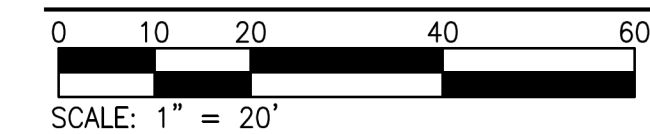




EXISTING SITE PLAN



NOTE:
1. SURVEY ELEVATIONS BASED ON MD STATE PLANE AND NAVD88 DATUM.

DEMOLITION NOTES

- ① AFTER COFFERDAM INSTALLATION REMOVE EXISTING AREA OF BULKHEAD & TIMBER PILES FOR NEW BOAT RAMP WINGWALLS AS SHOWN IN THEIR ENTIRETY AND DISPOSE OF OFF-SITE.
- ② CONTRACTOR SHALL PROVIDE TEMPORARY SUPPORT AS NEEDED FOR BULKHEAD, UTILITY POLES, ELECTRICAL CONDUIT, AND POWER EQUIPMENT TO ALLOW FOR OPERATION DURING DEMOLITION AND CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL REMOVE SUCH TEMPORARY SUPPORT AND REINSTALL ANY REMOVED COMPONENTS UPON COMPLETION OF CONSTRUCTION.
- ③ STAGING AREAS TO BE COORDINATED WITH OWNER

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**REPLACEMENT OF BOAT RAMP, BULKHEAD,
AND REVETMENT IMPROVEMENT**

GEORGE ISLAND LANDING
 WORCESTER COUNTY, MARYLAND

**EXISTING
SITE PLAN**

SCALE	AS NOTED	SHEET NO.
DESIGN BY	ARM	S1.1
DRAWN BY	TFVT	
CHECKED BY	ARM	
GMB FILE	240262	
DATE	MARCH 2026	

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PROPOSED SITE PLAN



- LEGEND:**
- EXTENTS OF PAVEMENT PLACEMENT; SUPERPAVE 9.5MM HOT MIX ASPHALT PAVING 1 1/2" COMPACTED THICKNESS, INSTALL PER COUNTY STANDARDS; ANY LOW SPOTS SHALL BE REPAIRED TO ALLOW FOR POSITIVE SHEET FLOW OFF OF PARKING LOT
 - EXTENTS OF REVETMENT IMPROVEMENT (SEE DETAIL 4/S2.3)

