assessments and collection, and including input from a series of public and stakeholder meetings, and regional workshops, a plan is currently being reviewed and key initiatives refined. A draft of the plan is expected to be released in the Fall of 2018. The final version is to be completed in January of 2019. For more information, please visit the BPMP website at http://www.mdot.maryland.gov/newMDOT/Planning/Bike_Walk/Bike_Ped_Plan_Update.html.

Consolidated Transportation Program (CTP)

The CTP is Maryland's six-year capital budget for transportation projects. The CTP includes major and minor transportation projects for the Secretary's Office and MDOT's business units. MDOT works together with residents, businesses, local jurisdictions, and local and state elected officials to include projects in the CTP that preserve investments, enhance transportation services, and improve accessibility throughout the State.

The CTP takes nearly a full year to create through the collaboration and work of MDOT staff with state, regional and local elected officials. Each year, local jurisdictions are encouraged to submit priority project(s) to the Department by April. It is important for MDOT to hear from local jurisdictions to facilitate collaboration on state and local needs. To ensure that every dollar available for transportation in Maryland is spent efficiently, MDOT employs a strategic decision making process, using established priorities and criteria to prioritize programs and projects under consideration for inclusion in the CTP. The full CTP is located at: ctp.maryland.gov.





CTP Development

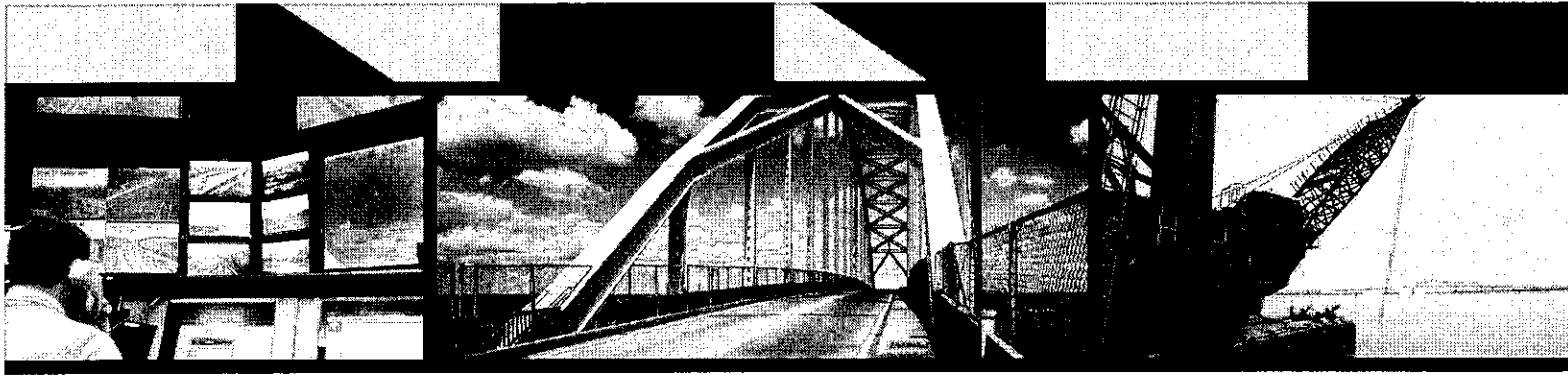
MDOT uses the following criteria to identify projects and programs that align with the State's transportation priorities. These criteria include:

- Meets all federal and other legal mandates (e.g. Total Maximum Daily Load (TMDL) compliance, Positive Train Control (PTC), Federal Aviation Administration (FAA) regulations to maintain airport permits);
- Supports the Department's mission, program priorities and Maryland Transportation Plan (MTP) goals (safety, system preservation, economic development, etc.);
- Meets all federal match requirements to maximize federal revenue sources;
- Supports State plans and objectives;
- Supports existing project commitments and upholds intergovernmental agreements;
- Is the single top priority within a local priority letter;
- Is consistent with local plans; and
- Is included in the regional Metropolitan Planning Organization (MPO) long-range plan (if the project is located within an MPO boundary).

CTP Coordination and Input

The majority of formal local and public input is provided through "county priority letters" and input received during the annual county tour, where the Secretary of Transportation visits each of the counties and Baltimore City to present the draft CTP. MDOT requests that counties submit their priority letters on or around the first of April each year. Priority letters should be endorsed by the commissioner's/council (and/or county executive), as well as the local legislative delegation. Input is also received from the MPOs, generally through the content of their constrained long range transportation plans. In addition, the Department of Budget and Management provides recommendations on the final program to the Secretary in early December as a part of the budget process.





MDOT SHA – Ensuring Quality Road Construction

MDOT SHA has 524 active construction projects underway totaling \$3.95 billion. The MDOT SHA continues to advance the recent Governor Hogan Traffic Relief Plan projects, including:

- \$7.6 billion public-private partnership to add managed lanes to I-495 and I-270 (Montgomery, Prince George's, Frederick Counties);
- \$251 million to convert the shoulder to a travel lane on I-695 from I-70 to MD 43 and upgrade the I-695/I-70 interchange (Baltimore County); and
- \$50.3 million to implement Smart Signals statewide.



Other major project efforts with the total estimated cost include:

- \$119.7 million for the new I-270 interchange at Watkins Mill Road (Montgomery County);
- \$112.1 million innovative project to reduce congestion on I-270 (Frederick and Montgomery Counties);
- \$47.6 million to construct a two-lane roadway along MD 97 around Brookeville (Montgomery County);
- \$61.6 million US 219 realignment project north of I-68 (Garrett County);
- \$131.5 million interchange at MD 4 and Suitland Parkway (Prince George's County);
- \$55.9 million interchange at MD 5 and Brandywine Road (Prince George's County);
- \$105 million widening project along the I-695 Southwest Outer Loop (Baltimore County);
- \$18.3 million MD 140 widening project at Painter's Mill Road (Baltimore County);
- \$47.2 million widening project along MD 2/4 from Fox Run Boulevard to MD 231 (Calvert County);
- \$30.8 million MD 30 Business (Main Street) reconstruction project in Hampstead (Carroll County);
- \$26.2 million widening project at MD 5 to Point Lookout State Park (St. Mary's County);
- \$20.1 million widening project along MD 175 from Disney Road to Reese Road (Anne Arundel County);
- \$24.3 million for a final phase of intersection improvements at US 40 and MD 7/MD 159 (Harford County);
- \$86.5 million reconstruction of the MD 85/I-270 interchange (Frederick County);
- \$100.8 million I-81 widening from US 11 in West Virginia to MD 63/MD 68 in Maryland including the replacement of the dual bridges over the Potomac River;
- \$81 million final phase of the US 113 widening project (Worcester County); and
- \$125.4 million to widen MD 32 from Linden Church Road to I-70 (Howard County).

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Over the past year, MDOT SHA has completed projects to improve safety and operations throughout the State of Maryland, including:

- \$157.8 million widening MD 404 from US 50 to MD 309 and West of Hillsboro Road to Holly Road in record time (Caroline, Queen Anne's, and Talbot Counties);
- \$25 million US 50 Severn River Bridge adding a fourth lane eastbound (Anne Arundel County);
- \$28.2 million replacement of the I-695 Bridge over Milford Mill Road (Baltimore County);
- \$34.6 million widening of I-695 from MD 41 to MD 147 to create a continuous auxiliary lane between the interchanges. The project also included the replacement of the Old Harford Road Bridge (Baltimore County);
- \$65.3 million replacement of the MD 331 Dover Bridge over the Choptank River (Caroline and Talbot Counties);
- \$20.9 million intersection improvement at MD 22 and MD 462 (Harford County);
- \$26.5 million bicycle and pedestrian improvements along MD 4 from Forestville Road to MD 468 (Prince George's County);
- \$49.7 million new interchange at US 301 and MD 304 (Queen Anne's County); and
- \$52.5 million widening of US 113 from Massey Branch to Five Mile Branch (Worcester County).

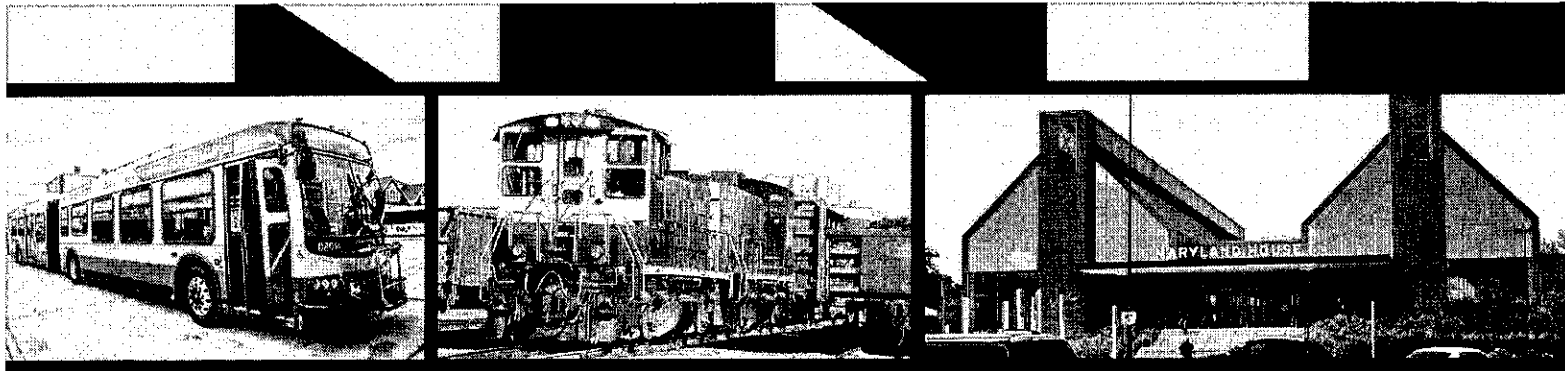
Thanks to practical design and innovative project delivery, MDOT SHA is able to deliver more projects for Marylanders using existing resources. MDOT SHA is increasingly using an innovative cost plus time approach, known as A+B bidding. The A+B bidding considers both the cost and the time to build the improvements to determine the award of a project. This approach minimizes impacts to the traveling public, ensures safety considerations are addressed, and delivers improvements for capacity and economic development faster than a traditional award based solely on low bidding thereby providing better value to the public. The MDOT SHA is also using the Design-Build project delivery method to allow faster project delivery through overlapping design and construction, which results in cost and time savings. The widening of MD 404 on the Eastern Shore used this method.

MDOT MTA – Improving the Journey Through Transit

BaltimoreLink

Launched in June 2017, the BaltimoreLink network provides more people with faster and more reliable access to transit, jobs, and services in the region. Since MDOT Maryland Transit Administration (MTA) launched BaltimoreLink, on-time performance has dramatically improved from 59.5 percent in Fall 2016 to 68 percent in May 2018 – a 14.4 percent improvement. Additionally, MDOT MTA has launched a partnership with the Transit app, which allows customers to receive highly accurate location and arrival information for CityLink, LocalLink, and Express BusLink in the BaltimoreLink fleet. To learn more about Transit app, visit mta.maryland.gov/transit. In partnership with Baltimore City, more than 5.5 miles of dedicated lanes were installed and have improved travel times by up to 25 percent. Transit signal priority sensors on the entire bus fleet and at nearly 50 intersections are improving travel times up to 22 percent. The bus system is safer than ever. Despite providing over 300,000 miles of additional service there have been 20 percent fewer accidents than under the previous system. To learn more about BaltimoreLink, visit baltimorelink.com.





Purple Line

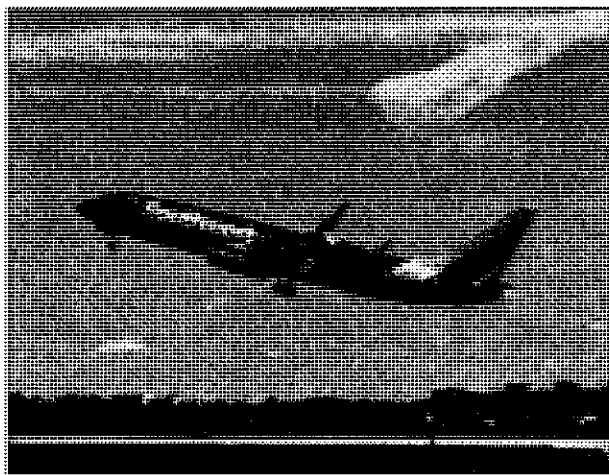
Construction is now underway along the entire 16-mile light rail corridor. Since the project broke ground in August 2017, utility relocation, earthwork, and tunnel excavation have been ongoing. Light rail vehicle production is also underway. MDOT officials signed the \$5.6 billion public-private partnership contract in April 2016 with the Purple Line Transit Partners to design, build, finance, operate, and maintain the light rail system. The Purple Line will run east-west inside the Capital Beltway, with 21 stations connecting to: Metrorail's Orange, Green, and Red lines; the MARC Brunswick, Camden, and Penn lines; and Amtrak at New Carrollton. For more information, visit purplelinemd.com.

Fleet Improvements

MDOT MTA's revenue vehicles are being replaced and overhauled to improve the customer experience with more comfortable, reliable vehicles. MDOT MTA recently purchased 140 clean diesel buses and ensured long-term commitment through a five-year replacement contract. Metro Subway Line vehicles will be replaced starting in the next few years and Light Rail Line vehicles are undergoing a comprehensive overhaul now. Mobility Line vehicles continue to be replaced. MARC received eight locomotives in 2018 and is overhauling 63 of the passenger cars.

MDOT MAA – Serving Customer Needs, Improving Facilities, and Protecting the Environment

In 2017, a record 26.3 million passengers flew through BWI Marshall Airport – 1.4 million more than 2016. Now the 22nd busiest airport in the U.S., and the busiest in the Washington-Baltimore region, the MDOT Maryland Aviation Administration (MDOT MAA) continues to improve facilities and passenger amenities at BWI Marshall Airport. Following the installation of a new International Checked Baggage Inspection System in 2017, a six-gate expansion of the International Concourse will open to the public in Fall 2018.



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To accommodate the approximate 60 percent growth in air cargo activity at BWI Marshall Airport over the last year, a fast-track construction project expanded the Midfield Cargo Apron with six new aircraft parking positions that allowed for a new carrier to begin service in time for peak 2017 holiday season demand. With support from the Maryland Department of the Environment (MDE), MDOT MAA procured 20 new articulated Shuttle Buses powered by Compressed Natural Gas (CNG) for transportation between the BWI Marshall Airport terminal and the Consolidated Rental Car Facility. For those travelers seeking the highest level of convenience, "concierge style" valet parking was launched in the Hourly Garage in early 2018. All these projects continue to make BWI Marshall Airport "The Easy Come, Easy Go Gateway to the Baltimore-Washington Region."

BWI Marshall Airport is a major economic development and transportation resource for the State of Maryland and the entire National Capital region. In January 2018, Governor Larry Hogan announced a new economic impact report which revealed that BWI Marshall Airport produces a total economic impact of \$9.3 billion. The Airport and visitors support 106,000 jobs throughout the region. In 2017, MDOT MAA also contributed more than \$4.1 million to public-use airports across the State through the Statewide Aviation Grant Program. These grants support the flying public with airport improvement and infrastructure preservation projects, safety equipment acquisitions, and environmental compliance activities. This investment leveraged over \$17.7 million in matching Federal Aviation Administration funds, a 36 percent larger share than in 2016.

MDTA – Reducing Tolls and Fees Delivers Nice Bridge and Expanded I-95 ETLs

Fulfilling Governor Hogan's promise to lower tolls and fees, Maryland drivers have saved \$186 million due to reductions in tolls and fees since 2015. This includes more than \$122 million in savings at the Bay Bridge, on the I-95 Express Toll Lanes (ETL), and on the Intercounty Connector (ICC). During this time, traffic on the ICC has grown by 44 percent and traffic on the I-95 ETLs has more than doubled. On Nov. 21, 2016, Governor Hogan announced \$765 million in funding for construction to replace the 77-year old Nice Bridge. The practical design of the project saved MDTA \$200 million. Officials recently advertised a design-build project and anticipate starting construction in 2020 and opening the new wider and safer bridge in early 2023, which will be built north of and parallel to the existing bridge with two lanes of traffic in each direction.



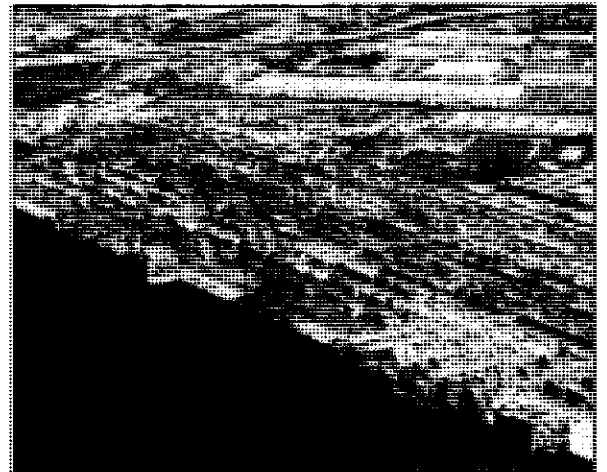
On Aug. 30, 2016, Governor Hogan announced the start of the \$5 million Tier 1 National Environmental Policy Act study, which will identify a preferred corridor alternative and evaluate financial feasibility for a new Chesapeake Bay crossing. The study began in Fall 2016 and will take up to 48 months to complete.

In advance of the I-895 bridge project, MDTA has been reconfiguring travel lanes on a four-mile stretch of I-95 north of the Fort McHenry Tunnel. Once complete by Summer 2018, the \$49.4 million project will expand I-95 from three lanes and merge lanes to four continuous lanes between the tunnel and Moravia Road, allowing for better traffic movement in this heavily traveled corridor and making it a much more viable alternate route. The MDTA also began the \$24 million project to rehabilitate the Curtis Creek Drawbridge in December 2017. Lanes are expected to reopen in November 2018, prior to the start of the I-895 bridge project.



MDOT MPA – A Leading Economic Engine that is Breaking Cargo Records

The MDOT Maryland Port Administration (MDOT MPA) is ensuring Maryland is "Open for Business" by continuing to break cargo records throughout 2018, and maintaining its role as one of Maryland's top economic generators. In 2017, the Port's public and private marine terminals handled 38.4 million tons of cargo. This was the most since 1979 and the third-highest tonnage in its history. Through May 2018, general cargo at the State-owned public marine terminals are 5.3 percent ahead of the same period in 2017. If this trend continues, it will be the third consecutive year the State-owned terminals handled more than 10 million tons of general cargo. Since welcoming its first supersized container ship from the newly expanded Panama Canal in Summer 2016, the Port of Baltimore has had strong growth in containers. The Port of Baltimore is one of only a few U.S. east coast ports with the necessary 50-foot deep channel to handle large vessels. Since welcoming that first large ship in 2016, the Port of Baltimore has seen a 14 percent increase in its container business. The Port of Baltimore set a record in 2017 by handling 807,194 cars and light trucks. It was the first time the Port surpassed 800,000 cars/light trucks and the seventh consecutive year handling more of that cargo commodity than any other U.S. port. The trend for 2018 continues to be higher than previous years.

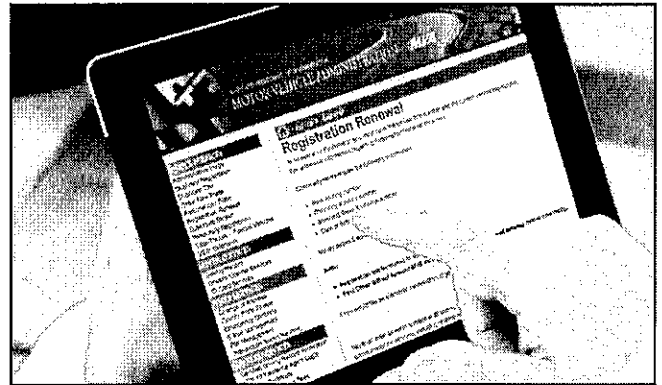


The Port of Baltimore's cruise business continues to be a popular choice with mid-Atlantic cruisers. In 2017, more than 210,000 people sailed on a cruise from Baltimore. With year-round cruises to the Bahamas, Bermuda, and Caribbean offered by Carnival and Royal Caribbean, the Port of Baltimore continues to offer cruise passengers plenty of different vacations of a lifetime. The Port of Baltimore's cruise terminal, operated by the MDOT MPA, has undergone renovations that include new carpeting, restrooms, a "VIP" lounge, ticketing kiosks and a public-address system. Exterior improvements include signage, new entrance gate and vehicular circulation. Passengers returning from their cruise are also now provided a sitting area in a new comfort trailer with vending machines while they wait for their pickup transportation.

Aside from its cargo and cruise responsibilities, the Port of Baltimore has also proven to be environmentally responsible. The MDOT MPA is committed to being a good environmental steward. Its award-winning and nationally renowned dredging program has rebuilt eroded islands in Maryland using sediment removed from channels leading to the Port of Baltimore. There are also green initiatives on the marine terminals that are reducing our carbon footprint. One of these green initiatives, the Port's dray truck replacement program, has resulted in 172 older dray trucks being replaced with newer, cleaner-running engines. The MDOT MPA's green efforts led to the selection of the Port of Baltimore as the first North American host of the 2018 GreenPort Congress in 2018.

MDOT MVA – Delivering Convenience to Customers, Truckers, and Boaters

Under Governor Hogan's leadership and in an effort to continue to offer premier customer service as noted in the Governor's Customer Service Plan, the MDOT Motor Vehicle Administration (MDOT MVA) reduced wait times over the last several years. As of June 2018, the average statewide customer wait time is 16.9 minutes, which is a 40 percent reduction from 2014. MDOT MVA has generated many of the enhancements that support a wait time reduction through employee ideas about how to deliver better customer service. We are engaging all employees at MDOT MVA to bring the best ideas to improve the service we provide every day. Through the launch of our "New Way to MVA" campaign, we have made numerous services conveniently available online that customers typically come into a branch to complete.

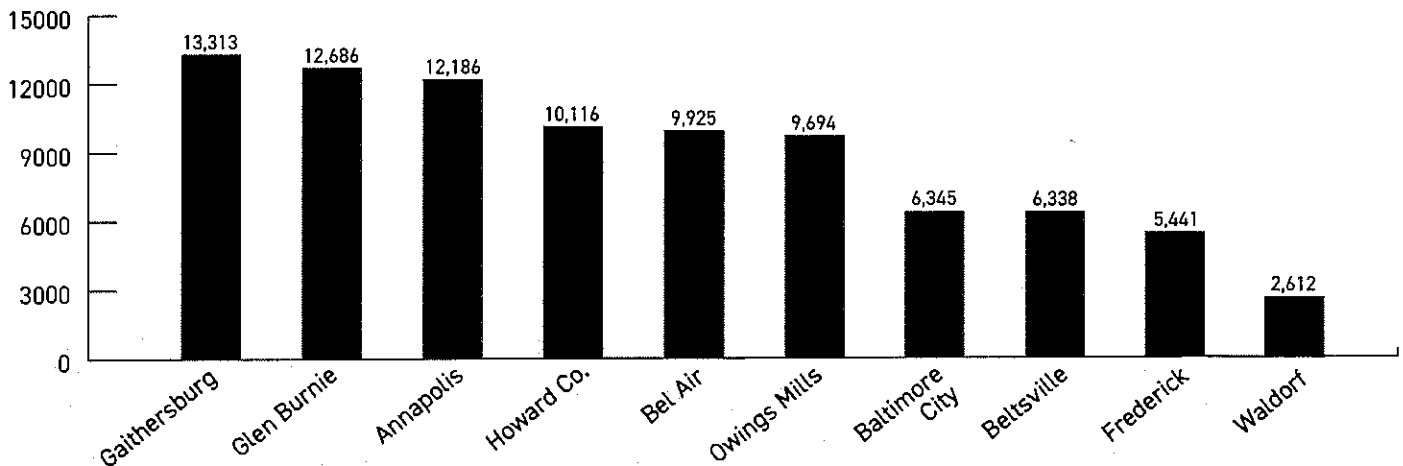


Kiosk usage is one of the alternative service delivery methods. The Vehicle Emissions Inspection Program (VEIP) usage at kiosks have been continually increasing. Governor Hogan reduced the fees for VEIP self-service kiosk by \$4 [February 2016]. Since then, MDOT MVA has added an additional kiosk, developed user-friendly screens and offered staffing on weekends at some of the kiosk locations to support customers. There are a total of 10 VEIP kiosks across the State and since inception the total usage has grown to over 3,000 tests monthly.