NOTE:
1. SURVEY ELEVATIONS BASED ON MD STATE PLAN AND NAVD88 DATUM

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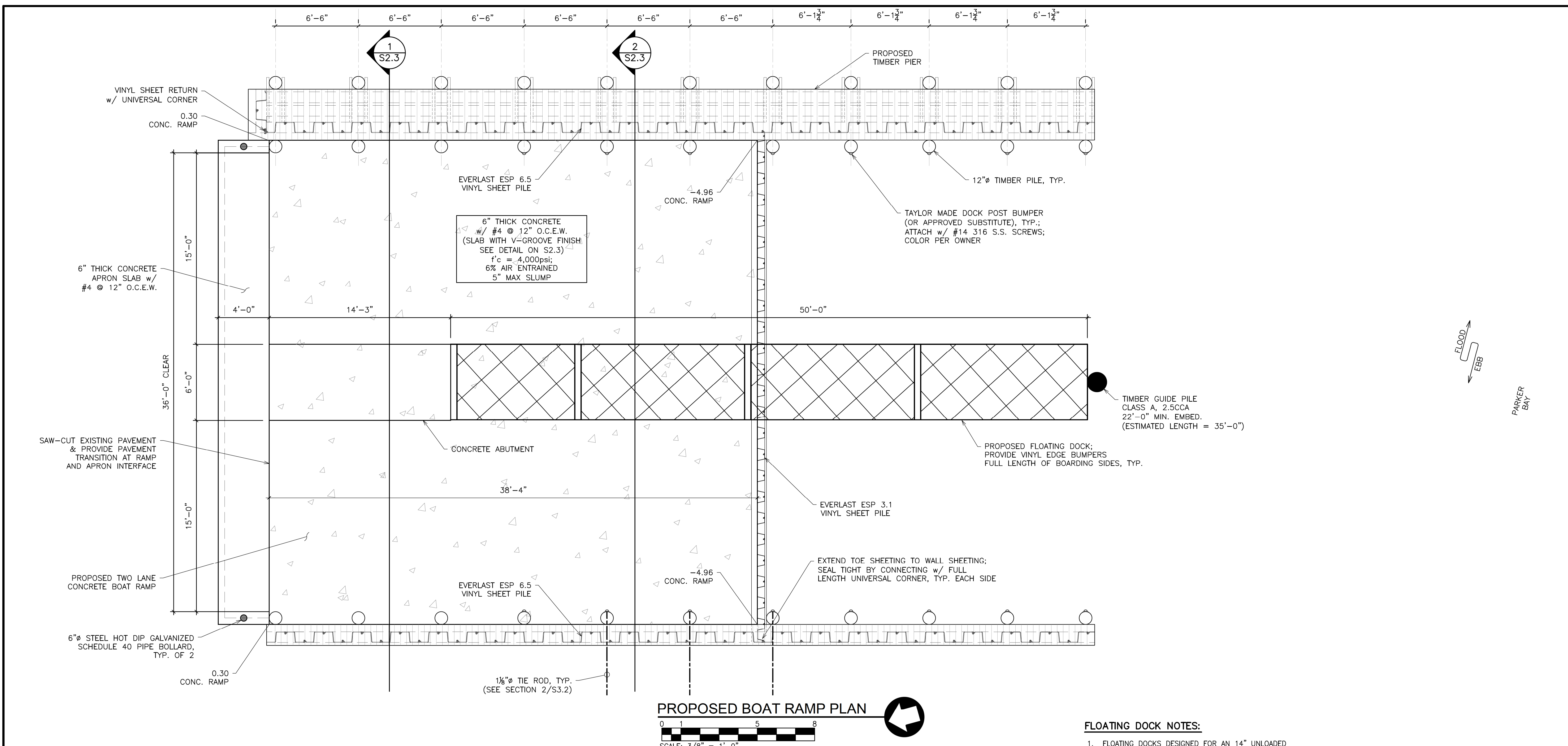
**REPLACEMENT OF BOAT RAMP, BULKHEAD,
AND REVETMENT IMPROVEMENT**

GEORGE ISLAND LANDING
 WORCESTER COUNTY, MARYLAND

**PROPOSED
SITE PLAN**

SCALE: AS NOTED	SHEET NO.
DESIGN BY: ARM	S1.2
DRAWN BY: TFVT	
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STRUCTURAL NOTES