In addition, MDOT MVA began offering Transportation Security Administration (TSA) Pre-Check and Transportation Worker Identification Credential (TWIC) cards, as well as Department of Natural Resources services at several MDOT MVA branch offices. In addition to boat registration, Charm Card Sales and EZ Pass devices are now sold through MDOT MVA's eStore. We are constantly looking for ways to save our customers valuable time. The goal of delivering premier customer service is at the heart of every decision we make at MDOT MVA.

FY18 Year End Completed Total Transactions				
	Kiosk	Web	Total	Start Date
EZ Pass	2,635	21,881	24,516	30-Sep-17
DNR	221	1,760	1,981	30-Sep-17
CharmCard	39	127	166	24-Mar-18

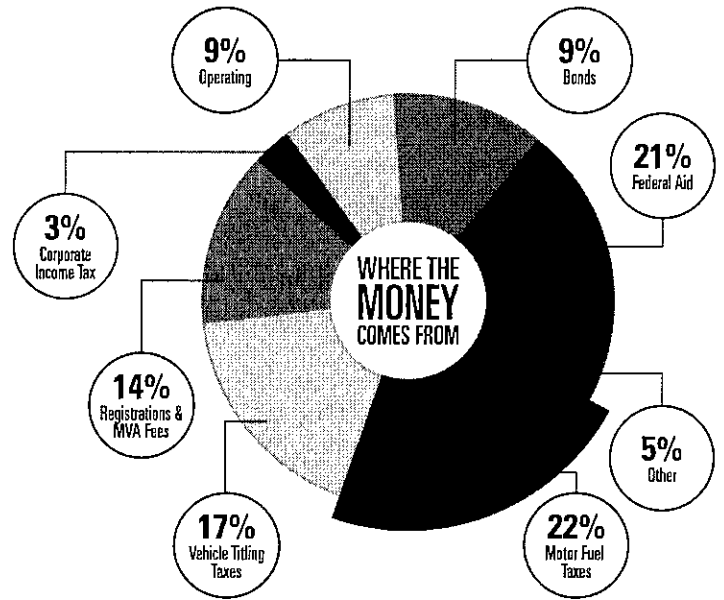
VEIP Kiosk Total Test (Aug 2015 to May 2018)



Where the Money Comes From

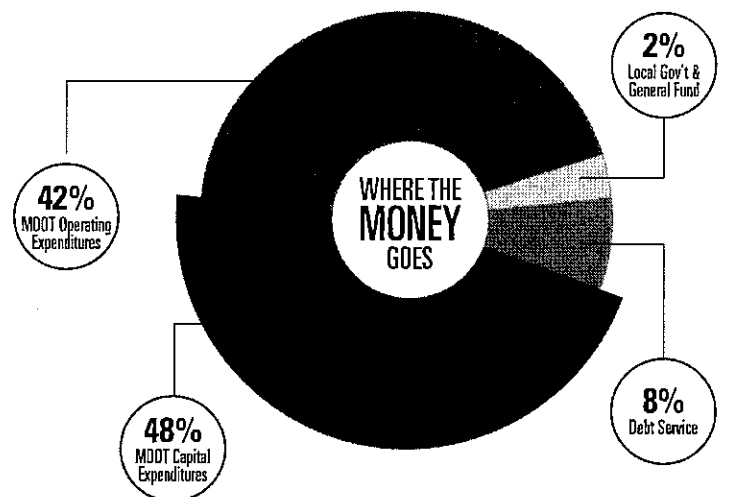
Maryland's transportation system is funded through several dedicated taxes and fees, federal aid, operating revenues, and bond sales, which are assigned to the Transportation Trust Fund. This fund is separate from the State's General Fund, which pays for most other State government operations and programs. MDOT's customers pay user fees for transportation infrastructure and services through motor fuel taxes, vehicle titling taxes, registration fees, operating revenues, and corporate income taxes. The motor fuel tax and vehicle titling tax are two of the largest sources of MDOT revenue. Operating revenues include transit fares and usage fees generated at the Port of Baltimore and BWI Marshall Airport. In addition to collecting revenue within the State, Maryland also receives federal aid for its transportation program. These federal funds must be authorized by a congressional act. The United States Congress enacted federal surface transportation authorizing legislation, known as the FAST Act, in December 2015, which provides investment in transportation infrastructure through FFY 2020.

Total projected Trust Fund revenues amount to \$30.4 billion for the six-year period covered by this CTP. These amounts are based on the assumption that the economy will continue along a moderate growth scenario for the next six years.



Where It Goes

The MDOT program is fiscally constrained, meaning that the list of projects is tied to estimates of future revenue. The Trust Fund supports operation and maintenance of State transportation systems, administration, debt service, and capital projects. A portion of these funds is directed to the General Fund and a share is also dispersed among Maryland's counties and Baltimore City for local transportation needs. After operating costs, debt service, and local distributions, the remaining money goes toward funding capital projects. This document, Maryland's Draft CTP, is the six-year capital budget for all State transportation projects. This Draft FY 2019 - FY 2024 CTP totals \$16.0 billion, \$14.1 billion of which comes through the Trust Fund and \$1.9 billion from "Other" fund sources, including local contributions, WMATA direct funding, PFC airport fees, etc.



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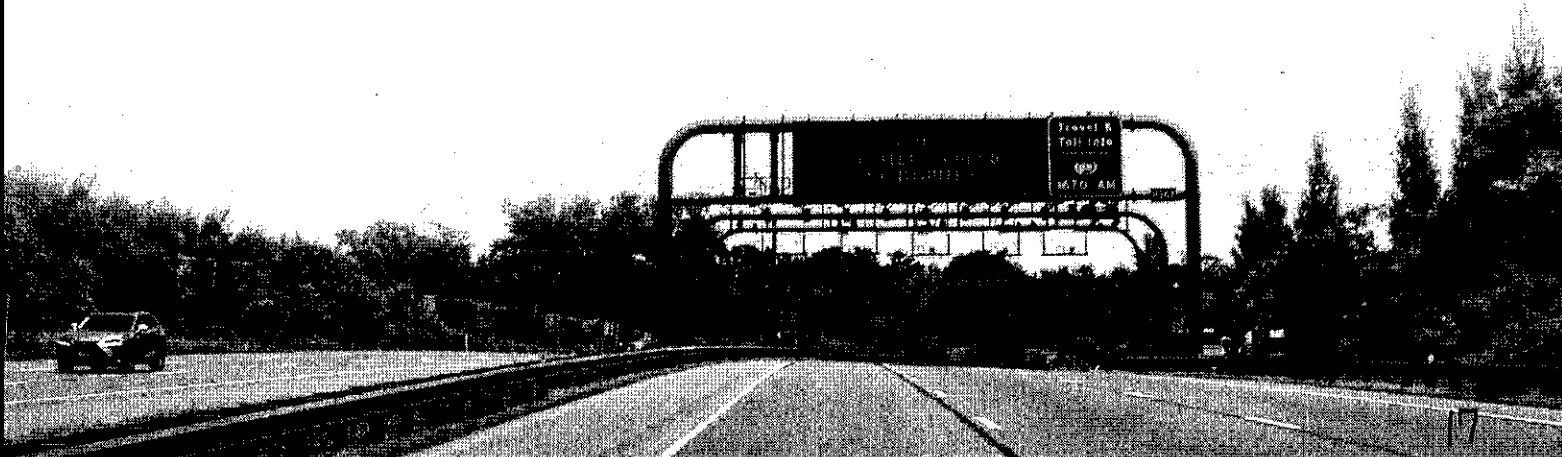
MDOT Addresses Congestion Through Commuter Choice Maryland

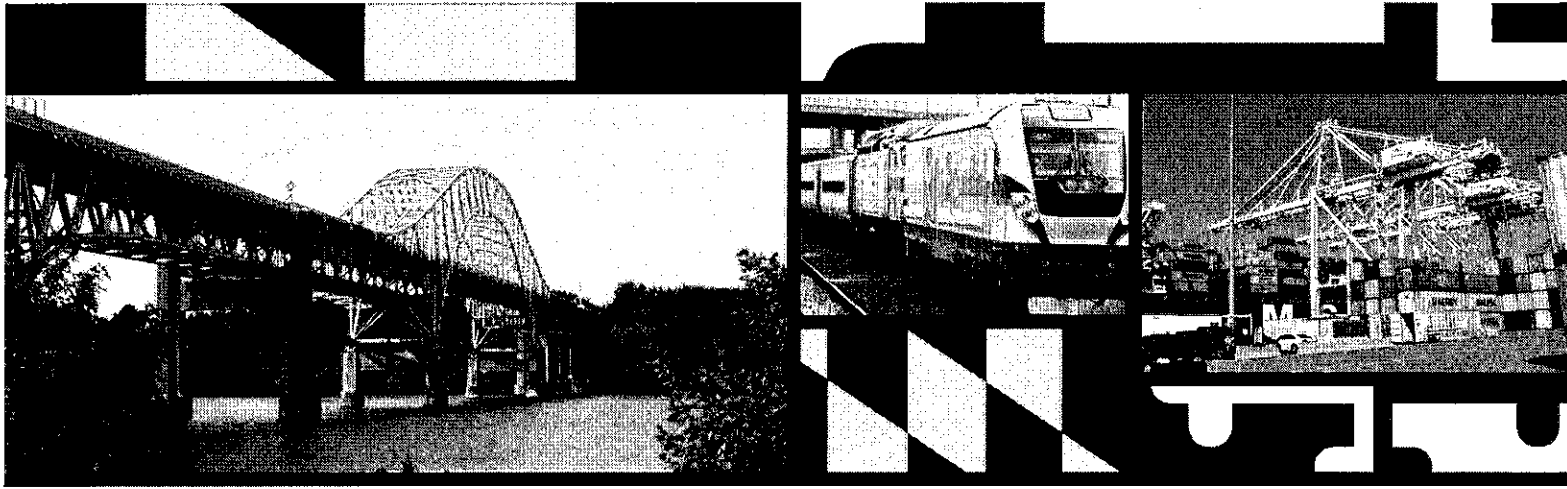
Commuter Choice Maryland is MDOT's Travel Demand Management (TDM) program. Commuter Choice Maryland enables MDOT to address key goals, objectives, and strategies that will maximize traveler choices, and deliver transportation solutions and services that can reduce congestion, conserve energy, protect the environment, and facilitate economic opportunity. Commuter Choice Maryland promotes the use of public transportation, ridesharing, walking, biking, teleworking, and alternative work schedules, to enhance the quality of life for all Marylanders.

In 2018, MDOT began a process to revitalize the program, with the intent of expanding and enhancing delivery of workplace transportation assistance services and resources to employers, including a new website and updated resources and tools. New marketing strategies were developed to promote the use of transportation options during the heavily traveled rush hour periods that do not involve single vehicle travel such as telework, alternative work schedules, and ridesharing. The program also promoted the Maryland Commuter Tax Credit to businesses. MDOT continues to seek and identify opportunities that support TDM options and raise awareness of their availability to commuters and employers. Please visit the Commuter Choice Maryland website for more information at CommuterChoiceMaryland.com.

Discretionary Grant Opportunity and Coordination

The U.S. Department of Transportation (U.S. DOT) and other federal agencies often provide opportunities to apply for discretionary grants. Occasionally these are limited to State agencies, but are more often open to regional and local agencies as well. Discretionary grants differ from federal formula funding because they are competitive in nature and the applicant must meet eligibility criteria. Some examples of discretionary funding include the Better Utilizing Investments to Leverage Development (BUILD) Grant program, formerly the Transportation Investment Generating Economic Recovery (TIGER) grant program, as well as the Bus and Bus Facilities Grant Program, and the new Infrastructure for Rebuilding America (INFRA) grant program, previously the FAST LANE grant program. MDOT pursues these discretionary grant opportunities and partners with local jurisdictions when feasible. To be competitive throughout the grant application process, all partnering entities must provide resources toward the application and the project. If a local jurisdiction is looking to partner with MDOT or seeking MDOT support, it is essential to coordinate early with MDOT, The Secretary's Office of Planning and Capital Programming.





Transportation System Performance

In 2000, the Maryland General Assembly passed a bill requiring MDOT to develop an annual Attainment Report (AR) on Transportation System Performance. The main objectives of the AR are to do the following:

- Report on progress toward achieving the goals and objectives in the MTP and the CTP;
- Establish performance indicators that quantify achievement of these objectives; and
- Set performance targets.

The performance measures evolve and are updated periodically in a collaborative effort between The Secretary's Office, the transportation business units, and, every five years with an AR Advisory Committee. The performance measures were updated this year, in the spring of 2018, with the AR Advisory Committee, based on the updated 2040 MTP Goals and Objectives (please visit mdot.maryland.gov/ARAC). The AR documents show how MDOT is achieving its goals and objectives based on performance indicators and helps Maryland citizens assess improvements to their transportation system.

Since 1996, MDOT has also participated in the State's Managing for Results (MFR) effort as part of the budget process. MFR is a strategic planning, performance measurement, and budgeting process that emphasizes use of resources to achieve measurable results, accountability, efficiency, and continuous improvement in State government programs.

Through coordination with Metropolitan Planning Organizations (MPOs) and adjacent state DOTs, MDOT developed baseline performance measures and targets for the MAP-21/FAST Act federal safety, infrastructure condition, and system performance measures:

- Pavement condition of the Interstate System and on the remainder of the National Highway System (NHS).
- Travel time reliability on the Interstate System and the remainder of the NHS.
- Bridge condition on the NHS.
- Fatalities and serious injuries (both number and rate per vehicle miles traveled) on all public roads.
- Traffic congestion.
- On-road mobile source emissions.
- Reliability of truck travel time on the Interstate System.

MDOT will continue to work with the U.S. DOT, the regional MPOs, and other stakeholders to respond to these new requirements now that the final regulations and guidance have been issued to ensure we demonstrate the effectiveness of MDOT's programs.

Finally, MDOT is internally assessing its performance in meeting customer needs through the quarterly MDOT Excellerator Performance Management System. The program is a living, evolving performance process that is in a constant state of evaluation, analysis and action. MDOT reports quarterly on performance results and uses the process to drive daily business decisions.

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MARYLAND'S CONSOLIDATED TRANSPORTATION PROGRAM

The Maryland Department of Transportation (MDOT) is pleased to present the State's Draft six-year capital investment program for transportation, the Draft FY 2019-2024 Consolidated Transportation Program (CTP).

The CTP is the capital budget outlook and a key part of the State Report on Transportation (SRT) that MDOT publishes each year. The SRT contains three important documents: the Maryland Transportation Plan (MTP), the Consolidated Transportation Program (CTP), and the annual Attainment Report (AR) on Transportation System Performance. The MDOT last updated the MTP, a 20-year vision for Maryland's transportation system, in January 2014. The MTP is updated every four to five years through an extensive outreach effort with the public, local jurisdictions, and state agencies to ensure it reflects the needs and priorities of Marylanders. We are currently updating the MTP for January 2019 release. To learn more or review the draft 2040 MTP, visit the MTP website at www.mdot.maryland.gov/MTP. The CTP contains projects and programs across MDOT. It includes capital projects that are generally new, expanded or significantly improved facilities or services that may involve planning, environmental studies, design, right-of-way acquisitions, construction, or the purchase of essential equipment related to the facility or service.

ESTABLISHING PRIORITIES

This year's CTP reflects the priorities of MDOT as embodied in the goals outlined in the current MTP, our mission, and the results we aim to achieve. These priorities must address federal and state requirements; local government mandates, interests, and concerns; and customer needs. The mission of the Department of Transportation is to be *a customer-driven transportation leader that delivers safe, efficient, intelligent and exceptional transportation solutions in order to connect our customers to life's opportunities.*

While the existing revenues are addressing many needs, MDOT recognizes that these revenues cannot address every need. Consequently, MDOT will use our resources strategically and efficiently to ensure that transportation investments address our mission and goals, as well as to:

- Facilitate economic opportunity in Maryland;
- Provide a safe and secure transportation experience;
- Provide exceptional customer service;
- Provide an efficient, well connected transportation experience;
- Use resources wisely;
- Deliver transportation solutions and services of great value;
- Communicate effectively with our customers;
- Be a good neighbor;
- Be a good steward of our environment; and
- Be fair and reasonable to our partners.

The Maryland Open Transportation Investment Decision Act – Application and Evaluation (Chapter 30) requires MDOT to develop a project-based scoring system to rank major highway and transit transportation projects under consideration for funding in the Consolidated Transportation Program (CTP). Chapter 30 establishes nine goals and twenty-three measures to evaluate these major projects and required MDOT to develop a scoring model on or before January 1, 2018. The project prioritization model required under Chapter 30 does not select major transportation projects for funding but is one of many tools MDOT will utilize in its project selection process. The first project evaluation is now complete and is shown in the appendix of this Draft CTP.

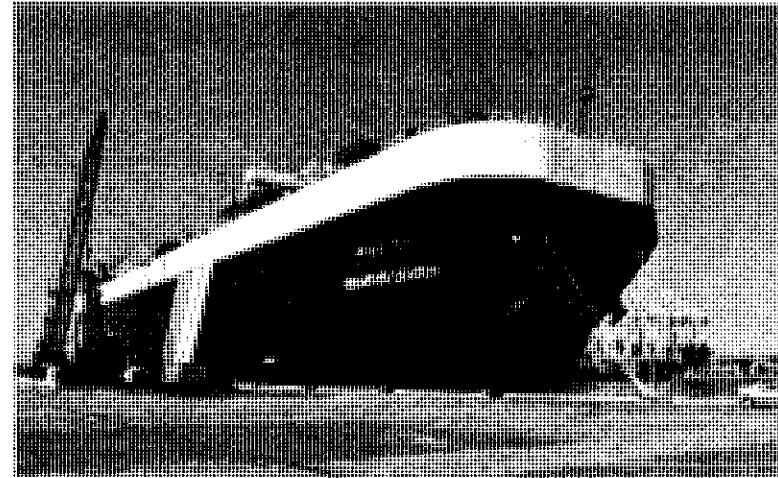
MDOT has created a Chapter 30 scoring model that establishes how the twenty-three measures are defined and measured as well as created a weighting structure to score and prioritize the projects. For more information see mdot.maryland.gov/newMDOT/Planning/Chapter_30_Score/Index.html. The next round of project applications are due March 1, 2019 so projects can be scored for the Draft FY 2020 – FY 2025 CTP.

Facilitate Economic Opportunity in Maryland

Maryland's transportation system is essential to the State's economy. An efficient transportation system provides a competitive advantage to businesses in a regional, national and global marketplace. Transportation directly impacts the viability of a region as a place that people want to live, work and raise families, all critical to attracting a competent workforce. Transportation infrastructure provides value and investing in Maryland's transportation system creates jobs and supports Maryland industries and businesses. MDOT works to ensure its investments support a healthy and competitive state economy. It will do this by undertaking projects that improve access to jobs as well as improve freight and commodity flows and the movement of goods and services in and through Maryland. MDOT is currently advancing its largest construction program in its history reflecting unprecedented growth in transportation investments. In the fall of 2018, MDOT has 816 airport, highway, transit, port, bicycle and motor vehicle projects underway at a value of \$8.8 billion.

The Hogan Administration has declared Maryland "Open for Business" and continues to challenge MDOT to facilitate economic opportunity and to help create jobs. To do this, MDOT must focus on fixing our highways and bridges and addressing congestion issues all around the State by employing efficient and innovative transportation solutions. This year's CTP continues the implementation of key new projects to address long-standing transportation issues across the State. Cost savings, reallocation and innovative project delivery mechanisms have allowed MDOT to be in the fortunate position to move numerous projects forward to address many of the State's needs and invest public dollars in the most efficient and cost-effective way while supporting economic development and creating or supporting jobs and minimizing impacts to Marylanders,

The Port of Baltimore is ensuring that Maryland is "Open for Business" by continuing to break cargo records and maintaining its role as one of Maryland's top economic generators. In 2017, the Port's public and private marine terminals handled 38.4 million tons of cargo. This was the most since 1979 and the third-highest tonnage in its history. It is expected that 2018 will be the third consecutive year the state-owned terminals handled more than 10 million tons of general cargo. The Port of Baltimore also set a record in 2017 by handling 807,194 cars and light trucks. It was the first time the Port surpassed 800,000 cars/light trucks and the seventh consecutive year handling more of that cargo commodity than any other U.S. port. The trend for 2018 continues to be higher than previous years.



Freight

Freight activity in Maryland and throughout the East Coast is expected to increase significantly in the coming decades. Maryland's location at the crossroads of the I-95 corridor and significant rail and marine corridors means that the infrastructure in Maryland is critical to the state, regional, and national economy. As much of Maryland's freight network is shared with passenger or vehicle operations, both freight and passenger growth will exacerbate already congested infrastructure throughout the State. The resulting chokepoints create significant challenges for freight and passenger movement in the region. It is imperative that MDOT work with local, state and federal officials and freight stakeholders to plan and facilitate the necessary improvements to accommodate freight demand and allow for the cost-effective and safe movement of goods by all modes of transportation.

To accommodate the approximate 60 percent growth in air cargo activity at BWI Marshall Airport over the last year, a fast-track construction project expanded the Midfield Cargo Apron with six new aircraft parking positions that allowed for a new carrier to begin service in time for peak 2017 holiday season demand.

To meet these needs, MDOT is taking an aggressive approach to implement other multimodal freight solutions in Maryland. Please refer to the CTP Freight Summary Section on page FRT-1 of the CTP for a listing of all of MDOT's freight projects. In addition to capital projects, MDOT is involved in several freight planning efforts, and recently updated the Strategic Goods Movement Plan at the end of 2017. Additional information on MDOT's freight activities can be found on MDOT's website at www.mdot.maryland.gov.

Provide a Safe & Secure Transportation Infrastructure

MDOT will not compromise on our commitment to continually improve the safety and security of our customers and partners in everything we do. It is critical that we commit to safety and security in our designs, in our construction, as well as how we operate and maintain the State's transportation system. We promote a culture of safety in our business practices and educate our traveling public on good safety behavior and practices. MDOT works with our federal and local law enforcement partners on a daily basis to constantly evaluate and implement measures to reduce the vulnerability of Maryland citizens and facilities. With federal and state investments, progress is being made on a variety of fronts.



MDOT Safety Pledge

Secretary Pete Rahn first issued a challenge to the more than 10,000 MDOT employees and now to all Maryland citizens to take the traffic safety pledge and agree to follow the State's highway safety guidelines when they drive, ride, or walk. Recent increases in traffic-related deaths prompted the Secretary to announce the pledge and to make sure that traffic safety starts at home. That is why he urges all Marylanders to demonstrate safety behavior by following some simple, common sense rules:

- Have everyone wear a seat belt;
- Always have a safe and sober ride;
- Park the phone before driving;

- Use crosswalks and be seen while walking or biking;
- Look for pedestrians and motorcyclists and give bicyclists three feet of clearance;
- Wear proper protective gear while riding a motorcycle; and
- Allow enough time to get to one's destination safely. Speed is a factor in many crashes and greatly increases crash severity.

Traffic fatalities in Maryland rose from 547 in 2017 to 557 in 2018, continuing a higher trend. For more information, see the safety pledge at: surveymonkey.com/r/MDOTsafetypledge.

Strategic Highway Safety Plan

MDOT promotes traffic safety through the many infrastructure and behavioral programs and projects implemented by the MDOT State Highway Administration (MDOT SHA) and MDOT Motor Vehicle Administration's (MDOT MVA) Highway Safety Office, which is the lead agency for the State's Strategic Highway Safety Plan (SHSP). The SHSP is a statewide, comprehensive safety plan that provides a coordinated framework for reducing deaths and severe injuries on all public roads. Through extensive outreach and involvement of federal, state, local, and private sector safety stakeholders, the plan establishes statewide goals and critical emphasis areas. As an outgrowth of this initiative, many local communities are developing local road safety plans using the SHSP as a guide. The Federal Highway Administration (FHWA) has designated this program as a proven countermeasure because it tailors solutions to local issues and needs.

Recent investments completed to enhance public safety and security include projects at BWI Marshall Airport to create state-of-the-art passenger security screening areas between Concourses B and C and between Concourses D and E. These projects are providing for a post-security connection between concourses A, B and C, as well as between concourses D and E (the International Terminal).

In the Summer of 2018, MDOT SHA completed MD 4 urban reconstruction improvements between Forestville Road and MD 458. These included construction of raised curbs, new sidewalks along southbound MD 4, a 10-foot shared-use path along northbound MD 4, and other traffic calming improvements to improve pedestrian and bicyclist safety. This \$26 million investment is one example of how we are working to safely accommodate all users of our roads.

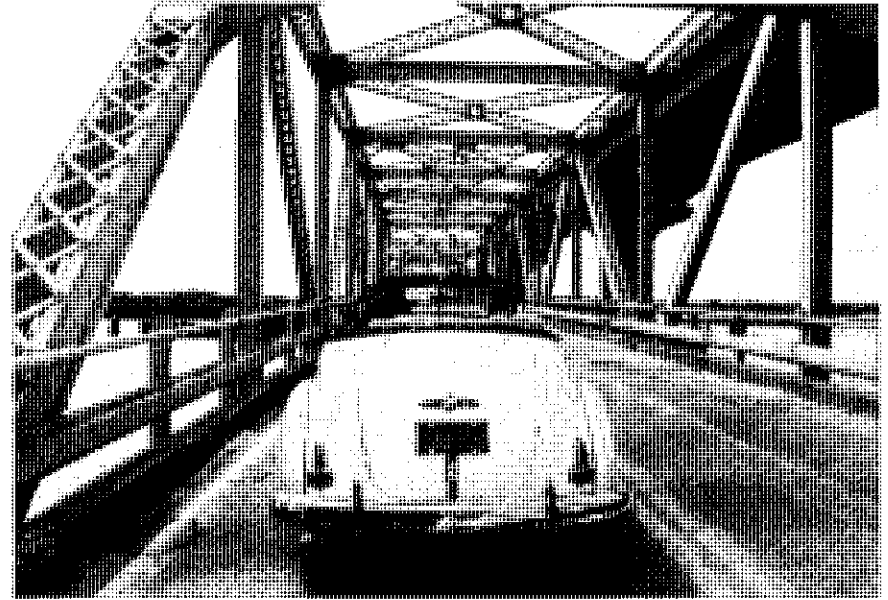
Provide an Efficient, Well-Connected Transportation Experience

MDOT will provide an easy, reliable transportation experience throughout the system including enhancing connections and developing world class transportation facilities and services. The users of Maryland highways face some of the nation's worst congestion. This fact has stifled economic development across the State. The Hogan Administration continues to direct MDOT to address long-standing congestion issues by initiating projects statewide that will serve to increase mobility and move traffic more efficiently. Construction of new highway capacity to accommodate travel has not kept pace with demand.

Maryland has the second-longest commuting times in the country, and the Washington metropolitan region is the most congested region in the nation based on annual delay and congestion cost per auto-commuter data. The Hogan Administration's proposed Washington area Traffic Relief Plan will add new managed or toll lanes to I-270, I-495 (Capital Beltway), and MD 295 (Baltimore-Washington Parkway), leaving the existing lanes untolled. These managed lanes will provide drivers with the choice to pay for a quicker trip, simultaneously reducing delays for those who choose to stay in the existing free lanes. The Hogan Administration's proposed Baltimore area Traffic Relief Plan will extend the Express Toll Lanes (ETLs) on I-95 and provide innovative congestion relief on I-695 (Baltimore Beltway). The third element of the Traffic Relief Plan is the deployment of cutting-edge smart traffic signals to improve traffic operation and ease congestion 14 major corridors across the state. The system uses real-time traffic conditions and computer software that adjusts the timing of traffic signals, synchronizes the entire corridor, and effectively deploys artificial intelligence to keep traffic moving. These improvements are critical to relieve our most congested State roadways, spur economic development, and restore quality of life for countless Marylanders who have been negatively affected by years of traffic congestion. More information on the Traffic Relief Plan is available on the MDOT SHA website at www.roads.maryland.gov.

Use Resources Wisely

MDOT receives resources from our customers and they expect excellent products and services in return. In order to better serve our customers, MDOT must maximize the value of every dollar we spend. MDOT continues to place a high priority on allocating funds toward system preservation. The CTP reflects significant investments in the bridge program, road and runway resurfacing, rail car overhauls and replacements, bus replacements, and general facility rehabilitation, replacement and upkeep.



A key focus area is the condition of bridges across Maryland. SHA continues to make significant progress in reducing the number of structurally deficient bridges (bridges are safe but need repairs/replacement) on the State's highway system to ensure safe travel for Maryland motorists and users of our system. In CY2017, SHA maintained one of the lowest percentages (less than 3%) of structurally deficient bridges of any State DOT with only 62 out of 2,564 bridges rated as structurally deficient. In addition, SHA spent more than \$260 million in FY 2018 on resurfacing roads. MDOT SHA has surpassed the halfway point of resurfacing or treating all state highway lane miles since 2015. This milestone marks the improvement of nearly 8,500 lane miles, calculated by miles of highway multiplied by the number of lanes, improving safety and enhancing the customer experience for millions of drivers across the state.

Deliver Transportation Solutions and Services of Great Value

MDOT will deliver transportation solutions on time and within budget. We will use strategies to ensure that the transportation solution meets the needs of our customers and eliminates unnecessary costs.

Transit

Providing safe, efficient and reliable transit services with world-class customer service is a priority for MDOT. MDOT is committed to working with all of MDOT MTA's customers to improve the region's transit system.

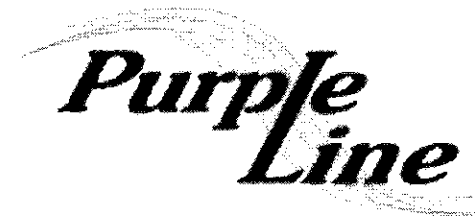
BaltimoreLink, which redesigned local and express bus systems to create an interconnected transit system, launched in June 2017. The goals were to improve service quality and reliability, maximize access to high-frequency transit, strengthen connections between bus and rail routes, and align the network with existing and emerging job centers. BaltimoreLink is now a unified, interconnected transit network that incorporates LocalLink (Local Bus), Light RailLink and Metro SubwayLink. It also includes dedicated bus lanes, transfer facilities and transit signal priority



A major component is CityLink, which are 12 new high-frequency, color-coded bus routes that better connect riders to Amtrak, Commuter Bus lines, Light RailLink, MARC trains, Metro SubwayLink and other services in Baltimore and the surrounding suburbs. In Baltimore City, new CityLink routes run at 10-15 minute frequencies. Buses are branded and travel on color-coded routes with easy-to-read signage and detailed maps making the system easier to use. The BaltimoreLink network is providing more people with access to transit, jobs, and services in the region with an estimated 130,000 additional people within a ¼ mile access to frequent transit operating every 15 minutes or less during peak and midday periods. Eleven percent more jobs are accessible within 30 minutes and BaltimoreLink added a number of public schools, libraries, pharmacies, hospitals, and supermarkets to the frequent transit network.

Since MDOT MTA launched BaltimoreLink, on-time performance has dramatically improved from 59.5 percent in Fall 2016 to 68 percent in May 2018 – a 14.4 percent improvement. Additionally, MDOT MTA has launched a partnership with the Transit app, which allows customers to receive highly accurate location and arrival information for CityLink, LocalLink, and Express BusLink in the BaltimoreLink fleet. To learn more about Transit app, visit mta.maryland.gov/transit. In partnership with Baltimore City, more than 5.5 miles of dedicated lanes were installed and have improved travel times by up to 25 percent. Transit signal priority sensors on the entire bus fleet and at nearly 50 intersections are improving travel times up to 22 percent. The bus system is safer than ever. Despite providing over 300,000 miles of additional service there have been 20 percent fewer accidents than under the previous system.

To learn more about BaltimoreLink, visit baltimorelink.com.



MDOT officials signed the \$5.6 billion public private partnership contract in April 2016 with the Purple Line Transit Partners to design, build, finance, operate, and maintain the light rail system. The Purple Line will run east-west inside the Capital Beltway, with 21 stations connecting to: Metrorail's Orange, Green, and Red lines; the MARC Brunswick, Camden, and Penn lines; and Amtrak at New Carrollton. Construction is now underway along the entire 16-mile light rail corridor. Since the project broke ground in August 2017, utility relocation, earthwork, and tunnel excavation have been ongoing. Light rail vehicle production is also underway.

The P3 alternative delivery approach involves a long-term, performance-based agreement between MDOT MTA and the Purple Line Transit Partners. The innovative project delivery approach creates a predictable, transparent, and streamlined approach, incorporating best practices and lessons learned from other states and countries, while addressing the transportation and economic development needs of Marylanders. MDOT entered into a Full Funding Grant Agreement (FFGA) in fall 2017 with the Federal Transit Administration; this agreement provides for \$900 million for the construction of the project. Construction began with a groundbreaking ceremony on August 28, 2017. For more information, visit www.purplelinemd.com.

Practical Design

To benefit the entire Maryland transportation network, MDOT has been developing engineering policies for all of its business units that incorporate the principles of practical design, which focuses on producing safe and efficient projects that address the most important needs at the most economical cost. The goal is to build good projects to achieve a safe, well-performing transportation system throughout the State without shifting the cost burden to maintenance. MDOT's Practical Design Policy provides a process for all planning, preliminary engineering, and design activities. The policy ensures that safety is never compromised, design solutions are reached collaboratively, and the project's needs are met.

Thanks to practical design and innovative project delivery, MDOT SHA has been able to deliver more projects for Marylanders using existing resources. MDOT SHA is increasingly using an innovative cost plus time approach, known as A+B bidding. The A+B bidding considers both the cost and the time to build the improvements to determine the award of a project. This approach minimizes impacts to the traveling public, ensures safety considerations are addressed, and delivers improvements for capacity and economic development faster than a traditional award based solely on low bidding thereby providing better value to the public.

Be a Good Neighbor

As the owner of statewide transportation facilities, MDOT must work with our neighbors to find solutions that work for our customers and are sensitive to our neighbors. This includes examining all of the modes of travel including flying, driving, riding transit and even freight coming into the Port. One way to connect better and work with our neighbors is to provide better bicycle and pedestrian connections.

Maryland, like many parts of the country, is seeing an increased interest in biking and walking as significant transportation mode choices. Each is an integral part of the state's broader transportation approach, and a key element to how the agency seeks to deliver on our mission of providing excellent customer service. Safe infrastructure for cyclists and pedestrians is also essential in how MDOT contributes to the broader statewide goals of reducing greenhouse gas emissions, alleviating congestion, encouraging healthy activities, and supporting activity-based tourism and economic development.

This year's CTP reflects the strength of MDOT's ongoing commitment to improving bicycle and pedestrian safety and access across the state. The \$177 million figure shown here includes not only continued commitment to

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our key discretionary programs, such as the state's Bikeways Program, but significant investment in MDOT projects that support the improved access and safety of cycling and walking throughout Maryland. Moreover, these program investments will be strengthened by the fresh guidance being provided by the new goals and priorities identified in the 2019 Bicycle and Pedestrian Master Plan Update. Newly targeted strategies and initiatives identified in this document, along with the 2040 Maryland Transportation Plan, ensure that MDOT is responsive to the changing needs and opportunities to improve bike and walk connectivity and safety for all MDOT customers.

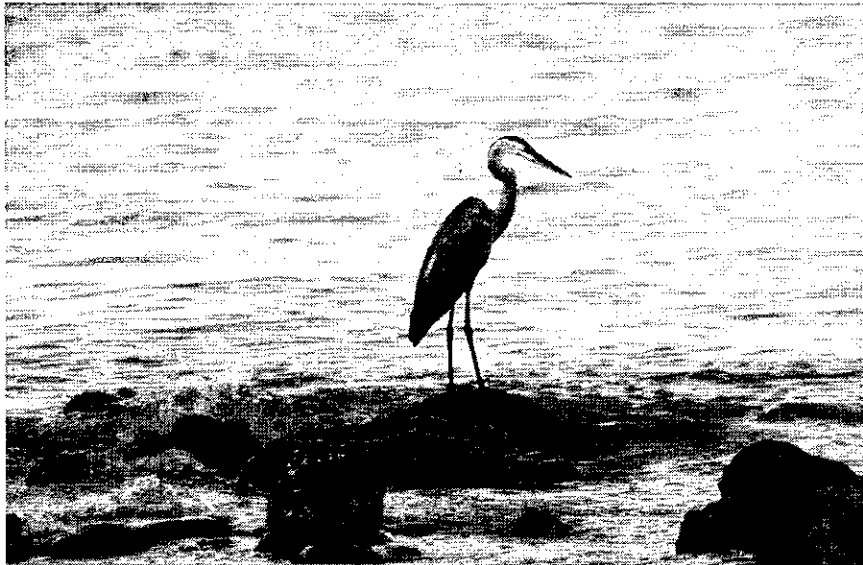


Be a Good Steward of our Environment

MDOT will be accountable to our customers for the wise use of limited resources and our impacts on the environment when designing, building, operating and maintaining Maryland's transportation system. MDOT's commitment to Environmental Stewardship is one aspect of a larger commitment to use innovative and forward-looking strategies to ensure our transportation system protects our natural, cultural and community resources.

By coordinating land-use, transportation, and resource planning with partners in other agencies and local governments, MDOT helps to ensure that the investments made will meet multiple needs for the citizens of Maryland. Using the State's Green Infrastructure Plan and Chesapeake Bay Restoration

priorities as a guide, MDOT agencies are minimizing negative impacts and using project mitigation to support the State's broader conservation goals. To help decrease pollution from entering our waterways, the CTP supports a three-pronged approach. Retrofitting older parts of the transportation network with the latest stormwater management technology; restoring natural filters through stream restoration, forest establishment and wetland creation; and adopting protective operational practices will move the State closer to meeting mandated water quality targets.



Aside from its cargo and cruise responsibilities, the Port of Baltimore has also proven to be environmentally responsible. The MDOT MPA is committed to being a good environmental steward. Its award-winning and nationally renowned dredging program has rebuilt eroded islands in Maryland using sediment removed from channels leading to the Port of Baltimore. There are also green initiatives on the marine terminals that are reducing our carbon footprint. One of these green initiatives, the Port's dray truck replacement program, has resulted in 172 older dray trucks being replaced with newer, cleaner-running engines. The MDOT MPA's green efforts led to the selection of the Port of Baltimore as the first North American host of the 2018 GreenPort Congress in 2018.

MDOT is working to reduce air emissions and managing energy consumption related to the transportation industry. These issues are being addressed by continued efforts to advance vehicle technologies and provide alternatives to traveling by single occupant vehicles. MDOT uses a variety of Travel Demand

Management (TDM) strategies to support alternatives to driving alone and limit emissions from the transportation sector. TDM efforts can also help reduce congestion, lower commuting costs, and improve air quality. Some of these efforts are: carpooling, car sharing, transit, teleworking, and variable pricing infrastructure. MDOT is at the forefront of promoting alternative and advanced vehicle technologies, which will result in a significant decrease in transportation-related air pollution. MDOT is implementing these strategies in cooperation with our partners in the metropolitan planning organizations (MPOs), the Maryland Departments of the Environment and Energy, local governments, and the private and not-for-profit sectors.

Commuter Choice Maryland

Commuter Choice Maryland is MDOT's Travel Demand Management (TDM) program. Commuter Choice Maryland enables MDOT to address key goals, objectives, and strategies that will maximize traveler choices, and deliver transportation solutions and services that can reduce congestion, conserve energy, protect the environment, and facilitate economic opportunity. Commuter Choice Maryland promotes the use of public transportation, ridesharing, walking, biking, teleworking, and alternative work schedules, to enhance the quality of life for all Marylanders.

In 2018, MDOT began a process to revitalize the program, with the intent of expanding and enhancing delivery of workplace transportation assistance services and resources to employers, including a new website and updated resources and tools. New marketing strategies were developed to promote the use of transportation options during the heavily traveled rush hour periods that do not involve single vehicle travel such as telework, alternative work schedules, and ridesharing. The program also promoted the Maryland Commuter Tax Credit to businesses. MDOT continues to seek and identify opportunities that support TDM options and raise awareness of their availability to commuters and employers. Please visit the Commuter Choice Maryland website for more information at CommuterChoiceMaryland.com.



LEGISLATIVE CHANGES

The 2018 legislative session brought two significant changes to the Consolidated Transportation Program (CTP). These changes affect local Highway User Revenues (HUR) and Transit funding. As a result of these changes, MDOT's six-year CTP increases by \$1.2 billion, totaling \$16.0 billion.

Chapter 330, Laws of Maryland 2018 increases the allocation of HUR to Baltimore City, the Counties and the Municipalities from 9.6% to 13.5% of the funds credited to the Gasoline and Motor Vehicle Revenue Account (GMVRA). This increased allocation is for five fiscal years (FY 2020 – FY 2025). Chapter 330 also changes the definition of HUR from funds in the GMVRA to capital grants appropriated to the local jurisdictions. The capital grants are to be appropriated only if all debt service requirements and operating expenditures have been funded and sufficient funds are available to fund the Department's capital program. These changes are effective July 1, 2019. This change increases local transportation funding and is now included in the 6-year CTP total funding level. The calculation of local funding will continue to be based on a formula that incorporates roadway lane miles maintained by the jurisdiction and the number of registered vehicles in that jurisdiction.



The second legislative change is the Maryland Metro/Transit Funding Act. In terms of capital funding, the key components of this Act are as follows.

The second legislative change is the Maryland Metro/Transit Funding Act. In terms of capital funding, the key components of this Act are as follows:

- Starting in FY 2020, the Governor is to include a State budget appropriation of \$167 a year million from revenues available for the State capital program in the TTF as a grant to be used to pay WMATA capital costs. The Governor has authority to appropriate general funds for this purpose.
- The Act also calls for an increase of 3% a year of the existing WMATA funding. This increases funding to WMATA by \$28.6 million over the six-year period.
- An additional \$29.1 million a year for FY 2020 – FY 2022 is to be applied towards the capital needs of MTA

The MTA funding will be spent on key system preservation projects. The following chart shows how those funds will be allocated.

<p>Bus System Preservation</p> <p>This project includes replacement/repair of bus facility assets including concrete slab floors, electrical, and mechanical components.</p>	\$10.2 million
<p>Light Rail System Preservation</p> <p>This project includes the replacement/repair of light rail system assets including rail, brackets, ties, electrical, and mechanical components.</p>	\$47.6 million
<p>Metro System Preservation</p> <p>This project includes the replacement/repair of metro facility and system assets including rail, interlockings, doors, electrical, and mechanical components.</p>	\$18.2 million
<p>Agencywide System Preservation</p> <p>This project includes the replacement/repair of facility and system assets throughout the agency including server infrastructure and fare collection components beyond useful life.</p>	\$11.3 million
Total allocation	\$87.3 million

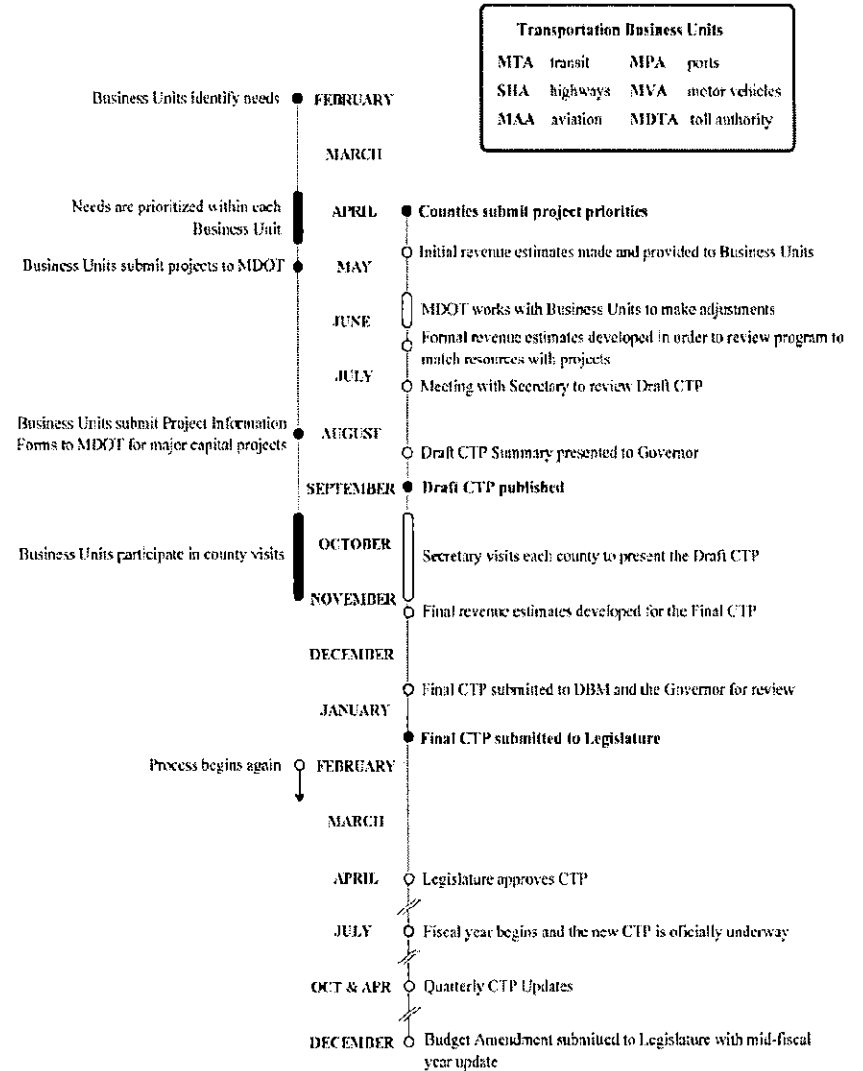
Process for CTP Development

The CTP takes nearly a full year to create through the collaboration and work of MDOT staff with state, regional and local elected officials. Each year, local jurisdictions are encouraged to submit priority project(s) to the State by April. It is important for MDOT to hear from local jurisdictions to facilitate collaboration on state and local needs. MDOT uses the following criteria to identify projects and programs that respond to the State's transportation priorities.