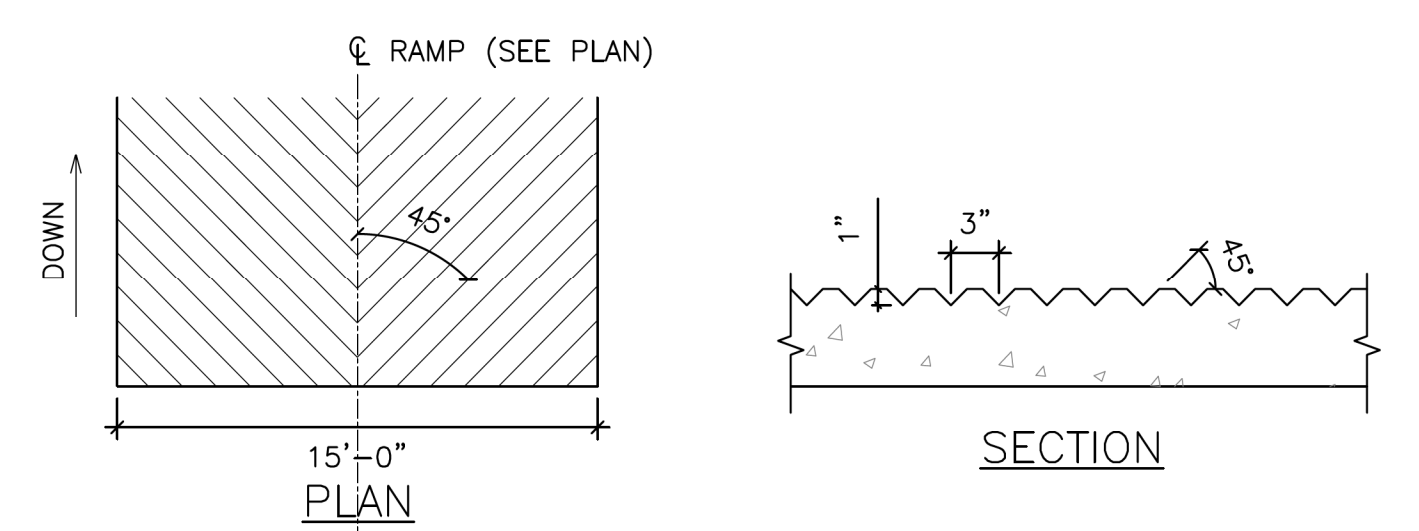
- TIMBER PILES SHALL BE SOUTHERN PINE CONFORMING TO ASTM D-25, CLASS B WITH A 2.5 CCA RATING. PILES SHALL BE DRIVEN TO AN ALLOWABLE BEARING CAPACITY OF 7 TONS WITH A MINIMUM EMBEDMENT LENGTH OF 15.00' UNLESS NOTED OTHERWISE. A TEST PILE SHALL BE DRIVEN PRIOR TO ORDERING THE PRODUCTION PILE LENGTHS AND TO VERIFY THAT THE MINIMUM TIP ELEVATIONS AND BEARING CAPACITY REQUIREMENTS CAN BE MET. DRIVING RECORDS FOR ALL PILES SHALL BE RECORDED AND SUBMITTED TO ENGINEER FOR RECORD. DAMAGED PILES SHALL BE REMOVED AND REPLACED.
- ALL BOLTS SHALL BE DOME HEAD WEATHER-TUFF TIMBER BOLTS CONFORMING TO ASTM A307, ALL WASHERS SHALL BE 1/4" THICK N.Y.D.D. DOCK WASHERS AND NUTS SHALL BE HEAVY HEX TYPE UNLESS NOTED OTHERWISE. ALL NAILS SHALL BE STAINLESS STEEL RING SHANK OR ANNULAR NAILS AND IN ACCORDANCE WITH THE SIZES GIVEN IN THE CONTRACT DOCUMENTS. ALL HARDWARE SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM A153, EXCEPT NAILS AND SCREWS, WHICH SHALL BE STAINLESS STEEL. TIE RODS SHALL CONFORM TO ASTM 1554-55.
- ALL WALERS, STRINGERS, LINERS, AND CAPS TO BE SOUTHERN PINE, NO. 1 GRADE TREATED 0.15CA. ALL DECKING BOARDS SHALL BE SOUTHERN PINE, NO.1 GRADE TREATED 0.15CA.
- PRIOR TO ORDERING THE TIMBER PILES FOR THE PROJECT, THE CONTRACTOR SHALL SUPPLY AND DRIVE ONE (1) FORTY FIVE FOOT (45') LONG CLASS "B" TEST PILE. THE TEST PILE LOCATION WILL BE COORDINATED WITH THE ENGINEER WITH THE INTENT TO HAVE IT REMAIN IN PLACE FOR USE. THE TEST PILES SHALL BE CCA TREATED AND OTHERWISE CONFORM TO THE SPECIFICATIONS. THE LENGTHS OF THE PROJECT PILES MAY BE ADJUSTED BASED ON THE OUTCOME OF THE TEST PILES.
- CONCRETE (CAST-IN-PLACE) DESIGN AND DETAILING SHALL CONFIRM TO THE REQUIREMENTS OF ACI 318-19. CONTRACTOR SHALL SUBMIT MIX DESIGNS ACCOMPANIED BY APPROPRIATE GRAPHS AND BACKGROUND DATA FOR APPROVAL. MIX DESIGN SHALL INDICATE 7 AND 28 DAY STRENGTHS, CEMENT CONTENT, AIR CONTENT, WATER-CEMENT RATIO, AMOUNT OF FINE AND COARSE AGGREGATES, AND ADMIXTURES.
- MINIMUM COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS SHALL BE AS FOLLOWS, UNLESS NOTED OTHERWISE.
RAMP: 4000 PSI
- ALL EXTERIOR CONCRETE AND CONCRETE EXPOSED TO WEATHER SHALL BE AIR-ENTRAINED (6% +/- 1%). SLUMP SHALL BE 5" MAX. USE OF ADDITIVES SHALL NOT BE PERMITTED UNLESS SPECIFICALLY APPROVED BY THE STRUCTURAL ENGINEER. USE OF ADDITIVES CONTAINING CALCIUM CHLORIDE SHALL NOT BE PERMITTED.
- CONCRETE PROTECTION FOR REINFORCEMENT (CAST-IN-PLACE CONCRETE) PROVIDE THE FOLLOWING MINIMUM CONCRETE COVER FOR REINFORCEMENT: CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH - 3"
- CONTRACTOR SHALL PREPARE DETAILED CONSTRUCTION DRAWINGS AND SPECIFICATIONS OF COFFERDAM AND DEWATERING SYSTEM AND SUBMIT TO ENGINEER.
- COFFERDAM SHALL BE CONSTRUCTED WITH STEEL SHEET PILING OR OTHER MATERIAL SUBJECT TO APPROVAL OF THE ENGINEER.
- DEWATERING OPERATIONS MAY BE SUSPENDED 24-HOURS AFTER LAST CONCRETE POUR IF THE TEMPERATURE OF THE SURROUNDING WATER IS NO MORE THAN 20' F BELOW THE TEMPERATURE OF THE CONCRETE. IF THE WATER IS MORE THAN 20' F BELOW THE TEMPERATURE OF THE CONCRETE, THE CONCRETE SHALL BE CURED PER ACI301 SPECIFICATIONS FOR 72 HOURS, AFTER WHICH THE COFFERDAM MAY BE FLOODED. THE CONTRACTOR SHALL, AT HIS EXPENSE, REPLACE OR REPAIR ANY DAMAGE TO THE CONCRETE RESULTING FROM FLOODING THE COFFERDAM.
- SUBGRADE INSIDE COFFERDAM SHALL REMAIN FREE OF STANDING WATER DURING PLACEMENT OF STONE BASE THROUGH PLACEMENT OF RAMP SLAB.
- DEWATERING SYSTEM MUST INCLUDE CODE COMPLIANT WATER FILTRATION AT DISCHARGE.
- THE GENERAL CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL STRUCTURAL ELEMENTS SHOWN ON THE CONTRACT DOCUMENTS FOR APPROVAL. THE STRUCTURAL ENGINEER WILL NOT BE RESPONSIBLE FOR THE STRUCTURAL CERTIFICATION AND DESIGN OF THE PROJECT IF THE GENERAL CONTRACTOR FAILS TO OBTAIN APPROVAL OF THE SHOP DRAWINGS. THE GENERAL CONTRACTOR SHALL INFORM THE STRUCTURAL ENGINEER IN WRITING CONCERNING DEVIATIONS AND/OR OMISSIONS FROM THE CONTRACT DOCUMENTS AT THE TIME OF SHOP DRAWING SUBMISSION. THE GENERAL CONTRACTOR SHALL REVIEW ALL SHOP DRAWINGS AND SHALL MAKE ALL CORRECTIONS HE DEEMS NECESSARY BEFORE SUBMISSION. THE GENERAL CONTRACTOR SHALL STATE ON THE SHOP DRAWINGS THAT CONTRACT DOCUMENT REQUIREMENTS HAVE BEEN MET AND THAT ALL DIMENSIONS, CONDITIONS AND QUANTITIES HAVE BEEN REVIEWED AND VERIFIED AS SHOWN AND/OR CORRECTED ON THE SHOP DRAWINGS.
- THE CONTRACTOR SHALL ENGAGE A GEOTECHNICAL ENGINEERING CONSULTANT TO PROVIDE WRITTEN CONFIRMATION THAT:
 - THE PROPOSED PILE INSTALLATION MEETS THE CAPACITY REQUIREMENTS
 - THE SUBGRADE BELOW THE RAMP MEETS THE BEARING REQUIREMENTS
 - THE CONCRETE MIX MEETS THE NOTED REQUIREMENTS, MINIMUM ONE SET OF CYLINDERS PER POUR
- ALL IRON AND STEEL USED IN THE PROJECT ARE PRODUCED IN THE UNITED STATES. THIS MEANS ALL MANUFACTURING PROCESSES, FROM THE INITIAL MELTING STAGE THROUGH THE APPLICATION OF COATINGS, OCCURRED IN THE UNITED STATES.
- ALL MANUFACTURED PRODUCTS USED IN THE PROJECT ARE PRODUCED IN THE UNITED STATES. THIS MEANS THE MANUFACTURED PRODUCT WAS MANUFACTURED IN THE UNITED STATES, AND THE COST OF THE COMPONENTS OF THE MANUFACTURED PRODUCT THAT ARE MINED, PRODUCED, OR MANUFACTURED IN THE UNITED STATES IS GREATER THAN 55% OF THE TOTAL COST OF ALL COMPONENTS OF THE MANUFACTURED PRODUCT, UNLESS ANOTHER STANDARD FOR DETERMINING THE MINIMUM AMOUNT OF DOMESTIC CONTENT OF THE MANUFACTURED PRODUCT HAS BEEN ESTABLISHED UNDER APPLICABLE LAW OR REGULATION.
- ALL CONSTRUCTION MATERIALS ARE MANUFACTURED IN THE UNITED STATES. THIS MEANS THAT ALL MANUFACTURING PROCESSES OF THE CONSTRUCTION MATERIAL OCCURRED IN THE UNITED STATES. FOR CONSTRUCTION MATERIAL STANDARDS, SEE 2 CFS SECTION 184.6.
- THIS BUY AMERICA PREFERENCE ONLY APPLIES TO ARTICLES, MATERIALS, AND SUPPLIES CONSUMED IN, INCORPORATED INTO, OR AFFIXED TO AN INFRASTRUCTURE PROJECT. AS SUCH, IT DOES NOT APPLY TO TOOLS, EQUIPMENT, AND SUPPLIES, SUCH AS TEMPORARY SCAFFOLDING, BROUGHT TO THE CONSTRUCTION SITE AND REMOVED AT OR BEFORE THE COMPLETION OF THE INFRASTRUCTURE PROJECT. NOR DOES A BUY AMERICA PREFERENCE APPLY TO EQUIPMENT AND FURNISHING, SUCH AS MOVABLE CHAIRS, DESKS, AND PORTABLE COMPUTER EQUIPMENT, USED AT OR WITHIN THE FINISHED INFRASTRUCTURE PROJECT BUT ARE NOT AN INTEGRAL PART OF THE STRUCTURE OR PERMANENTLY AFFIXED TO THE INFRASTRUCTURE PROJECT.