These criteria include:

- Meets all federal and other legal mandates (e.g. Total Maximum Daily Load (TMDL) compliance, Positive Train Control (PTC), Federal Aviation Administration (FAA) regulations to maintain airport permits);
- Supports MDOT's program priorities and MTP goals (safety, system preservation, economic development, etc.);
- Meets all federal match requirements to maximize federal revenue sources;
- Supports state plans and objectives;
- Supports existing project commitments and upholds intergovernmental agreements;
- Is the single top priority within a local priority letter;
- Is consistent with local plans; and
- Is included in the regional Metropolitan Planning Organization (MPO) long-range plan (if the project is located within an MPO boundary).

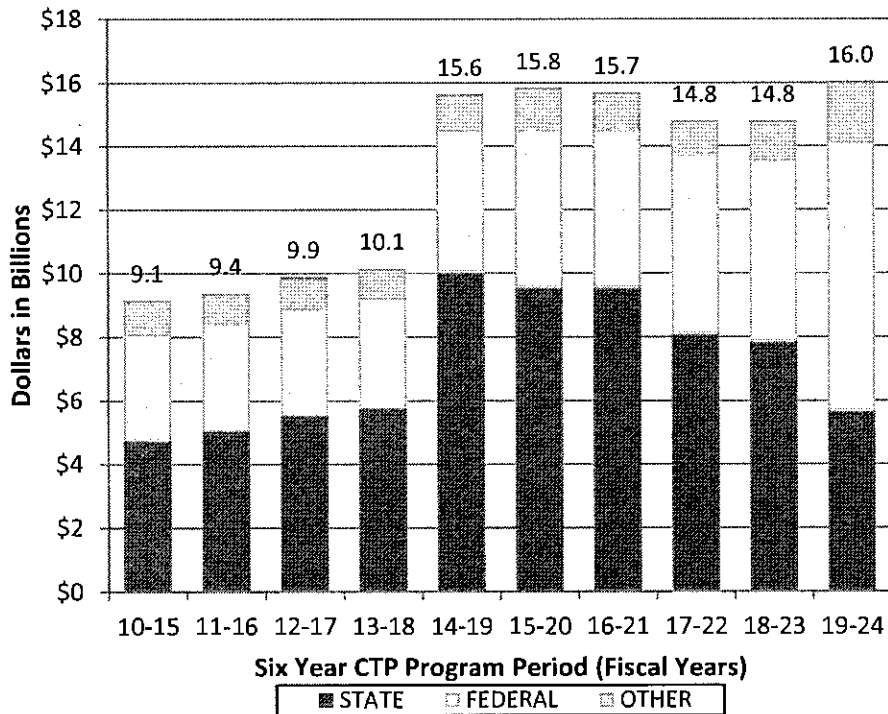
CTP Development Process
Maryland Department of Transportation - January 2016



FINANCING MARYLAND'S TRANSPORTATION PRIORITIES

In developing the CTP and establishing funding levels, MDOT must account for state and local economic growth, fluctuations in state transportation revenue, and allocations of federal funding. The State's Transportation Trust Fund supports MDOT investments through a dedicated account. The Transportation Trust Fund utilizes a variety of revenue sources, which provides funding that enables MDOT to address important capital and operating needs including congestion relief, safety improvements, transit availability; and maintain the competitiveness of the Port of Baltimore and the BWI Marshall Airport.

Maryland Department of Transportation TOTAL CAPITAL PROGRAM LEVELS



State Revenue Projections

Total projected revenues amount to \$30.4 billion for the six-year period. This estimate is based on the revenue sources used by MDOT and includes bond proceeds and federal funds that will be used for operating, capital and debt payment expenses. The projection does not assume any future State tax or fee increases beyond those changes enacted to date.

Pertinent details are as follows:

- **Opening Balance:** MDOT's goal is to transition to a \$150 million fund balance over the program period to accommodate working cash flow requirements throughout the year.
- **Motor Fuel Tax:** This revenue is projected to be \$7.1 billion over the six-year period. As of July 1, 2018, the motor fuel tax rates were 35.3 cents per gallon gasoline and the 36.05 cents per gallon diesel fuel. These rates include the revenue components provided by the Transportation Act. The Consumer Price Index (CPI) effect is estimated to average 3.6 cents per gallon over the program period. The 5 percent sales and use tax equivalent rate effective July 1, 2018 is 9.7 cents per gallon. The rate is estimated to average 10.6 cents per gallon over the program period.
- **Motor Vehicle Titling Tax:** This source is projected to yield \$5.5 billion. The titling tax of 6 percent of the fair market value of motor vehicles, less an allowance for trade-in vehicles, is applied to new and used vehicles sold and to vehicles of new residents. This revenue source follows the cycle of auto sales with periods of decline and growth. It is projected that this six-year planning period will follow a normal business cycle around an underlying upward trend.
- **Motor Vehicle Registration/Miscellaneous, and Other Fees:** These fees are projected to generate \$4.0 billion. This forecast assumes revenues will increase an average of 1.5 percent every two-year cycle.
- **Corporate Income Tax:** The transportation share of corporate income tax revenues is estimated to be \$1.1 billion. MDOT receives 14.6 percent of the State's 8.25 percent corporate income tax revenues.
- **Federal Aid:** This source is projected to contribute \$6.2 billion for operating and capital programs. This amount does not include \$599 million received directly by the WMATA. The majority of federal aid is capital; only \$588 million is for operating assistance. Since federal aid supports a significant portion of the capital program, a more detailed

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discussion of federal aid assumptions is presented in the next section of this summary.

- **Operating Revenues:** These revenues are projected to provide a six-year total of \$2.9 billion, with \$1.1 billion from MDOT MTA, \$329 million from MDOT MPA, and \$1.5 billion from MDOT MAA. MDOT MTA revenues primarily include rail and bus fares, which became indexed to inflation beginning in fiscal year 2015, as provided by the Transportation Act. MDOT MPA revenues include terminal operations, the World Trade Center, and other Port related revenues. MDOT MAA revenues include flight activities, rent and user fees, parking, airport concessions, and other aviation-related fees.
- **Bond Proceeds:** It is projected that \$3.0 billion of bonds will be sold in the six-year period. The level of bonds that could be issued is dependent on the net revenues of MDOT. This level of bonds is affordable within the financial parameters used by MDOT.
- **Other Sources:** The remaining sources are projected to provide \$620 million. These sources include earned interest from trust funds, reimbursements, and miscellaneous revenues.

FEDERAL AID ASSUMPTIONS

Enacted in December 2015, the Fixing America's Surface Transportation (FAST) Act re-authorized federal funding for highway, transit and other multimodal projects through September 30, 2020. The FAST Act includes some policy changes, a new focus on freight and provides funding certainty for five full years through September 2020 including built-in inflation from existing funding levels.

The bill focuses on establishing a new formula program for freight, increases some flexibility in spending by converting certain funds into block grants, and streamlines certain functions by eliminating duplications and creating some pilot programs. Authorization does not mean appropriation. While Congress authorized a five-year transportation bill, each year, Congress must then appropriate the funds through the federal budget process, which can be at lower amounts than authorized. For FFY 19, this CTP assumes that Congress will appropriate the FAST Act authorized amounts in the transportation bills for FFY 2019 through FFY 2020 and continue this inflated funding through FFY 2024.

Federal aid, representing 21 percent of the total funding in Maryland's Transportation Trust Fund (TTF), supports the multimodal investments in the State's FY 2019 - FY 2024 Consolidated Transportation Program (CTP).

Highways and Transit

Most of the federal funds received by MDOT come from the Federal Highway Trust Fund (FHTF), which provides transportation investment for projects in the following areas: highways and transit, multimodal freight, safety and security, system preservation, bike and pedestrian, and congestion mitigation.

The CTP allocates these federal funds to projects in the program based on reasonable assumptions of authorization given the FAST Act. MDOT expects to have approximately \$648 million in highway formula funding and \$162 million in transit formula funding in FFY 2019 for MDOT projects. The Purple Line has received a commitment from the Federal Transit Administration for New Starts funding. The FFY 2018 Appropriations Act supported the Administration's request for \$900 million for Maryland's Purple Line, and to date the project has received \$445 million in appropriations from the combined FFY 2016 - FFY 2018 Appropriations bills. In FFY

Federal highway program funds authorized and apportioned to the states are subject to annual ceilings, which determine how much of the authorized money can be obligated in any given year. This ceiling is referred to as Obligational Authority (OA) and is imposed by Congress annually in response to prevailing economic policy. Since FFY 2004, OA has ranged from 84 percent to 95 percent. The OA level received in FFY 2018 was 91.7 percent. Given that Congress has passed a long-term bill with inflation built in, this CTP assumes an OA level of 94.0 percent for FFY 2019 through FFY 2024.



Washington Metropolitan Area Transit Authority — WMATA

Under the Fixing America's Surface Transportation (FAST) Act in FY 2019, the Washington Metropolitan Area Transit Authority (WMATA) anticipates receiving \$311 million in FTA formula grants and \$26.1 million in other federal grants (Congestion Mitigation and Air Quality, and Department of Homeland Security grants) for bus and rail preservation activities. Additionally, FFY 2019 funding of \$148.5 million is provided through the Passenger Rail Investment and Improvement Act (PRIIA).

The region's jurisdictions created WMATA through an interstate compact as an agency of the State of Maryland, the District of Columbia and the Commonwealth of Virginia. With the recent dedicated funding from all three regional jurisdictions federal funding will be a smaller percentage but still WMATA's largest single source of funding. Overall WMATA's six-year capital budget is \$8.5 billion with \$2.2 billion from federal sources and \$6.3 billion state/local funding.

MDOT's top priority is to restore the safety and reliability of the WMATA system. This is demonstrated through investments in safety and state of good repair. This CTP includes a total of \$300 million (\$50 million each year in FFY 2019 through 2024) as Maryland's matching contribution required by the federal PRIIA legislation. To date, the signatory parties have fulfilled their promise by providing funds to match federal grants provided from FFY 2010 through FFY 2019.



Aviation

The Federal Aviation Administration (FAA), through the Airport Improvement Program (AIP), is authorized to provide federal entitlement and discretionary funding for airport projects. The MAA estimates annual AIP entitlement funding will range from \$3.5 million to \$4.0 million for the BWI Marshall Airport during the six-year period. Entitlement funding is calculated using enplanement and cargo-based formulas for the BWI Marshall Airport and adjusted based on the airport's authority to collect Passenger Facility Charges (PFC). The FAA Extension, Safety, and Security Act of 2016 extended FAA authority to September 30, 2017. Since October 1, 2017, the FAA has operated under two short-term extensions. The Consolidated Appropriations Act of 2018 extends FAA authority to September 30, 2018. The MAA received \$7.8 million of federal entitlement and discretionary funds in FFY 2017 toward the Concourse B Apron Reconstruction. The MAA has received entitlement AIP funding of \$3.9 million in FFY 2016 toward the Runway Safety Area, Standard and Pavement Improvement Program; \$3.1 million in FFY 2017 toward the Concourse E Passenger Boarding Bridges; and in FFY 2018 \$3.4 million toward the Midfield Area Apron Expansion.

Port of Baltimore

During FY 18 the Maryland Port Administration received \$5.2 million in federal assistance through Water Resources Reform and Development Act to support the Port's dredging activities. MPA received approximately \$0.4 million in other environmental-related grants to support its efforts in being a good steward of our environment. MPA expects to receive approximately \$3.0 million in dredging and environmental grants in FY 19.

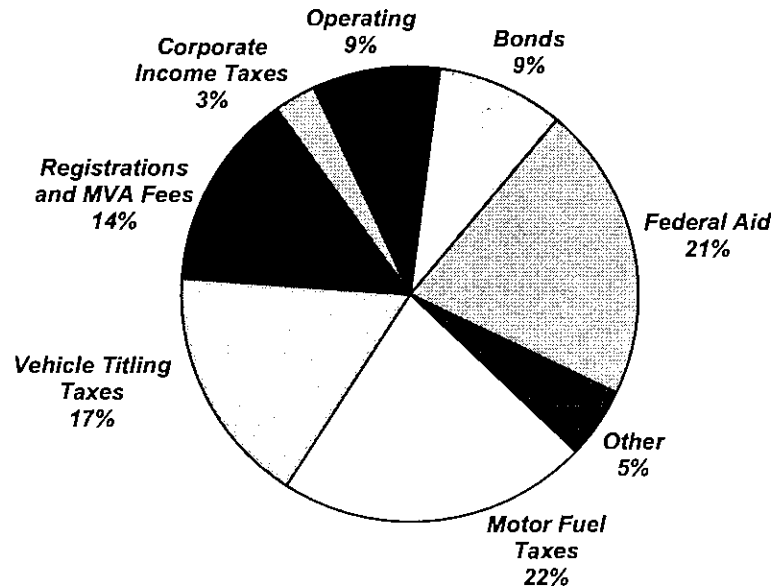
MPA was awarded a TIGER grant of \$10.0 million through U.S. Department of Transportation to support expansion projects at the Port of Baltimore. During FY 17 and 18 MPA completed work eligible for approximately \$7.0 million in federal funding. An additional \$3.0 million is anticipated in FY 19 and 20. This project includes improving channel access, extending rail service and increasing cargo capacity at Fairfield Marine Terminal.

Terminal security efforts are enhanced with federal assistance through the Port Security Grant Program. MPA anticipates federal assistance of approximately \$0.8 million to facilitate several projects to improve security at its terminals.

WHERE THE MONEY COMES FROM...

Maryland's transportation system is funded through several dedicated taxes and fees, federal aid, operating revenues, and bond sales, which are assigned to the Transportation Trust Fund. This fund is separate from the State's General Fund, which pays for most other State government operations and programs. MDOT's customers pay user fees for transportation infrastructure and services through motor fuel taxes, vehicle titling taxes, registration fees, operating revenues, and corporate income taxes. The motor fuel tax and vehicle titling tax are two of the largest sources of MDOT revenue. Operating revenues include transit fares and usage fees generated at the Port of Baltimore and BWI Marshall Airport. In addition to collecting revenue within the State, Maryland also receives federal aid for its transportation program. These federal funds must be authorized by a congressional act. The United States Congress enacted federal surface transportation authorizing legislation, known as the FAST Act, in December 2015, which provides investment in transportation infrastructure through FFY 2020.

Where The Money Comes From

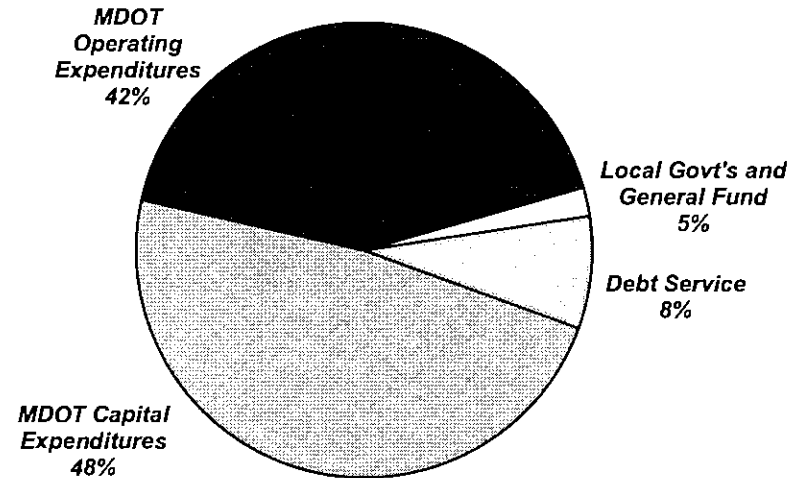


Total projected Trust Fund revenues amount to \$30.4 billion for the six-year period covered by this CTP. These amounts are based on the assumption that the economy will continue along a moderate growth scenario for the next six years.

WHERE THE MONEY GOES...

The MDOT program is fiscally constrained, meaning that the list of projects is tied to estimates of future revenue. The Trust Fund supports operation and maintenance of State transportation systems, administration, debt service, and capital projects. A portion of these funds is directed to the General Fund and a share is also dispersed among Maryland's counties and Baltimore City for local transportation needs. After operating costs, debt service, and local distributions, the remaining money goes toward funding capital projects. This document, Maryland's Draft CTP, is the six-year capital budget for all State transportation projects. This Draft FY 2019 - 2024 CTP totals \$16.0 billion, \$14.1 billion of which comes through the Trust Fund and \$1.9 billion from "Other" fund sources, including local contributions, WMATA direct funding, PFC airport fees, etc.

Where The Money Goes



Planned Capital Expenditures

FY 2019-2024 CTP SUMMARY (\$ MILLIONS)					
	STATE FUNDS	FEDERAL AID	OTHER*	TOTAL	PERCENT OF TOTAL
MDOT TSO	207.2	22.9	5.0	235.1	1.5
MDOT MVA	138.0	0.7	0.0	138.6	0.9
MDOT MAA **	236.3	76.2	220.5	533.0	3.3
MDOT MPA	792.9	6.3	0.0	799.2	5.0
MDOT MTA	1,087.8	1,905.2	237.1	3,230.1	20.1
WMATA	1,182.6	0.0	1,392.2	2,574.8	16.0
MDOT SHA	4,884.6	3,649.3	0.0	8,553.9	53.3
TOTAL	8,529.3	5,660.6	1,854.85	16,044.7	100.0

Note: Figures may not add perfectly due to rounding.

* Funds not received through the Trust Fund. Includes some funds from Maryland Transportation Authority (MDTA), Passenger Facility Charges (PFC), Customer Facility Charges (CFC) and federal funds received directly by WMATA.

** Projects using non-trust fund financing sources are included in the total.

MDOT TSO – Transportation Secretary's Office

MDOT MVA – Motor Vehicle Administration

MDOT MAA – Maryland Aviation Administration

MDOT MPA – Maryland Port Administration

MDOT MTA – Maryland Transit Administration

WMATA – Washington Metropolitan Area Transit Authority

MDOT SHA – State Highway Administration

EVALUATING OUR PERFORMANCE

In 2000, the Maryland General Assembly passed a bill requiring MDOT to develop an Annual Attainment Report (AR) on Transportation System Performance. The main objectives of the AR are to do the following:

- Report on progress toward achieving the goals and objectives in the MTP and the CTP;
- Establish performance indicators that quantify achievement of these objectives; and
- Set performance targets.

The performance measures evolve and are updated periodically in a collaborative effort between the Secretary's Office, the transportation business units, and, every 4-5 years, with an AR Advisory Committee. The performance measures were updated this year, in the spring of 2018, with the AR Advisory Committee, based on the updated 2040 MTP Goals and Objectives (please visit mdot.maryland.gov/ARAC). The AR documents show MDOT is achieving its goals and objectives based on performance indicators and helps Maryland citizens assess improvements to its transportation system.

Since 1996, MDOT has also participated in the State's Managing for Results (MFR) effort as part of the budget process. MFR is a strategic planning, performance measurement, and budgeting process that emphasizes use of resources to achieve measurable results, accountability, efficiency, and continuous improvement in state government programs.

Through coordination with MPOs and adjacent state DOTs, MDOT developed baseline performance measures and targets for the MAP-21/FAST Act federal safety, infrastructure condition, and system performance measures:

- Pavement condition on the Interstate System and on the remainder of the National Highway System (NHS);
- Performance of the Interstate System and the remainder of the NHS;
- Bridge condition on the NHS;
- Fatalities and serious injuries (both number and rate per vehicle miles traveled) on all public roads;
- Traffic congestion;
- On-road mobile source emissions; and
- Freight movement on the Interstate System.

MDOT will continue to work with USDOT, the regional MPOs, and other stakeholders to respond to these new requirements now that the final regulations and guidance have been issued to ensure we demonstrate the effectiveness of MDOT's programs.

Finally, MDOT is internally assessing its performance in meeting our customers' needs through our quarterly MDOT Excellerator Performance Management System. The program is a living, evolving performance process that is in a constant state of evaluation, analysis and action. MDOT reports quarterly on performance results and uses the process to drive daily business decisions.

HOW TO READ THIS DOCUMENT

The Maryland Department of Transportation (MDOT) is organized into transportation business units responsible for different modes of travel. Projects in the Consolidated Transportation Program (CTP) are listed under the transportation business unit responsible for the project's delivery.

For each major project, there is a Project Information Form (PIF). Each PIF contains a description of the project, its status, its justification, its compliance status with smart growth, and a brief explanation of how it fits with the goals of the Maryland Transportation Plan (MTP). It also shows any significant change in the project since the previous year's CTP, as well as the funding for the project over the six-year cycle. The information in each PIF is meant to provide a general description of the project along with some specifics such as alignments, status of environmental permitting, or alternatives under study.

Funding Phases

Planning – Once a proposal is funded for project planning, detailed studies and analyses are conducted to evaluate the need for the project, to establish the scope and location of proposed transportation facilities and to obtain environmental approvals.

Engineering – Engineering projects involve detailed environmental studies and preliminary and final design. Having been through a detailed analysis based on the information from the Project Planning phase, these projects are candidates for future addition to the Construction Program.

Right-of-Way – This funding is to provide the necessary land for the project or to protect corridors for future projects.

Construction – This last stage includes the costs of actually building the designed facility.

Total – This is the sum of any funding shown for Planning, Engineering, Right-of-Way, and Construction.

Federal-Aid – This is the amount of the total that will utilize federal funding.

Construction does not begin until a project receives necessary environmental permits, the State meets air and water quality requirements and the contracts are bid. PIFs can include specific facilities and corridor studies that examine multimodal solutions to transportation needs.

The CTP also contains information on minor projects. These projects are smaller in scope and cost. They also can include road resurfacing, safety improvements, and sidewalk and bicycle trail construction. Following this introduction is an explanation of some of the significant changes from last year's CTP. This section lists major projects added to the CTP or projects that have advanced to a new stage of development. It also lists changes in construction schedules and projects removed from the CTP. The CTP also includes information regarding the economic trends and assumptions and future revenue projects that inform the capital programming process.