FLOATING DOCK NOTES:

- FLOATING DOCKS DESIGNED FOR AN 14" UNLOADED FREEBOARD AND A 40 PSF LIVE LOAD.
- ANCHOR PILE LOCATION IS SUGGESTED. MANUFACTURER SHALL VERIFY REQUIRED ANCHOR LOCATIONS.
- DOCK DIMENSIONS SHOWN ARE NOMINAL FROM ALUMINUM TO ALUMINUM FRAME.
- SUBMIT SHOP DRAWINGS FOR EACH FLOATING DOCK SYSTEM.
- PROVIDE VINYL EDGE BUMPERS FULL LENGTH OF BOARDING SIDES.

LEGEND

- NEW FLOATING DOCK BY GATOR DOCK OR APPROVED ALTERNATE (ALUMINUM FRAME & DECKING WITH POLY FLOATS -14" FREEBOARD) - DESIGN LIVE LOAD OF 40 PSF INCLUDING CLEATS, HINGE CONNECTION TO ABUTMENT, AND STANDARD VINYL BUMPERS; 4 WHEEL ROLLERS PILE GUIDE; PROVIDE GROUND OUT FEET FOR SECTIONS THAT WILL BOTTOM OUT AT LOW WATER



TYPICAL RAMP SURFACING DETAIL
N.T.S.