PHASE	TOTAL		PROJECT CASH FLOW								SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPENDITURE THRU YEAR	CURRENT YEAR	BUDGET YEAR	FOR PLANNING PURPOSES ONLY				TOTAL			
					2015	2016	2017	2018				
Planning	0	0	0	0	0	0	0	0	0	0	0	
Engineering	77,892	55,392	8,500	6,300	3,200	2,700	1,900	1,900	22,500	0	0	
Right-of-way	20,535	13,365	900	800	2,800	700	1,000	1,000	7,200	0	0	
Construction	388,776	277,976	11,000	9,600	19,000	25,700	22,300	23,200	110,800	0	0	
Total	487,233	346,733	18,400	16,700	25,000	29,100	25,200	26,100	140,500	0	0	
Federal-Aid	129,621	73,221	13,500	1,600	5,400	13,200	10,000	11,800	56,400	0	0	

FY 2018 ACCOMPLISHMENTS
MAJOR PROJECT COMPLETIONS

The Department completed twenty-two major projects in FY 2018, at a total cost of \$690.6 million. These projects are listed below:

<u>PROJECT DESCRIPTION</u>	<u>TOTAL COST</u> <u>(\$ MILLIONS)</u>
Maryland Aviation Administration	
Consolidated Rental Car Facility Shuttle Bus Fleet Replacement	15.0
State Highway Administration	
US 50, John Hanson Highway; US 50, MD 70 to MD 2 (Anne Arundel)	25.0
I-695, Baltimore Beltway; I-695, MD 41 to MD 147 (Baltimore)	34.5
I-695, Baltimore Beltway; I-695, Replace Bridge over Milford Mill Road (Baltimore)	29.6
MD 295, Baltimore Washington Parkway; MD 295, Riverview/Baltimore Highlands Community (Noise Abatement) (Baltimore)	12.7
MD 331, Dover Road; MD 331, Replace Bridge over Choptank River (Caroline, Talbot)	65.3
MD 404, Shore Highway; MD 404, US 50 to MD 309 and west of Hillsboro Road to Holly Road (Caroline, Queen Anne's, Talbot)	157.8
MD 404, Shore Highway; MD 404, West of MD 309 to Cemetery Road. (Caroline, Queen Anne's)	32.6
MD 234, Budds Creek Road; MD 234, Bridge over Gilbert Swamp Run (Charles)	6.3
Maintenance Facility in Cambridge; City of Cambridge (Dorchester)	24.2
MD 22, Aberdeen Thruway; MD 22, at MD 462 (Harford)	21.0
I-95; I-95, Montgomery Road Overpass to I-895 Interchange (Noise Abatement) (Howard)	13.6
MD 195, Carroll Avenue; MD 195, Bridge over Sligo Creek and Sligo Creek Parkway (Montgomery)	14.1
MD 193, University Boulevard; MD 193, Replace Bridge over I-495 (Montgomery)	14.1
MD 187, Old Georgetown Road; MD 187, BRAC Intersection Improvements at West Cedar Lane (Montgomery)	5.7
I-495, Capital Beltway; I-495, Resurface from I-270Y to Seminary Road (Montgomery)	9.4
US 50, John Hanson Highway; US 50, Resurface from east of Lottsford Vista Road to Anne Arundel County Line (Eastbound) (Prince George's)	10.5
MD 4, Pennsylvania Avenue; MD 4, Forestville Road to MD 458 (Silver Hill Road) (Prince George's)	26.1

FY 2018 ACCOMPLISHMENTS
MAJOR PROJECT COMPLETIONS (Cont'd.)

<u>PROJECT DESCRIPTION</u>	<u>TOTAL COST</u> <u>(\$ MILLIONS)</u>
State Highway Administration	
US 50, John Hanson Highway; US 50, Resurface from east of Lottsford Vista Road to Anne Arundel County Line (Westbound) (Prince George's)	10.1
US 301, Blue Star Memorial Highway; US 301, at MD 304 (Queen Anne's)	49.7
US 113, Worcester Highway; US 113, Massey Branch to Five Mile Rd. (Phase 3). (Worcester)	52.5
Maryland Transportation Authority	
US 50/301 Bay Bridge - Cable Rewrapping and Dehumidification	60.8
Total	690.6

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MAJOR BRIDGE PROJECTS (Cont'd.)

<u>PIF LINE#</u>	<u>PROGRAM/PROJECT</u>	<u>DESCRIPTION</u>
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Garrett County

Construction Program

1.	MD 39, Hutton Road -- Secondary	MD 39, Bridge over Youghiogheny River
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Montgomery County

Construction Program

7.	MD 355, Frederick Road -- Secondary	MD 355, Bridge over Little Bennett Creek
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Prince George's County

Construction Program

1.	I-95/I-495, Capital Beltway -- Interstate	I-95/I-495, Bridge over Suitland Road
2.	I-95/I-495, Capital Beltway -- Interstate	I-95/I-495, Bridges over Suitland Parkway
4.	I-95, Capital Beltway -- Primary	I-95, Bridge over MD 214
11.	MD 381, Brandywine Road -- Secondary	MD 381, Bridge over Timothy Branch

Talbot County

Construction Program

1.	MD 331, Dover Road -- Secondary	MD 331, Replace Bridge over Choptank River
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Washington County

Construction Program

1.	I-81, Maryland Veterans Memorial Highway -- Interstate	I-81, Replace Bridge over Potomac River
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Worcester County

Development and Evaluation Program

3.	US 50, Ocean Gateway -- Primary	US 50, Replace Bridge over Sinepuxent Bay
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BICYCLE AND PEDESTRIAN RELATED PROJECTS

MD 210 - at Kerby Hill Road/ Livingston Road	sidewalks	0.1 miles	13,728
	wide curb lanes	0.1 miles	15,000
MD 337 - at MD 218 and I-495 NB off ramp	sidewalks	0.2 miles	27,456
	wide curb lanes	0.2 miles	30,000
MD 381 - Bridge over Timothy Branch	sidewalks	0.1 miles	13,728
	shoulders	0.1 miles	15,000
MD 500 - MD 208 to MD 410	sidewalks	1.2 miles	164,736
<u>Queen Anne's County</u>			
US 301 - at MD 304	shoulders	0.1 miles	15,000
<u>Saint Mary's County</u>			
MD 5 - south of Camp Brown Road to the Roger Station	shoulders	2.2 miles	330,000
MD 5 - at Abell Street/Moakley Street	wide curb lanes	0.2 miles	30,000
<u>Wicomico County</u>			
MD 349 - Bridge over Windsor Creek	sidewalks	0.1 miles	13,728
	shoulders	0.1 miles	15,000

<u>Worcester County</u>			
US 113 - Massey Branch to Five Mile Branch (Phase 3)	shoulders	4.6 miles	690,000
US 113 - Public Landing Road to Five Mile Branch	shoulders	4.3 miles	645,000

wide curb lanes	10.2 miles	sub-total	1,530,000
shoulders	24.5 miles	sub-total	3,675,000
pedestrian bridge	1.0 bridge	sub-total	1,500,000
sidewalks	17.5 miles	sub-total	2,402,400
TOTAL			9,107,400

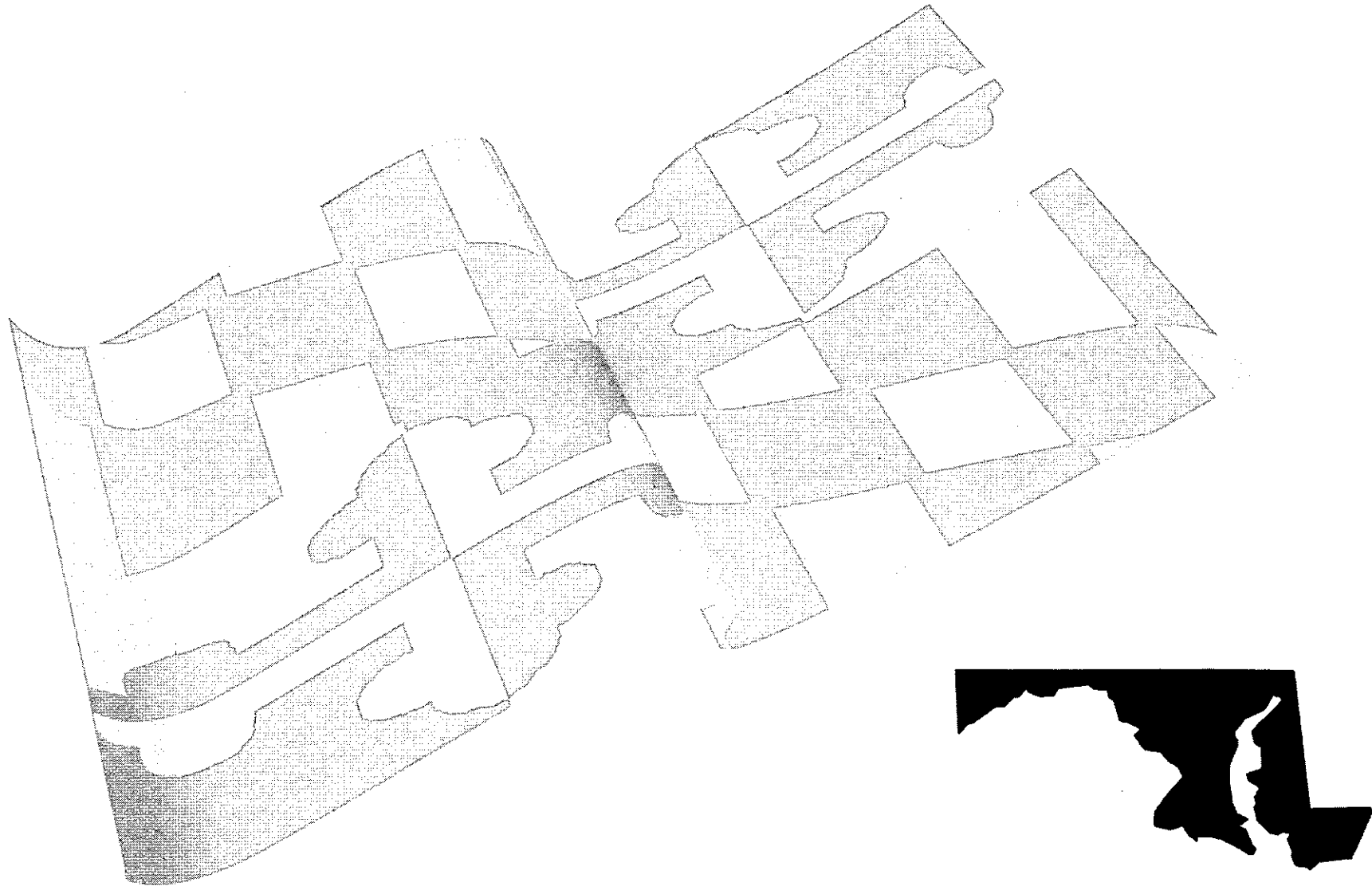
GENERAL AVIATION GRANTS-IN-AID
Fiscal Year 2019

The following is a list of General Aviation Airport Grants in the Consolidated Transportation Program. Additional information can be found under the respective Maryland Aviation Administration section.

MARYLAND AVIATION ADMINISTRATION

<u>COUNTY</u>	<u>AIRPORT</u>	<u>GRANT AMOUNT (\$000's)</u>			<u>Total</u>
		<u>Federal</u>	<u>State</u>	<u>Local/Owner</u>	
Somerset County	Crisfield-Somerset County	164	9	9	182
St. Mary's County	St. Mary's County Regional Airport	6,347	353	353	7,053
Talbot County	Easton Airport	6,372	417	375	7,164
Washington County	Hagerstown Regional Airport	1,000	153	153	1,306
Wicomico County	Salisbury-Ocean City: Wicomico Reg.	345	164	68	577
Worcester County	Ocean City Municipal Airport	517	1,584	1,035	3,136
Total			\$4,272		

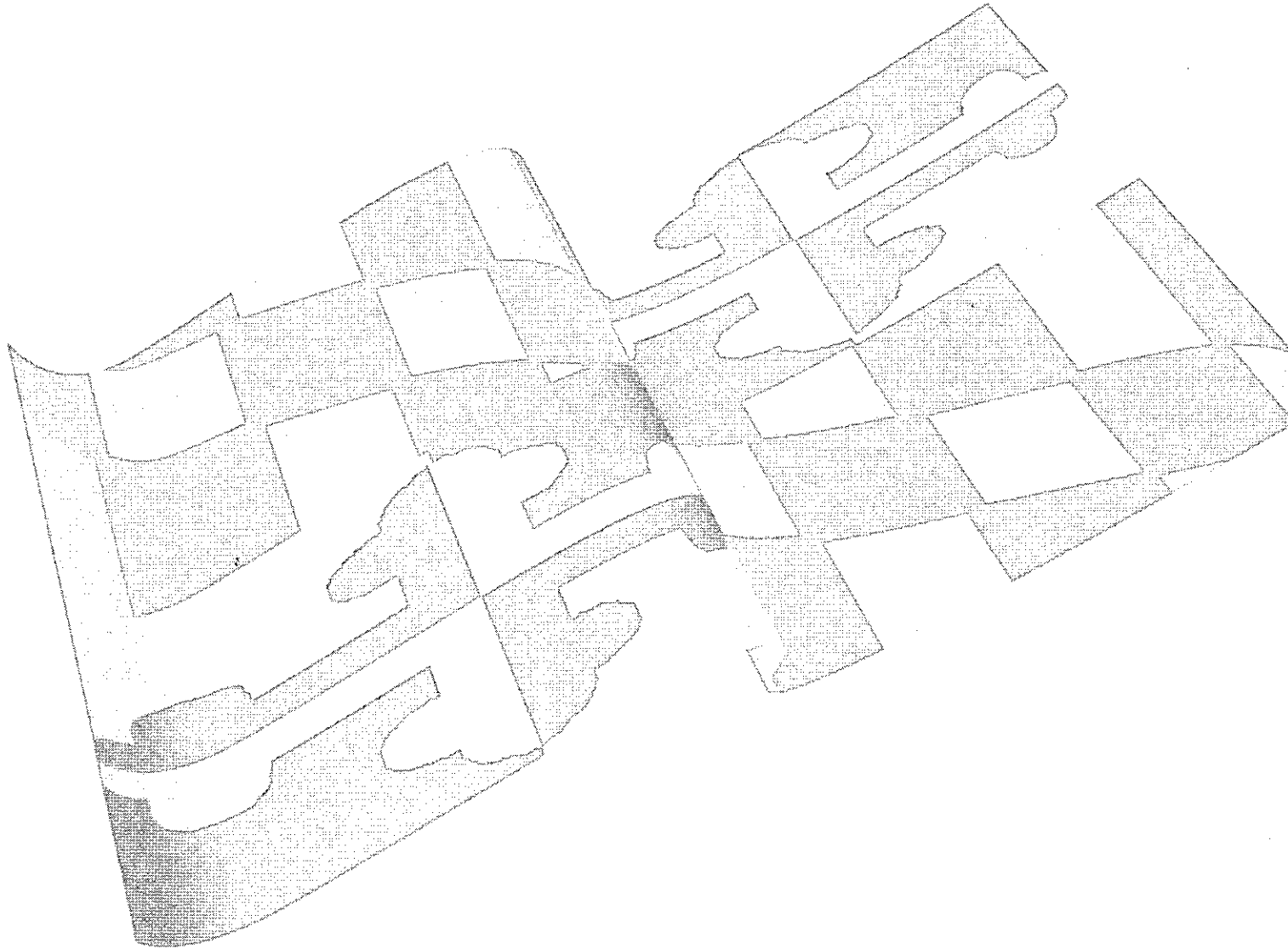
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MULTIMODAL FREIGHT PROJECTS

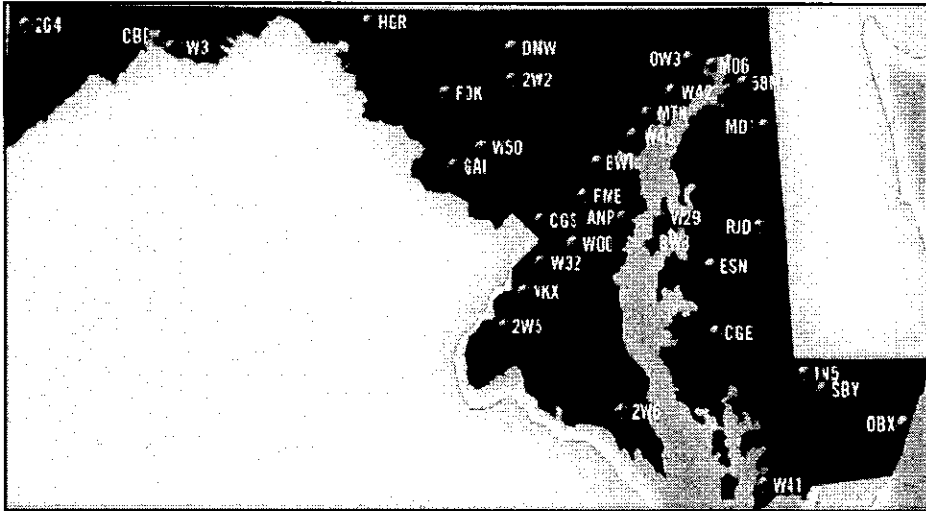
WASHINGTON I-70 interchange improvements study (bridge replacement and capacity improvements) I-81, study to reconstruct I-81 from the West Virginia State Line to the Pennsylvania State Line (bridge replacement and capacity improvements) I-81, widen and rehabilitate bridge over Potomac River	50,761
WORCESTER US 113, capacity improvements	40,369

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MDOT MARYLAND DEPARTMENT OF TRANSPORTATION

MARYLAND AVIATION ADMINISTRATION



PROJECT: Regional Aviation Assistance Program

DESCRIPTION: The Statewide Aviation Grant Program provides State funding to match federal funding to government or public-owned airports for the repair and upgrade of runways, taxiways, ramps, and lighting systems, as well as, for the removal of trees and other obstacles from runway approaches. If the airport meets the program criteria the Federal Aviation Administration (FAA) will cover 90% of the costs with the State and the airport each contributing 5 percent of the total project cost. Assistance is also provided to private-owned airports, open to the public, for financial grants to improve runways, taxiways, navigation aids, and other safety related projects.

JUSTIFICATION: This program supports the goals of the Maryland Department of Transportation to meet air service needs of Maryland and to promote safety and security, environmental stewardship, and economic development. There are over 130 licensed and registered airports in the State, of which 35 are public use facilities with three offering air carrier service.

SMART GROWTH STATUS: Project Not Location Specific Not Subject to PFA Law

- Project Inside PFA
- Project Outside PFA
- PFA Status Yet to Be Determined
- Grandfathered
- Exception Will Be Required
- Exception Granted

ASSOCIATED IMPROVEMENTS:
None.

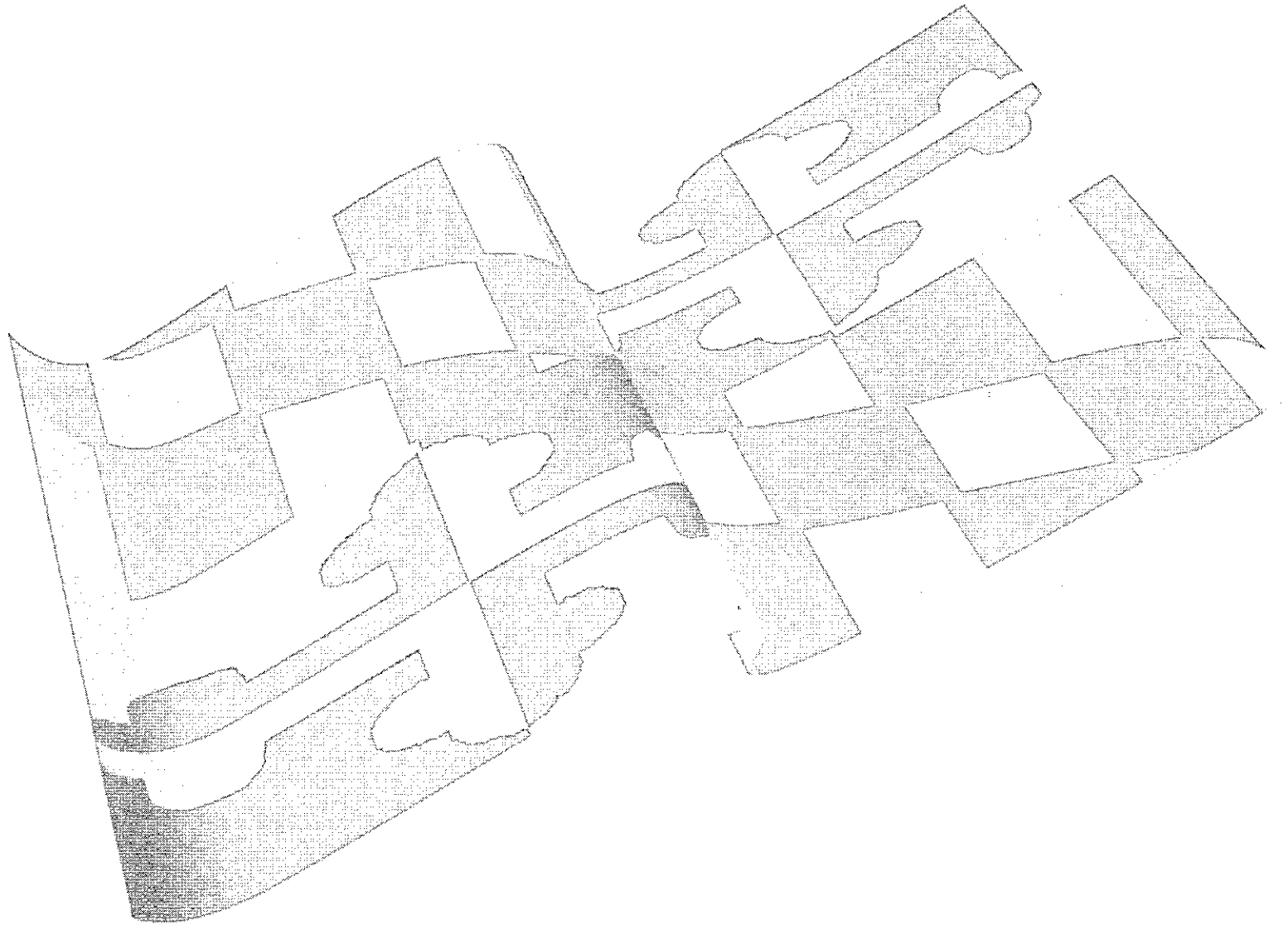
STATUS: Federal fiscal year 2019 grant development underway.

POTENTIAL FUNDING SOURCE: <input checked="" type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input type="checkbox"/> OTHER											
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS					SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2018	CURRENT YEAR 2019	BUDGET YEAR 2020	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY		
Planning	0	0	0	0	0	0	0	0	0	0	0
Engineering	0	0	0	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0	0
Construction	68,298	54,198	2,350	2,350	2,350	2,350	2,350	2,350	2,350	14,100	0
Total	68,298	54,198	2,350	2,350	2,350	2,350	2,350	2,350	2,350	14,100	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2018 - 23 CTP: Added to the Construction Program from the System Preservation Minor Projects Program.

1105, 1106, 1107

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MDOT MARYLAND DEPARTMENT OF TRANSPORTATION

MARYLAND TRANSIT ADMINISTRATION

LOCALLY OPERATED TRANSIT SYSTEMS

MARYLAND TRANSIT ADMINISTRATION -- LINE 46 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL PROJECT COST (\$000's)	CONSTRUCTION START
<u>LOCALLY OPERATED TRANSIT SYSTEMS (cont'd)</u>			
<u>MONTGOMERY COUNTY FY 2018 COMPLETIONS</u>			
1	Bus Replacement (FY16)	2,000	Complete
2	Bus Replacement (FY17)	2,000	Complete
3	Preventive Maintenance (FY18 WAG)	5,600	Complete
4	Ridesharing (FY16)	372	Complete
5	Ridesharing (FY17)	372	Complete
<u>OCEAN CITY FY 2018 COMPLETIONS</u>			
1	1 ADA Cutaway Bus (FY17 5339)	68	Complete
2	1 Heavy Duty Bus 40' (FY16 5311)	454	Complete
3	2 Heavy Duty Buses (FY17 5339)	935	Complete
4	2 Heavy Duty Replacement Buses - 1752, 1756 (FY18 5339)	935	Complete
5	3 Heavy Duty Buses 40' (FY16 5339)	1,362	Complete
6	3 Large Replacement Buses (FY14 5309)	1,362	Complete
7	Bus Surveillance System (FY17 5339)	500	Complete
8	Preventive Maintenance (FY18 5311)	600	Complete
<u>PRINCE GEORGE'S COUNTY FY 2018 COMPLETIONS</u>			
1	Ridesharing (FY16)	269	Complete
2	Ridesharing (FY17)	269	Complete
<u>QUEEN ANNE'S COUNTY FY 2018 COMPLETIONS</u>			
1	2 Small Replacement Buses - 262, 350 (FY18)	134	Complete
2	Preventive Maintenance (FY18)	65	Complete

hh

LOCALLY OPERATED TRANSIT SYSTEMS

MARYLAND TRANSIT ADMINISTRATION -- LINE 46 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL PROJECT COST (\$000's)	CONSTRUCTION START
<u>LOCALLY OPERATED TRANSIT SYSTEMS (cont'd)</u>			
<u>ST MARY'S COUNTY FY 2018 COMPLETIONS</u>			
1	2 Medium Duty Replacement Buses (FY14 5307) Converted to 5 Small Rpcmt Buses	387	Complete
2	2 Medium Replacement Buses (FY13) Converted to 3 Small Rpcmt Buses	233	Complete
3	3 Small Replacement Buses - 41, 42, 44 (FY18)	269	Complete
<u>TALBOT COUNTY FY 2018 COMPLETIONS</u>			
1	2 Small Replacement Buses - 201, 940 (FY18 5339)	158	Complete
2	3 Small Cutaway Buses (FY17)	206	Complete
<u>TRI-COUNTY COUNCIL FOR LOWER EASTERN SHORE FY 2018 COMPLETIONS</u>			
1	3 Small Cutaway Replacement Buses (FY16)	210	Complete
2	Expansion - Small Expansion Small Bus (FY15 5339)	62	Complete
3	Expansion Bus (FY15 5307)	62	Complete
4	Mobility Management (FY17)	143	Complete
5	Mobility Management (FY18 5307)	143	Complete
6	Preventive Maintenance (FY18 5307)	826	Complete
7	Small Cutaway 45 (FY17)	71	Complete
8	Small Cutaway 46 (FY17)	71	Complete
9	Small Cutaway 70 (FY17)	71	Complete
<u>WASHINGTON COUNTY FY 2018 COMPLETIONS</u>			
1	1 Small Replacement Bus (FY16)	74	Complete
2	Fuel Monitoring System (FY16)	16	Complete
3	On-Board Surveillance Cameras	80	Complete
4	On-Vehicle Video Surveillance	82	Complete
5	Small Bus Replacement (FY15)	70	Complete

Sh

LOCALLY OPERATED TRANSIT SYSTEMS

MARYLAND TRANSIT ADMINISTRATION -- LINE 46 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	FY19 + FY20 PROJECT COST (\$000's)	CONSTRUCTION START
<u>LOCALLY OPERATED TRANSIT SYSTEMS (cont'd)</u>			
<u>DORCHESTER COUNTY FY 2019 AND 2020</u>			
1	(2) Gas Engines (FY19 5311)	7	FY 2019
2	(2) Gas Transmissions (FY19 5311)	7	FY 2019
3	Preventive Maintenance (FY19)	50	FY 2019
4	Radios (14) (FY18)	50	FY 2019
5	1 Medium Duty Replacement Bus (FY13)	113	FY 2020
6	Preventive Maintenance (FY17)	75	Ongoing
7	Circuit Tester (FY15)	1	Underway
<u>EASTERN SHORE NON-PROFITS FY 2019 AND 2020</u>			
1	Delmarva Community Services - Mobility Management (FY16/17 5310)	324	Ongoing
2	Delmarva Community Services - VTCLI One Call/One Click Center (FY15)	500	Ongoing
3	New Freedom Program	369	Ongoing
4	Worcester County Comm on Aging - Mobility Management (FY18 5310 SS)	45	Ongoing

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LOCALLY OPERATED TRANSIT SYSTEMS

MARYLAND TRANSIT ADMINISTRATION -- LINE 46 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	FY19 + FY20 PROJECT COST (\$000's)	CONSTRUCTION START
<u>LOCALLY OPERATED TRANSIT SYSTEMS (cont'd)</u>			
<u>HOWARD COUNTY FY 2019 AND 2020</u>			
1	Ridesharing (FY19)	131	FY 2019
2	1 Heavy Duty Replacement Bus - 9533 (FY19 5339)	365	FY 2020
3	1 Heavy Duty Replacement Bus - 9537 (FY19 5339)	365	FY 2020
4	Pilot Rideshare Assistance (FY16)	197	Ongoing
5	Ridesharing (FY17)	131	Ongoing
6	Central Maryland Operations Facility	750	Underway
7	Electric Bus Project	3,778	Underway
8	Fleet Maintenance Plan (FY16)	75	Underway
9	Transportation Development Plan (FY16)	90	Underway
<u>MONTGOMERY COUNTY FY 2019 AND 2020</u>			
1	Bus Replacement (FY18 WAG)	2,000	FY 2019
2	Ridesharing (FY19)	372	FY 2019
3	Bus Replacement (FY19 WAG)	2,000	FY 2020
4	Ridesharing (FY18)	372	Ongoing
<u>OCEAN CITY FY 2019 AND 2020</u>			
1	Preventive Maintenance (FY19 5311)	600	FY 2019
2	Bus Barn D&E	1,250	Underway
3	Bus Barn Fire Suppression (FY13 5309)	15	Underway
4	Facility Construction Oversight (FY19 5311)	488	Underway
5	Transit Campus Construction (FY18 5311)	5,000	Underway
6	Transit Campus Construction (FY19 5311)	7,500	Underway
7	Transit Facility D & E (FY17)	520	Underway

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LOCALLY OPERATED TRANSIT SYSTEMS

MARYLAND TRANSIT ADMINISTRATION -- LINE 46 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	FY19 + FY20 PROJECT COST (\$000's)	CONSTRUCTION START
<u>LOCALLY OPERATED TRANSIT SYSTEMS (cont'd)</u>			
<u>TRI-COUNTY COUNCIL FOR LOWER EASTERN SHORE FY 2019 AND 2020</u>			
1	1 Medium Replacement Bus - 406 (FY19 5339)	119	FY 2019
2	1 Medium Replacement Bus - 407 (FY19 5339)	119	FY 2019
3	2 Medium Replacement Buses (FY18 5307)	236	FY 2019
4	Mobility Management (FY19 5307)	143	FY 2019
5	Preventive Maintenance (FY19 5307)	800	FY 2019
6	Small Replacement Bus - 55 (FY19 5339)	72	FY 2019
7	ADP Software - Vehicle Maintenance Records (FY14)	40	Underway
8	Bus Wash Equipment (FY18 5307)	500	Underway
9	EAM Maintenance Software (FY15)	80	Underway
10	Facility Construction Phase III	1,557	Underway
11	Trapeze Call Back Module (FY14)	30	Underway
12	Trapeze Cert. Module (FY14)	16	Underway
<u>WASHINGTON COUNTY FY 2019 AND 2020</u>			
1	Preventive Maintenance (FY19 5307)	300	FY 2019
2	1 Medium Replacement Bus - 705 (FY19 5339)	249	FY 2020
3	1 Medium Replacement Bus - 706 (FY19 5339)	249	FY 2020
4	2 Medium Replacement Buses - 701, 702 (FY18 5307)	652	FY 2020
5	2 Medium Replacement Buses - 703, 704 (FY18 5307)	652	FY 2020
6	Preventive Maintenance (FY17)	285	Ongoing
7	Preventive Maintenance (FY18 5307)	300	Ongoing
8	Passenger Shelter Installs	100	Underway
9	Route Match Fixed Route System (FY17)	310	Underway
10	Route Match Notification System	27	Underway

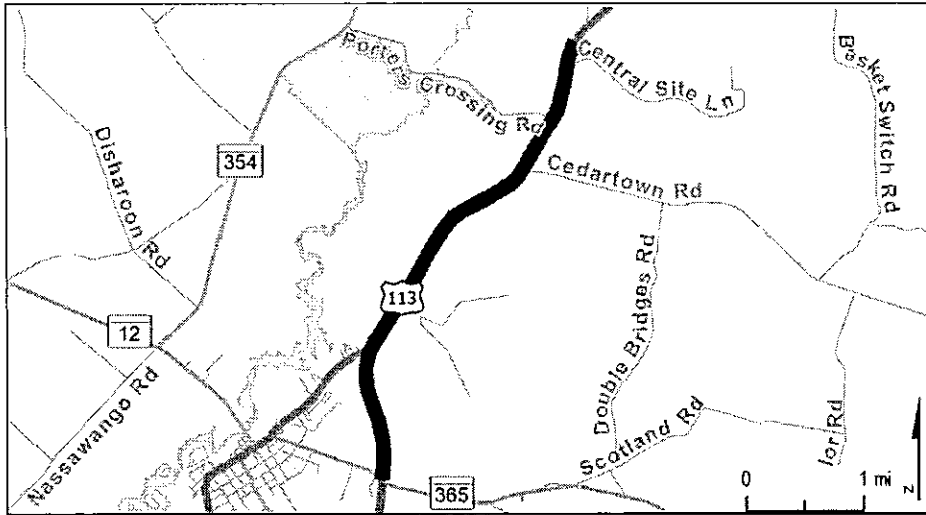
48



WORCESTER COUNTY

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Tab



PROJECT: US 113, Worcester Highway

DESCRIPTION: Upgrade existing US 113 as a four lane divided highway, including access controls from north of MD 365 (Public Landing Road) to Five Mile Branch (Phase 4)(4.3 miles). Bicycle and pedestrian accommodations will be included where appropriate.

PURPOSE & NEED SUMMARY STATEMENT: The US 113 corridor is experiencing deterioration in safety and operations due to increasing seasonal traffic volumes coupled with local commercial/residential development along the highway. This project will improve the highway's safety, operations, and freight movement.

SMART GROWTH STATUS: Project Not Location Specific Not Subject to PFA Law
 Project Inside PFA Grandfathered
 Project Outside PFA Exception Will Be Required
 PFA Status Yet To Be Determined Exception Granted

ASSOCIATED IMPROVEMENTS:
 US 113, Massy Branch to Five Mile Branch (Phase 3) (Line 2)

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Safety & Security
- System Preservation
- Quality of Service
- Environmental Stewardship
- Community Vitality
- Economic Prosperity

EXPLANATION: This project will improve safety, operations, and freight movement.

STATUS: Construction underway.

SIGNIFICANT CHANGE FROM FY 2018 - 23 CTP: The cost decrease of \$6.8 million is due to reduced property and utility needs.

PHASE	TOTAL				PROJECT CASH FLOW					SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2018	CURRENT YEAR 2019	BUDGET YEAR 2020	FOR PLANNING PURPOSES ONLY						
					.....2021.....	.....2022.....	.....2023.....	.....2024.....			
Planning	0	0	0	0	0	0	0	0	0	0	0
Engineering	5,265	5,265	0	0	0	0	0	0	0	0	0
Right-of-way	8,995	4,443	2,811	1,741	0	0	0	0	4,552	0	0
Construction	59,905	24,123	18,126	17,656	0	0	0	0	35,782	0	0
Total	74,165	33,831	20,937	19,397	0	0	0	0	40,334	0	0
Federal-Aid	71,779	32,120	20,490	19,169	0	0	0	0	39,659	0	0

CLASSIFICATION:

STATE - Intermediate Arterial
 FEDERAL - Other Principal Arterial

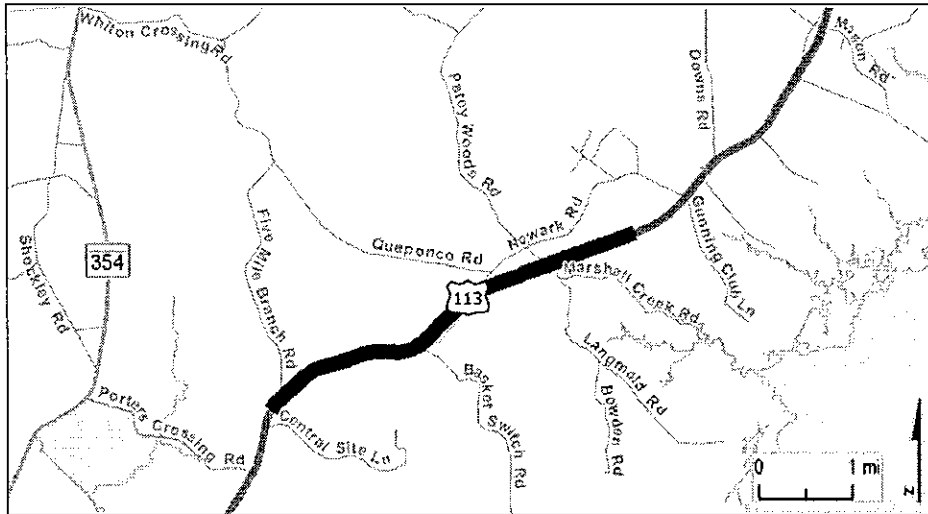
STATE SYSTEM: Primary

Annual Average Daily Traffic (vehicles per day)

CURRENT (2018) - 8,000 - 11,975

PROJECTED (2040) - 11,625 - 17,450

50



PROJECT: US 113, Worcester Highway

DESCRIPTION: Upgrade existing US 113 as a 4 lane divided highway, Massey Branch to Five Mile Branch (Phase 3) (4.6 miles). Bicycle and pedestrian accommodations will be included where appropriate.

PURPOSE & NEED SUMMARY STATEMENT: The US 113 corridor is experiencing deterioration in safety and operations due to increasing seasonal traffic volumes coupled with local commercial/residential development along the highway. This project will improve the highway's safety, operations, and freight movement.

SMART GROWTH STATUS: Project Not Location Specific Not Subject to PFA Law
 Project Inside PFA Grandfathered
 Project Outside PFA Exception Will Be Required
 PFA Status Yet To Be Determined Exception Granted

ASSOCIATED IMPROVEMENTS:
 US 113, Public Landing Rd. to Five Mile Branch (Phase 4) (Line 1)

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:
 Safety & Security Environmental Stewardship
 System Preservation Community Vitality
 Quality of Service Economic Prosperity

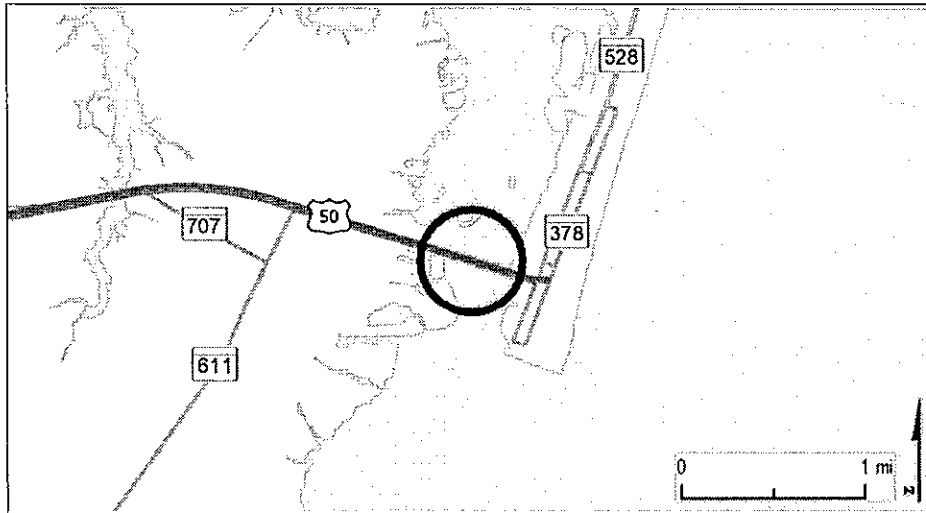
EXPLANATION: This project will improve safety, operations, and freight movement.

STATUS: Open to Service.

SIGNIFICANT CHANGE FROM FY 2018 - 23 CTP: None.

POTENTIAL FUNDING SOURCE:		<input checked="" type="checkbox"/> SPECIAL <input checked="" type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input type="checkbox"/> OTHER								
PHASE	TOTAL		PROJECT CASH FLOW							
	ESTIMATED COST (\$000)	EXPEND THRU 2018	CURRENT YEAR 2019	BUDGET YEAR 2020	FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
					.....2021.....	.....2022.....	.....2023.....	.....2024.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	3,207	3,207	0	0	0	0	0	0	0	0
Right-of-way	12,298	9,748	2,550	0	0	0	0	0	2,550	0
Construction	36,972	36,972	0	0	0	0	0	0	0	0
Total	52,477	49,927	2,550	0	0	0	0	0	2,550	0
Federal-Aid	40,964	39,204	1,760	0	0	0	0	0	1,760	0

CLASSIFICATION:
 STATE - Intermediate Arterial
 FEDERAL - Other Principal Arterial
STATE SYSTEM: Primary
Annual Average Daily Traffic (vehicles per day)
 CURRENT (2018) - 11,975
 PROJECTED (2040) - 17,450



PROJECT: US 50, Ocean Gateway

DESCRIPTION: Study to replace Bridge 23007 over the Sinepuxent Bay. The study will investigate options to eliminate/upgrade the drawspan structure.

JUSTIFICATION: The drawspan is estimated to have 15 to 20 years of life span left. This high traffic volume arterial has experienced mechanical problems with the drawbridge during peak seasonal traffic. This project would improve the highway's safety and operations.

SMART GROWTH STATUS: Project Not Location Specific Not Subject to PFA Law
 Project Inside PFA Grandfathered
 Project Outside PFA Exception Will Be Required
 PFA Status Yet To Be Determined Exception Granted

ASSOCIATED IMPROVEMENTS:

STATUS: Planning complete.

SIGNIFICANT CHANGE FROM FY 2018 - 23 CTP: None.

PHASE	TOTAL		PROJECT CASH FLOW							SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2018	CURRENT YEAR 2019	BUDGET YEAR 2020	FOR PLANNING PURPOSES ONLY						
					2021	2022	2023	2024			
Planning	2,907	2,907	0	0	0	0	0	0	0	0	0
Engineering	0	0	0	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0	0
Construction	0	0	0	0	0	0	0	0	0	0	0
Total	2,907	2,907	0	0	0	0	0	0	0	0	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0

CLASSIFICATION:

STATE - Principal Arterial

FEDERAL - Other Principal Arterial

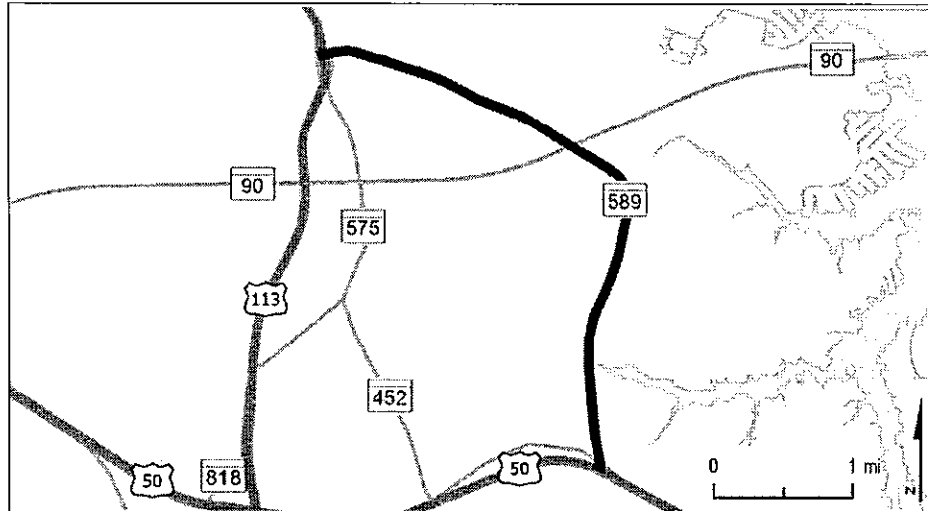
STATE SYSTEM ; Primary

Annual Average Daily Traffic (vehicles per day)

CURRENT (2018) - 22,450
36,875 (Summer)

PROJECTED (2040) - 34,700
57,000 (Summer)

57



PROJECT: MD 589, Racetrack Road

DESCRIPTION: Study for potential improvements to the existing MD 589 corridor from US 50 to US 113 (4.7 miles).

JUSTIFICATION: This project will relieve traffic congestion and improve traffic safety along MD 589 and at the US 50 intersection.

SMART GROWTH STATUS: Project Not Location Specific Not Subject to PFA Law
 Project Inside PFA Grandfathered
 Project Outside PFA Exception Will Be Required
 PFA Status Yet To Be Determined Exception Granted

ASSOCIATED IMPROVEMENTS:

STATUS: Feasibility study complete.

SIGNIFICANT CHANGE FROM FY 2018 - 23 CTP: None.

POTENTIAL FUNDING SOURCE:		<input checked="" type="checkbox"/> SPECIAL <input checked="" type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input type="checkbox"/> OTHER								
PHASE	PROJECT CASH FLOW									
	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2018	CURRENT YEAR 2019	BUDGET YEAR 2020	FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
					.....2021.....	.....2022.....	.....2023.....	.....2024.....		
Planning	1,417	1,417	0	0	0	0	0	0	0	0
Engineering	0	0	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	0	0	0	0	0	0	0	0	0	0
Total	1,417	1,417	0	0	0	0	0	0	0	0
Federal-Aid	246	246	0	0	0	0	0	0	0	0

CLASSIFICATION:

STATE - Minor Arterial

FEDERAL - Minor Arterial

STATE SYSTEM : Secondary

Annual Average Daily Traffic (vehicles per day)

CURRENT (2018) - 16,625
21,650 (Summer)

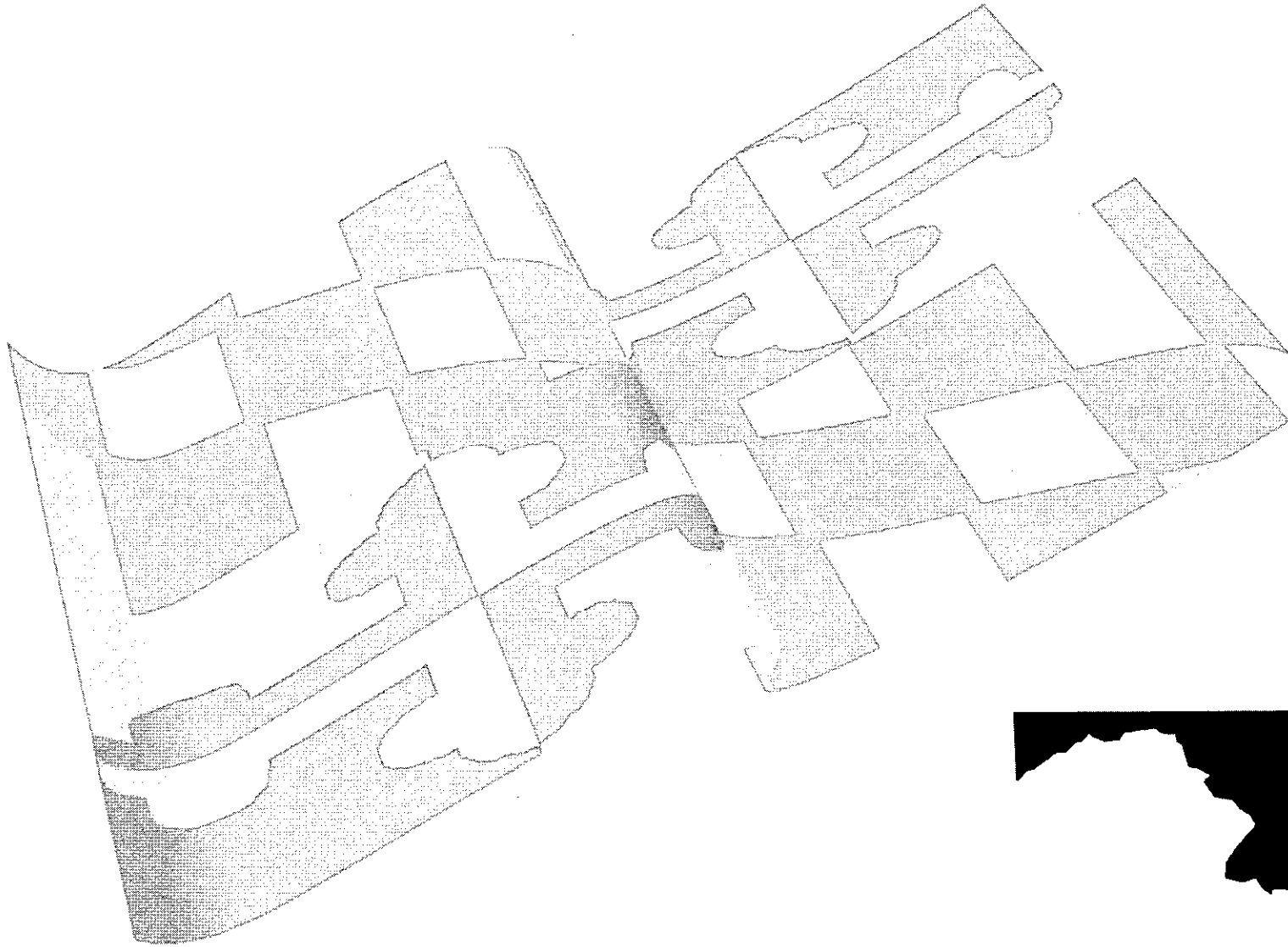
PROJECTED (2040) - 20,700
28,150 (Summer)

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SAFETY, CONGESTION RELIEF, HIGHWAY AND BRIDGE PRESERVATION PROGRAM

STATE HIGHWAY ADMINISTRATION -- WORCESTER COUNTY LINE 5

ITEM NO.	ROUTE NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL 2018 PROJECT COST (\$000's)	CONSTRUCTION START Status as of August 1, 2018
<u>Fiscal Year 2019 and 2020</u>				
<u>Resurface/Rehabilitate</u>				
1		At various locations in Worcester County; mill and resurface	9,893	FY 2019
2		At various locations in Worcester County; mill and resurface	11,798	Under construction
3	US 113	Worcester Highway; North of Hayes Landing Road to US 50; safety and resurfacing	2,330	FY 2019
4	MD 528	Coastal Highway; 62nd Street to 26th Street; resurface	6,900	Under construction
<u>Safety/Spot Improvement</u>				
5		MD 346 and MD 589; drainage improvement	708	FY 2019
6	US 13 BUS	Salisbury Boulevard; at Firehouse Wetland site along US 113, north of Church Branch; landscape	199	Under construction
7	US 50	Ocean Gateway; At MD 589; geometric improvements	3,800	FY 2019
<u>Urban Reconstruction</u>				
8	MD 528	Coastal Highway; from 62nd Street/MD 90 (Ocean City Expressway) to Convention Center Drive; urban reconstruction	143	FY 2019
<u>C.H.A.R.T. Projects</u>				
9		US 50 and MD 90 - CHART DMS deployment; miscellaneous	706	Under construction



APPENDIX A - CHAPTER 30 SCORES

**Appendix A: Chapter 30 Project Scores
Draft FY 19-24 CTP**

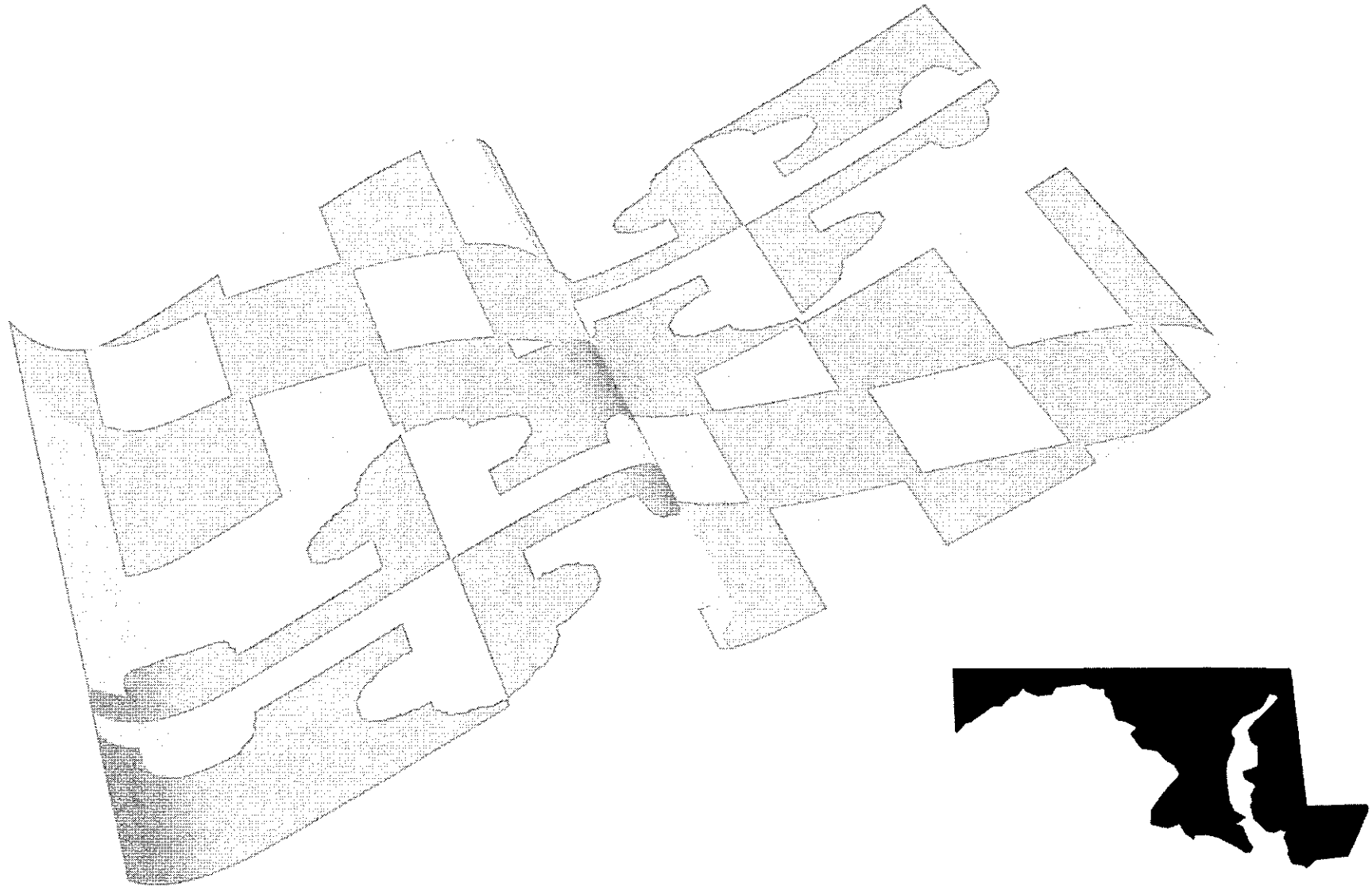
The Maryland Department of Transportation (MDOT) evaluates major transportation projects through a project-based scoring model as required by the Maryland Open Transportation Investment Decision Act – Application and Evaluation (Chapter 30). Major Transportation projects are defined as highway and transit capacity projects over \$5 million. The major transportation projects evaluated through the scoring model in this year's CTP and their corresponding scores are shown below. Please note that the maximum allowable score for any project was determined to be 500. The project prioritization model required under Chapter 30 does not select major transportation projects for funding but is one of many tools MDOT utilizes in its project evaluation and selection process.

<u>Project Name</u>	<u>Sponsor</u>	<u>Score</u>	<u>Rank</u>
I-270 and I-495 (US 40 to Potomac River)	MDOT SHA	500.00	1
US 40 & Otsego Street/Ohio Street Intersection	Harford County	106.38	2
MD 170 (MD 100 to south of MD 174)	Anne Arundel County	34.36	3
MD 713 (MD 175 to MD 176)	Anne Arundel County	32.59	4
MD 117 (I-270 to Watkins Mill Road)	Montgomery County	28.40	5
MD 175 (US 1 to Dorsey Run Road)	Howard County	22.64	6
MD 214 (MD 468 to Oakford Avenue)	Anne Arundel County	21.51	7
MD 177 (Catherine Avenue to Edwin Raynor Boulevard)	Anne Arundel County	21.50	8
MD 18 (Castle Marina Road to the Kent Narrows)	Queen Anne's County	17.21	9
US 29 (Middle Patuxent River to Seneca Drive)	Howard County	15.99	10
US 220 and MD 53 at Cresaptown	MDOT SHA	15.86	11
MD 85 (English Muffin Way to Guilford Drive)	Frederick County & City of Frederick	15.38	12
MD 586 Veirs Mill Road Bus Rapid Transit	Montgomery County	14.96	13
I-95 (MD 32 to MD 100)	MDOT SHA	14.60	14
MD 175 (MD 170 to west of MD 295)	Anne Arundel County	13.57	15
US 15/US 40 (I-70 To MD 26)	Frederick County & City of Frederick	13.08	16
MD 355 (MD 27 to Stringtown Road)	Montgomery County	12.38	17
I-81 (North of MD 63/68 to Pennsylvania state line)	Washington County	8.25	18
MD 26 (MD 32 to Liberty Reservoir)	Carroll County	7.67	19
MD 198 (MD 295 to MD 32)	Anne Arundel County	7.32	20
MD 210 (MD 228 to Kerby Hill Road)	Prince George's County	6.23	21
MD 295 (I-195 to MD 100) and Hanover Road (MD 170 to CSX Railroad)	Anne Arundel County	5.42	22
MD 32 (MD 26 to Howard County line)	Carroll County	5.37	23
I-70 (I-270 to Alternate US 40)	Frederick County & City of Frederick	4.61	24
MD 197 (MD 450 to Kenhill Drive)	Prince George's County	4.57	25
US 29 @ Musgrove/Fairland Interchange	MDOT SHA	4.45	26

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**Appendix A: Chapter 30 Project Scores
Draft FY 19-24 CTP**

<u>Project Name</u>	<u>Sponsor</u>	<u>Score</u>	<u>Rank</u>
US 29 @ Industrial Parkway/Tech Road Interchange	MDOT SHA	4.38	27
I-95/I-495 @ Greenbelt Metro Station Interchange	Prince George's County	3.97	28
I-70 (US 29 to US 40)	Howard County	3.86	29
MD 2 and MD 4 (north of Stoakley Road to south of MD 765A)	MDOT SHA	3.82	30
BWI Rail Station and Fourth Track	MDOT MTA	3.60	31
MD 650 New Hampshire Avenue Bus Rapid Transit	Montgomery County	3.59	32
MD 223 (MD 4 to Steed Road)	Prince George's County	3.19	33
I-795 @ Dolfield Boulevard Interchange	Baltimore County	3.09	34
US 29 @ Stewart Lane Interchange	MDOT SHA	2.70	35
MD 5 (north of I-95/I-495 to US 301)	Prince George's County	2.53	36
MD 4 (I-95/I-495 to MD 223)	Prince George's County	2.24	37
Southern Maryland Rapid Transit (SMRT)	Charles County & Prince George's County	2.03	38
US 301 @ MD 5 Business/MD 228	MDOT SHA	2.03	39
MD 4 Governor Thomas Johnson Bridge	MDOT SHA	1.91	40
MD 213 Bypass Chester River Boulevard	Kent County	1.66	41
East-West Light Rail Transit	Baltimore County	1.45	42



GLOSSARY

CONSOLIDATED TRANSPORTATION PROGRAM GLOSSARY

State Report on Transportation (SRT)	Consists of the Maryland Transportation Plan (MTP) and the Consolidated Transportation Program (CTP).
Maryland Transportation Plan (MTP)	The MTP identifies the focus of the Department and its modal administration that defines program objectives and serves to guide program development. It includes a 20-year forecast of needs based on anticipated resources available to the Department.
CHART	Coordinated Highways Action Response Team – Maryland’s program to employ Intelligent Vehicle Highway System (IVHS) technology to better manage highway capacity.
Consolidated Transportation Program (CTP)	The CTP designates capital projects that will be undertaken during the six-year period, and a summary of operating programs. The CTP also identifies projects added to the Construction Program and Development and Evaluation Program, delayed in schedule, deleted from the Development and Evaluation Program and finally completions.
Construction Program	List within the CTP of major projects (descriptions, cost estimates and schedules) under construction and those anticipated to begin construction within the six-year period. An estimate of annual expenditure levels for system preservation projects is also included.
Development & Evaluation Program (D&E)	List within the CTP of projects for planning studies, preparation of environmental studies and preliminary design. These projects are candidates for future addition to the Construction Program.
Remaining Cost to Complete	Amount of funds required after the budget year to complete a project.
Balance to Complete	Amount of funds required after the six-year program period of the CTP to complete a project.
Major Capital Project	New, expanded or significantly improved facility or service that generally involves planning, environmental studies, design, right-of-way acquisition, construction or purchase of essential equipment related to the facility or service.
System Preservation Project	Project of limited scope for the preservation or rehabilitation of an existing facility or service which generally does not have a significant impact on the human or natural environment.

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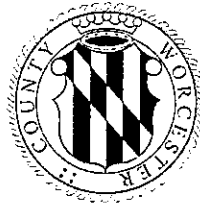
CONSOLIDATED TRANSPORTATION PROGRAM GLOSSARY (Cont'd.)

Reconstruction	Complete rebuilding of a facility/structure or system which is beyond the point where it may be economically repaired or renovated.
Rehabilitation	Restoration and/or modernization of a facility/structure or system in order that it may be effectively used for its designated functional purpose or comply with current requirements.
Highway System Preservation Program	Program of projects oriented toward preserving the existing highway system, including resurfacing, safety improvements, bridge rehabilitation, landscaping, traffic control and ridesharing lots and other miscellaneous improvements.
Reimbursables	State funds advanced for work performed for local jurisdictions, services performed for other agencies, recovery of damages to Department property, salaries and expenses of the Interstate Division for Baltimore City, fees for issuing commercial entrance permits, and other activities performed by the Department and are paid by various sources.
Capital Contributions Agreement	Agreement entered into by 8 local jurisdictions in Maryland, Virginia and the District of Columbia that provides a capital funding schedule for Metrorail construction in the Washington area.
(PP)	Project Planning: The state in the planning process where detailed studies and analysis are conducted to establish the scope and location of proposed transportation facilities.
(PE)	Preliminary Engineering: The state in project development when surveys, soil conditions, elevations, right-of-way plats, and detailed design plans and specifications are prepared.
(RW)	Right-of-Way: Acquisition of land for transportation projects.
(CO)	Construction.
(IN)	Inflated Cost.
(FA)	Federal-aid.
(STP)	Surface Transportation Program category of federal aid
(NHS)	National Highway System category of federal aid.

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CONSOLIDATED TRANSPORTATION PROGRAM GLOSSARY (Cont'd.)

(IM)	Interstate Maintenance category of federal aid.
(BR)	Bridge Replacement/Rehabilitation category of federal aid.
(CMAQ)	Congestion Mitigation/Air Quality category of federal aid.
(DEMO)	Specific projects identified in federal legislation for demonstration purposes.



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COMMISSIONERS
DIANA PURNELL, PRESIDENT
THEODORE J. ELDER, VICE PRESIDENT
ANTHONY W. BERTINO, JR.
MADISON J. BUNTING, JR.
JAMES C. CHURCH
MERRILL W. LOCKFAW, JR.
JOSEPH M. MITRECIC

OFFICE OF THE
COUNTY COMMISSIONERS

HAROLD L. HIGGINS, CPA
CHIEF ADMINISTRATIVE OFFICER
MAUREEN F.L. HOWARTH
COUNTY ATTORNEY

Worcester County

GOVERNMENT CENTER
ONE WEST MARKET STREET • ROOM 1103
SNOW HILL, MARYLAND
21863-1195

Scheduled at
11:30 AM

PROCLAMATION

WHEREAS, the Worcester County Commissioners join with the Worcester County Fire Marshal's Office and area fire agencies to recognize National Fire Prevention Week, October 7-13, 2018, and support the initiatives of this year's theme *Look. Listen. Learn. Be aware - fire can happen anywhere*; and

WHEREAS, the National Fire Protection Association's 2018 campaign calls for all to educate themselves about three basic, but essential steps to take to reduce the likelihood of a fire - **LOOK** for places a fire could start, **LISTEN** for the sound of the smoke alarm, and **LEARN** two ways out of every room - to escape safely in the event a fire occurs.

NOW, THEREFORE, we the County Commissioners of Worcester County, Maryland, proclaim October 7-13, 2018 as **Worcester County Fire Prevention Week** and recognize October as **National Fire Prevention Month**. To learn more about fire safety, visit www.nfpa.org.

Executed under the Seal of the County of Worcester, State of Maryland, this 2nd day of October, in the Year of Our Lord Two Thousand and Eighteen.



Diana Purnell, President

Theodore J. Elder, Vice President

Anthony W. Bertino, Jr.

Madison J. Bunting, Jr.

James C. Church

Merrill W. Lockfaw, Jr.

Joseph M. Mitrecic

Citizens and Government Working Together

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NOTICE OF PUBLIC HEARING
FOR AGRICULTURAL EASEMENT ACQUISITION
Worcester County, Maryland

Notice is hereby given that the Worcester County Commissioners will hold a

Public Hearing

on

Tuesday, October 2, 2018 at 11:35 AM

in the

COUNTY COMMISSIONERS MEETING ROOM

Room 1101- Government Center,

One West Market Street,

Snow Hill, MD 21863

The purpose of this public hearing is to hear comments on petitions to sell an agricultural easement to the Maryland Agricultural Land Preservation Foundation (MALPF) on the following properties in Worcester County:

1. 23-19-01; GMR LLC; Tax Map (TM) 91, Grid 14, Parcel (P) 14; 934 Bishop Road, Pocomoke City; 224.1 acres
2. 23-19-02; Shockley, Stephen; TM 38, Grid 5, P 26; Davis Branch Road; 91.3 acres
3. 23-19-03; Shockley, Patrick; Ralph Shockley and David and Sandra Shockley as Trustees; TM 38, Grid 5, P 15; Shockley Road, Snow Hill; 390.2 acres
4. 23-19-04; Fair, Freddie R. and Faye H.; TM 93, Grid 16, P 41; W side Steel Pond Rd, Stockton; 102.0 acres
5. 23-19-05; Gross, Frederick Mark; TM 32, Grid 16, P 217; Harrison Road, Berlin; 210.13 acres
6. 23-19-06; Powell, Harry and Annette; TM 56, Grid 15, P 25; Timmons Road, Snow Hill; 281.2 acres
7. 23-19-07; Shockley, Samuel (Glenn) and Brenda; TM 38, Grid 23; P 4; 5910 Laws Road, Snow Hill; 153.9 acres
8. 23-19-08; Holland, Glenn and Jean; TM 99, Grid 12, P 35; 1975 Colona Road, Pocomoke City; 153.91 acres
9. 23-19-09; Holland, Everett and Mark Holland; TM 99, Grid 11, P 39; 1546 Colona Road, Pocomoke City; 176.04 acres

Additional information is available for review at the Department of Environmental Programs, Worcester County Government Center, Suite 1306 (3rd floor), One West Market Street, Snow Hill, Maryland, 21863 during regular business hours of 8:00 am to 4:30 pm . Questions may be directed to Katherine Munson, Planner V, by calling (410) 632-1220, extension 1302 or email at kmunson@co.worcester.md.us.

County Commissioners of Worcester County, Maryland

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Worcester County
Department of Environmental Programs

Potential Public Hearing Dates:
- October 2 - Advertise once
- October 23 - Advertise twice

Memorandum

To: Harold L. Higgins, Chief Administrative Officer

From: Robert J. Mitchell, LEHS 
Director, Environmental Programs

Subject: Public Hearing for Proposed FY 17/18 MALPF Easement Applications

Date: August 9, 2014

Attached you will find a memorandum from Katherine Munson, of my staff with regard to the next round of easement applications. We are requesting a public hearing be scheduled for the review of FY 19 Maryland Agricultural Land Preservation Foundation (MALPF) applications. The County received a total of nine (9) applications for the sale of easements for the next fiscal year.

As you can see from Katherine's memorandum, the required reviews from the County's Agricultural Land Preservation Advisory Board and the Planning Commission have been completed. Both Ms. Munson and I were present at their September 5th meeting when the Agricultural Land Preservation Advisory Board made their recommendations for submission. At their meeting on September 6th, the Planning Commission found that all applications are consistent with the 2006 Comprehensive Plan and that a MALPF easement, if approved, would be appropriate.

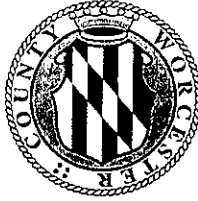
A draft notice for the mandated public hearing is attached to Ms. Munson's memo for use by Mr. Shannahan for submission for publication.

If you have any questions or need additional information please let me know.

Enclosures

cc: Katherine Munson

APPROVED
Worcester County Commissioners
Date HH 9/18/18



DEPARTMENT OF
ENVIRONMENTAL PROGRAMS

Worcester County

GOVERNMENT CENTER
ONE WEST MARKET STREET, ROOM 1306
SNOW HILL, MARYLAND 21863
TEL: 410.632.1220 / FAX: 410.632.2012

AGRICULTURAL PRESERVATION
CONSERVATION PROGRAM
WATER & SEWER PLANNING
SHORELINE CONSTRUCTION

WELL & SEPTIC
NATURAL RESOURCES
PLUMBING & GAS
COMMUNITY HYGIENE

Memorandum

To: Robert Mitchell, Director

From: Katherine Munson, Planner V *KM*

Subject: Public Hearing for Proposed FY 19 MALPF Easement Applications

Date: September 10, 2018

Please schedule a public hearing for the review of the following nine (9) applications to sell an agricultural preservation easement to the Maryland Agricultural Land Preservation Foundation (MALPF) in FY19.

1. 23-19-01; GMR LLC; TM 91, Grid 14, P 14; 934 Bishop Road, Pocomoke City; 224.1 acres
2. 23-19-02; Shockley, Stephen; TM 38, Grid 5, P 26; Davis Branch Road; 91.3 acres
3. 23-19-03; Shockley, Patrick and Ralph Shockley; David and Sandra Shockley as Trustees; TM 38, Grid 5, P 15; Shockley Road, Snow Hill; 390.2 acres
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In order for the applicants to be eligible to sell an easement to MALPF, their applications must be recommended for approval by the Worcester County Planning Commission and the Worcester County Agricultural Land Preservation Advisory Board, and approved by the Worcester County

Commissioners after a public hearing. The Public Hearing requirement is mandated by Maryland Annotated Code Title 2, Subtitle 5, Section 2-509(b)(3).

All applications meet the minimum requirements of the MALPF program and all have been recommended for approval by the Worcester County Planning Commission (September 6, 2018) and the Worcester County Agricultural Land Preservation Advisory Board (September 5, 2018).

Attached is a draft notice of public hearing. There is no State-mandated minimum time period between the date of advertisement and the date of public hearing. We will distribute the public hearing notice to the applicants and adjacent landowners.

Maps and detailed information about each application will be provided prior to the public hearing. Please do not hesitate to contact me with any questions you may have.

Attachment

having established an effective county agricultural land preservation program, and if there is money remaining in the Maryland Agricultural Land Preservation Fund at the end of the fiscal year, the county may apply to the Foundation for an amount equal to the difference between:

(1) The aggregate amount allotted on behalf of the county under general allotted purchases of easements as provided in § 2-508(b) of this subtitle for the fiscal year in which easement purchases are made; and

(2) The amount committed by the Foundation on behalf of the county under general allotted purchases of easements as provided in § 2-508(b) of this subtitle for the fiscal year in which easement purchases are made.

(b) *Time.* — The distribution under this section shall be made within 60 days of the end of each fiscal year.

(c) *Insufficient funds.* — If the money remaining in the Maryland Agricultural Land Preservation Fund at the end of the fiscal year are insufficient to distribute the total amount applied for under subsection (a) of this section, the maximum amount that may be distributed to any certified county is:

(1) The total sum available divided by the number of counties applying for additional funds under this section; less

(2) The amount committed by the Foundation on behalf of the county under general allotted purchases of easements as provided in § 2-508(b) of this subtitle for the fiscal year in which easement purchases are made.

(d) *Uses.* — A county may use the additional funds distributed under this section only for an approved agricultural land preservation program for the purposes stated under § 2-512 of this subtitle, including use for bond annuity funds, collateralizing loans, or matching funds. (1990, ch. 65; 1995, ch. 568; 2000, ch. 209, § 2; 2016, ch. 8, § 5.)

Editor's note. — Section 3, ch. 65, Acts 1990, provides that "in its annual report under § 2-506 of the Agriculture Article, the Maryland Agricultural Land Preservation Foundation shall include an evaluation of easement sales under both Round 1 and Round 2, by county, so as to permit an evaluation of which counties receive the most benefit under the

Foundation and the certification program under § 2-508.1 of the Agriculture Article."

Pursuant to § 5, ch. 8, Acts 2016, "is money" was substituted for "are moneys" in the introductory language of (a); and "money" was substituted for "moneys" in the introductory language of (c).

§ 2-509. Foundation regulations and procedures for sale of easements.

(a) *Compliance with section; adoption of regulations and procedures.* — (1) The Foundation shall follow the provisions under this section for the easement application process.

(2) The Foundation shall adopt regulations and procedures for:

(i) Evaluation of land for which application is made to sell an easement; and

(ii) Purchase of easements, including the purchase of easements under an installment purchase agreement.

(b) *Regulations and procedures for purchase of easements.* — Regulations and procedures adopted by the Foundation for the purchase of easements shall provide that:

(1) One or more owners of land actively devoted to agricultural use may file an application with the county governing body requesting the purchase of an easement by the Foundation on the land owned by the applicants. The application shall include maps and descriptions of the current use of land for the proposed easement, and any other information required by the Foundation to evaluate the land for purchase of an easement.

(2) Upon receipt of an application to purchase an easement the local governing body shall refer the application and accompanying materials both to the agricultural preservation advisory board and to the county planning and zoning body.

(i) After the referral of an application, the agricultural preservation advisory board shall advise the county governing body as to whether or not the land for the proposed easement meets the qualifications established by the Foundation under subsection (d) of this section, and whether or not the advisory board recommends the purchase of the easement.

(ii) In making its recommendation, the county agricultural preservation advisory board shall:

1. Take into consideration criteria and standards established by the Foundation under this subtitle, current local regulations, local patterns of land development, the kinds of development pressures currently existing on the land for the proposed easement, State smart growth goals, and any locally established priorities for the preservation of agricultural land; and

2. Recommend for ranking any application that qualifies and meets the priorities established by the county governing body for the preservation of agricultural land.

(iii) After the referral of an application, the county planning and zoning body shall advise the local governing body as to whether or not the purchase of the easement is compatible with existing and approved county plans and overall county policy, and whether or not the planning and zoning body recommends the purchase of the easement.

(3) If either the agricultural preservation advisory board or the planning and zoning body recommends approval, the county governing body shall hold a public hearing on the application for the proposed easement. Adequate notice of the hearing shall be given to all owners whose land would be encumbered by the proposed easement and all owners whose land is contiguous to the land for the proposed easement.

(4) In deciding whether to approve the application, the county governing body shall receive the recommendation of the county agricultural preservation advisory board established under § 2-504.1 of this subtitle.

(5) (i) After the receipt of the application and the recommendations of the agricultural preservation advisory board and the county planning and zoning body, the county governing body shall render a decision as to whether or not the application shall be recommended to the Foundation for approval.

(ii) If the county governing body decides to recommend approval of the application, it shall notify the Foundation and forward to the Foundation:

1. The application and all accompanying materials, including the recommendations of the advisory board and county planning and zoning body;

* Agricultural Easements

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2. A ranking of all applications based on:

A. The county governing body's locally established priorities as approved by the Foundation, which for purposes of enhancing competitive bidding may include a system that ranks properties in ascending order with respect to the proportion obtained by dividing the asking price by the value of the easement; and

B. Guidelines adopted by the Foundation under subsection (d) of this section; and

3. A statement of the total current development rights on the land for the proposed easement, which shall include the total number of development rights that have been subdivided or transferred.

(iii) If the county governing body recommends denial of the application, it shall inform the Foundation and the applicants.

(c) *Regulations and procedures for purchase of easements — Special requirements for certain counties.* — Regulations and procedures adopted by the Foundation for the purchase and monitoring of easements may not require, in Garrett County or Allegany County, a natural gas rights owner or lessee to subordinate its interest to the Foundation's interest if the Foundation determines that exercise of the natural gas rights will not interfere with an agricultural operation conducted on land subject to an easement.

(d) *Land which may be considered for purchase of easement.* — Regulations and criteria developed by the Foundation relating to land which may be considered for purchase of an easement shall provide that:

(1) Subject to item (2) of this subsection, land shall meet productivity, acreage, and locational criteria determined by the Foundation to be necessary for the continuation of farming;

(2) As long as all other criteria are met, land that is at least 50 acres in size or is contiguous to other permanently preserved land shall qualify for purchase of an easement;

(3) The Foundation shall attempt to preserve the minimum number of acres which may reasonably be expected to promote the continued availability of agricultural suppliers and markets for agricultural goods;

(4) Land within the boundaries of a 10-year water and sewer service district may be considered for purchase of an easement only if that land is outstanding in productivity and is of significant size;

(5) Land may be considered for purchase of an easement only if the county regulations governing the land permit the activities listed under § 2-513(a) of this subtitle; and

(6) Land be evaluated for:

(i) Location in a priority preservation area of the county;

(ii) Soil and other land characteristics associated with agricultural and silvicultural productivity;

(iii) Agricultural and silvicultural production and contribution to the agricultural and silvicultural economy; and

(iv) Any other unique county considerations that support the goals of the program. (1977, ch. 883; 1986, ch. 5, § 1; 1994, ch. 3, § 1; 1995, ch. 3, § 1; 1996, ch. 10, § 1; 1999, ch. 653; 2000, ch. 61, § 1; 2001, ch. 642; 2003, ch. 117;

2004, ch. 189; 2005, ch. 25, § 1; 2006, ch. 192; 2007, ch. 650; 2012, ch. 180; 2014, ch. 287.)

Effect of amendments. — Section 1, ch. 25, Acts 2005, approved April 12, 2005, and effective from date of enactment, redesignated the subdivisions in (b)(4) and (b)(7).

Chapter 192, Acts 2006, effective July 1, 2006, redesignated former (b)(7)(i) as present (b)(7)(i) and (ii); added present (b)(7)(iii) and made related changes; in (b)(7)(ii), deleted "however" from the beginning, inserted "of the district," and substituted "specified period of time" for "period of five years"; and in (b)(8), substituted "the period of time stipulated in the district agreement" for "five years from the establishment of a district".

Chapter 650, Acts 2007, effective July 1, 2007, added (a)(1) and made related changes; throughout (b) added language concerning applications for purchasing easements, proposed easements, and variants; in (b)(1) substituted "or an application . . . Foundation on" for "composed of"; in (b)(5)(i) and (b)(5)(10) substituted "(d)" for "(c)"; in (b)(7)(ii) made minor grammatical changes; throughout (c) and (d) added language concerning easements; and deleted (e).

Chapter 180, Acts 2012, effective June 30, 2012, rewrote the section.

Chapter 287, Acts 2014, effective July 1, 2014, reenacted the section without change.

Editor's note. — Section 2, ch. 117, Acts 2003, effective Oct. 1, 2003, provides that "on or before October 1, 2006, the Agricultural Land Preservation Foundation shall submit a report to the Governor and, subject to § 2-1246 of the State Government Article, to the General Assembly that evaluates the impact of this Act on the quality of agricultural land preserved and the Foundation's agricultural land preservation activities under the Maryland Agricultural Land Preservation Foundation."

Section 2, ch. 192, Acts 2006, provides that "counties are encouraged to provide property tax credits for agricultural districts."

Section 3, ch. 192, Acts 2006, provides that "on or before January 31, 2007, the Maryland Agricultural Land Preservation Foundation shall submit to the General Assembly, in accordance with § 2-1246 of the State Government Article, a report outlining procedures, laws, and regulations that the Foundation determines to be necessary in order to implement the elimination of agricultural districts from the Maryland Agricultural Land Preservation Program. The report shall include:

"(a) an implementation timeline;

"(b) statutory language for the repeal and reenactment of §§ 2-509 and 2-510 of the Agriculture Article, including the removal of the requirement for districts from the easement application process to become effective July 1,

2007, and the elimination of districts from the program to become effective June 30, 2008;

"(c) a process for county and State approval of easement applications;

"(d) a provision for optional county districts;

"(e) a provision that requires participating counties to establish a right-to-farm ordinance;

"(f) a provision that prohibits properties from being developed or subdivided during the easement application process; and

"(g) a provision that allows for the continuation of tax credits for existing districts."

Section 2 of ch. 650, Acts 2007, provides that "(a) Effective July 1, 2007, districts may not be a requirement for the easement application process to the Maryland Agricultural Land Preservation Foundation; and

"(b) Except as provided in Section 3 of this Act, as of June 30, 2012, all districts in the Maryland Agricultural Land Preservation Foundation shall be terminated and a landowner may not be bound to the terms of any Foundation district agreement."

Section 3 ch. 650, Acts 2007, provides that "the following agricultural land preservation districts established under § 2-509 of the Agriculture Article or by a county shall remain in force and may not be terminated:

"(a) Any district in which an easement has been transferred to the Foundation; and

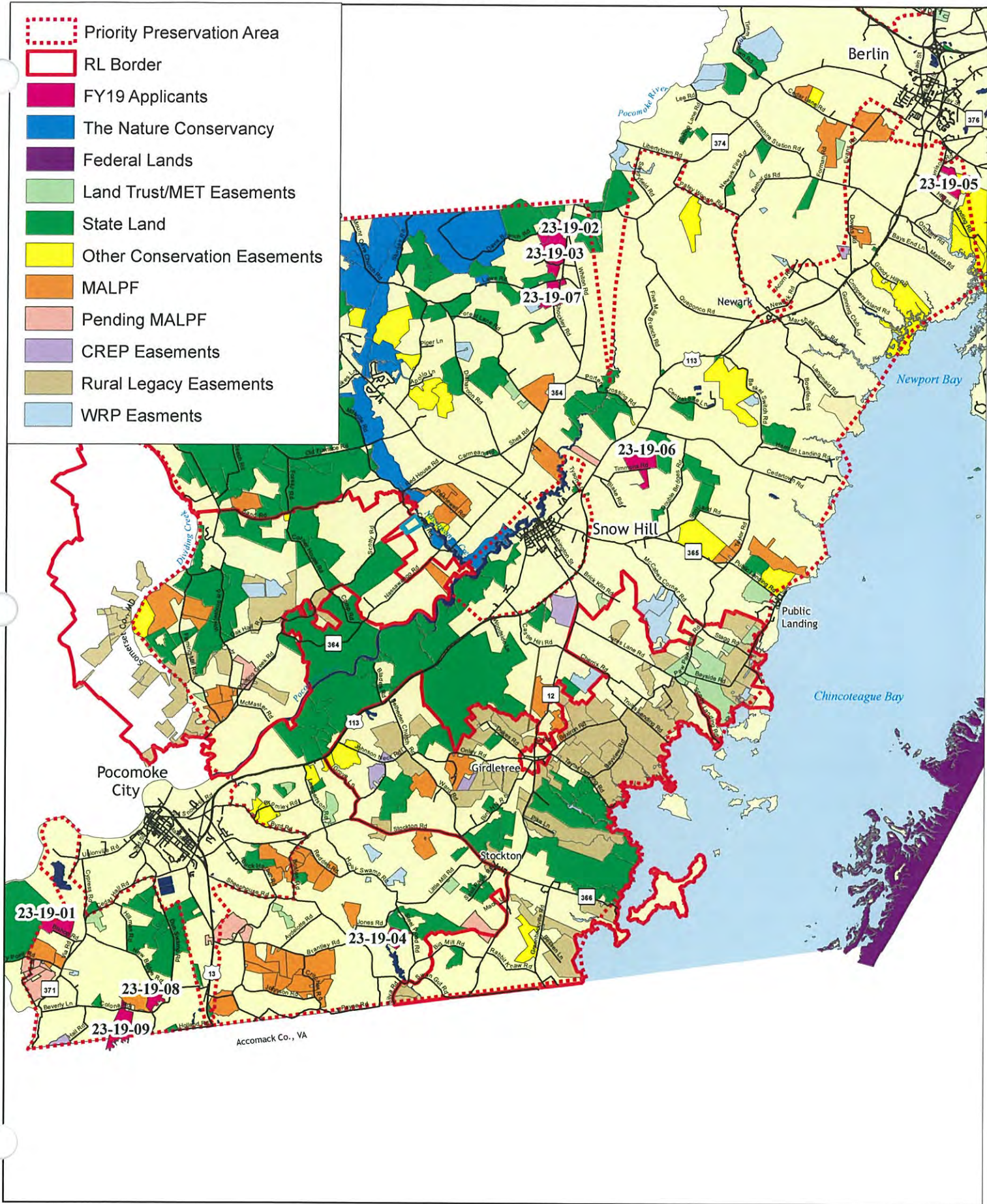
"(b) Any district established by a county and a landowner for the purpose of providing a property tax credit to the landowner."

Section 4 ch. 650, Acts 2007, provides that "the Foundation may not accept a district petition for any purpose after June 30, 2008."

University of Baltimore Law Review. — For article, "Preservation of Maryland Farmland: A Current Assessment," see 8 U. Balt. L. Rev. 429 (1979).

Conveyance of a lot subject to preservation easement was void. — Conveyance of a lot in a parcel subject to a preservation easement was void as: (1) although (b)(7)(v) of this section provided that an owner was not precluded from selling the property, § 2-504(4) [now § 2-504(5)] of this subtitle delegated to the Maryland Agricultural Land Preservation Foundation the power to adopt necessary regulations; (2) Md. Code Regs. 15.15.01.17H(1) prohibited the owners from subdividing land subject to an agricultural land preservation district or easement without written approval from the Foundation; (3) "subdivision" was defined as the division of land into two or more parts or parcels; and (4) the statutory definition in art. 66B, § 1.00(1) of the Code [now § 1-101 of the Land Use Article] did not prevail over the definition contained in Reg. 15.15.01.01-2B(7).

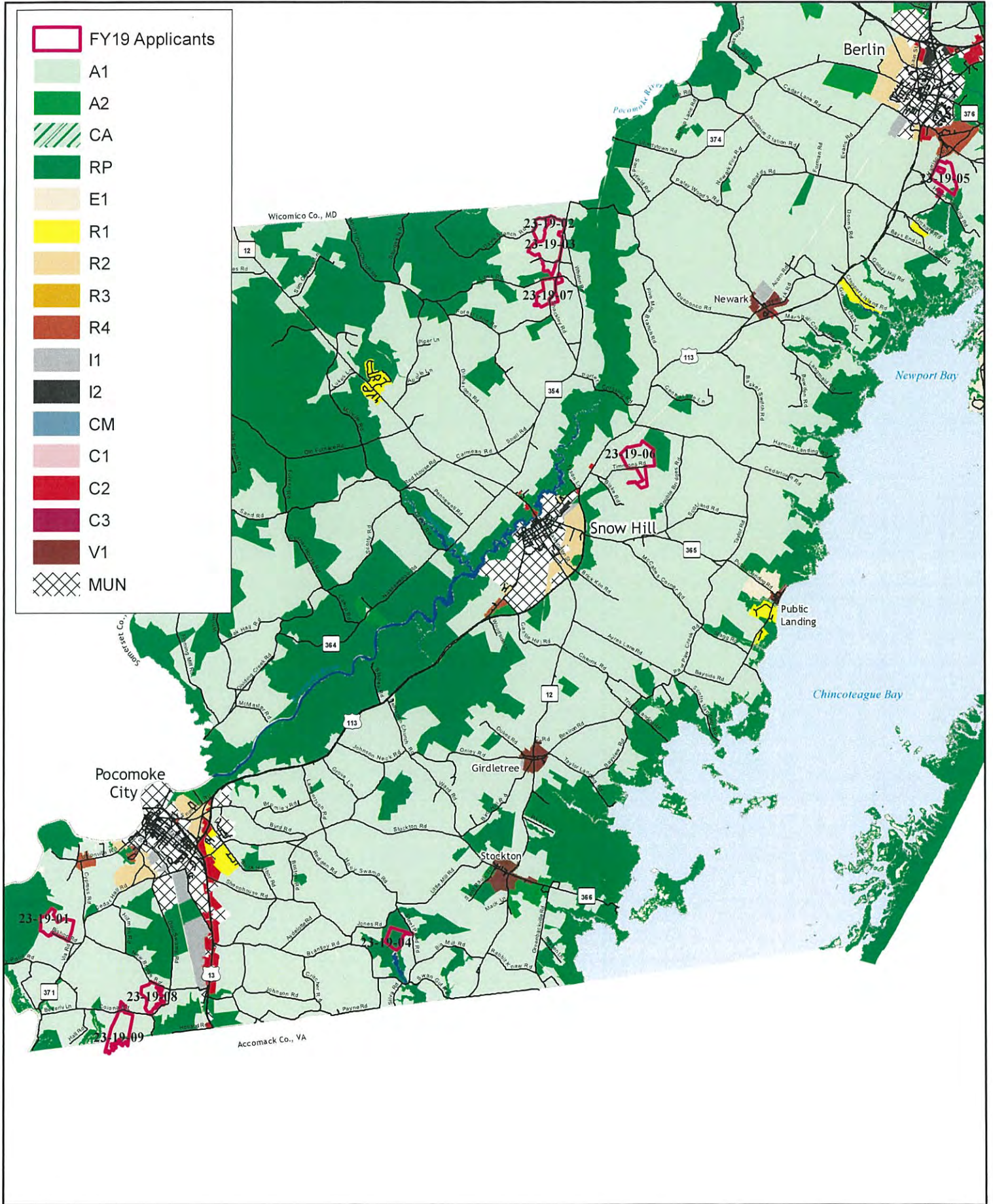
FY19 MALPF Applications and Protected Land, Worcester County



2.5 1.25 0 2.5 Miles



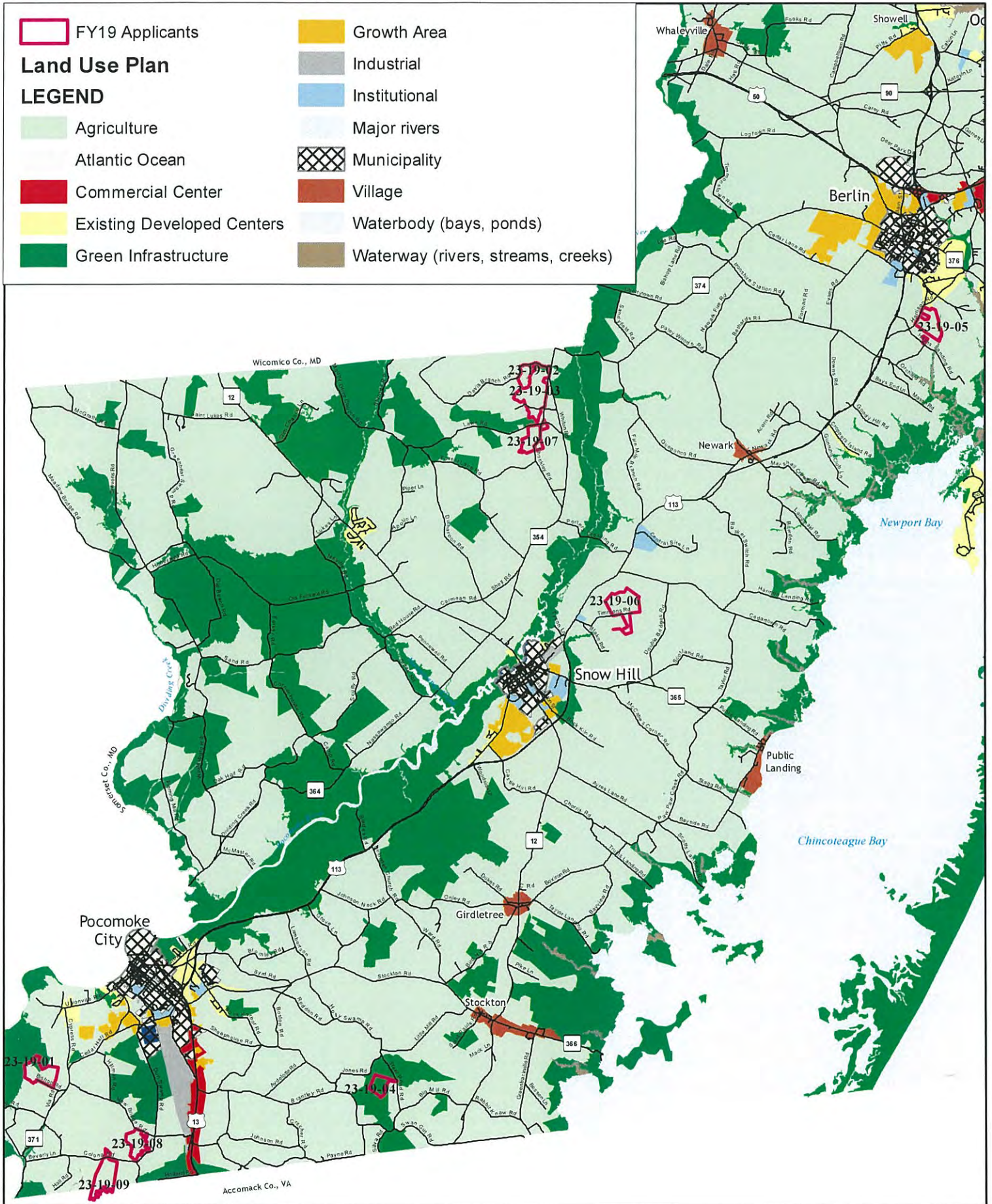
FY19 MALPF Applications and Protected Land, Worcester County



2.5 1.25 0 2.5 Miles



FY19 MALPF Applications and Land Use Plan, Worcester County



FY19 Applicants

Land Use Plan

LEGEND

- | | |
|---|---|
| Agriculture | Growth Area |
| Atlantic Ocean | Industrial |
| Commercial Center | Institutional |
| Existing Developed Centers | Major rivers |
| Green Infrastructure | Municipality |
| | Village |
| | Waterbody (bays, ponds) |
| | Waterway (rivers, streams, creeks) |

2.5 1.25 0 2.5 Miles