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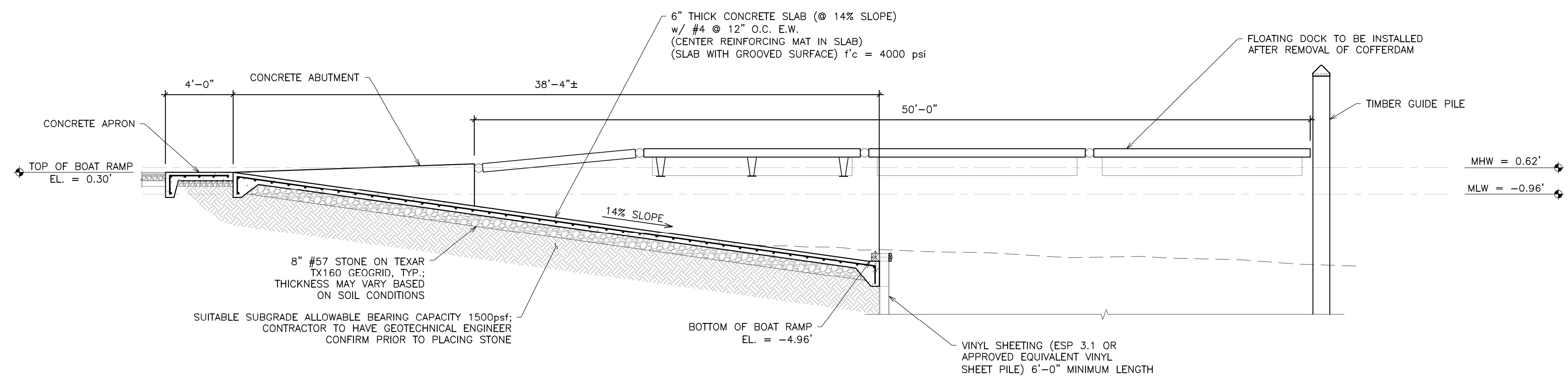
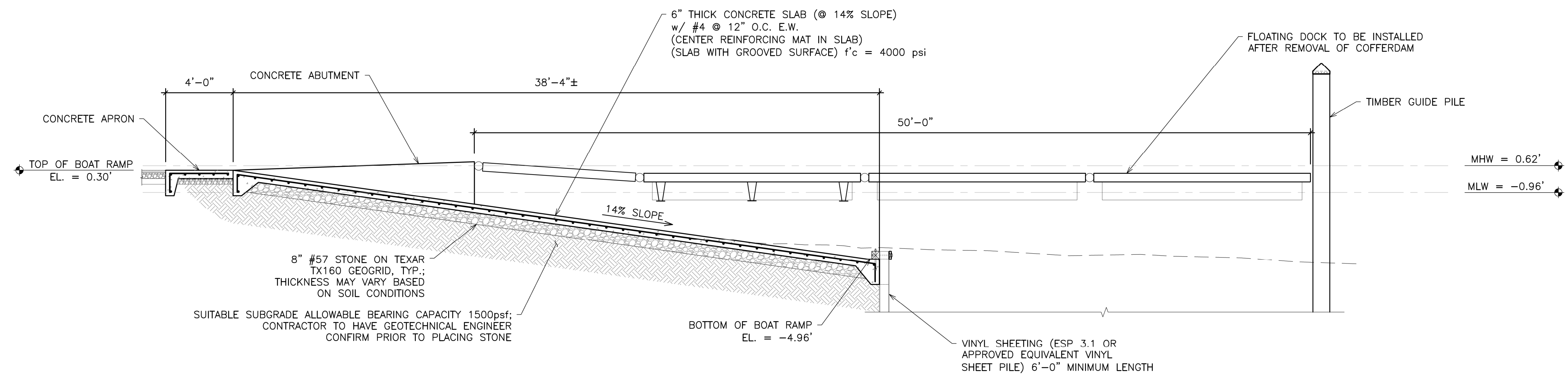
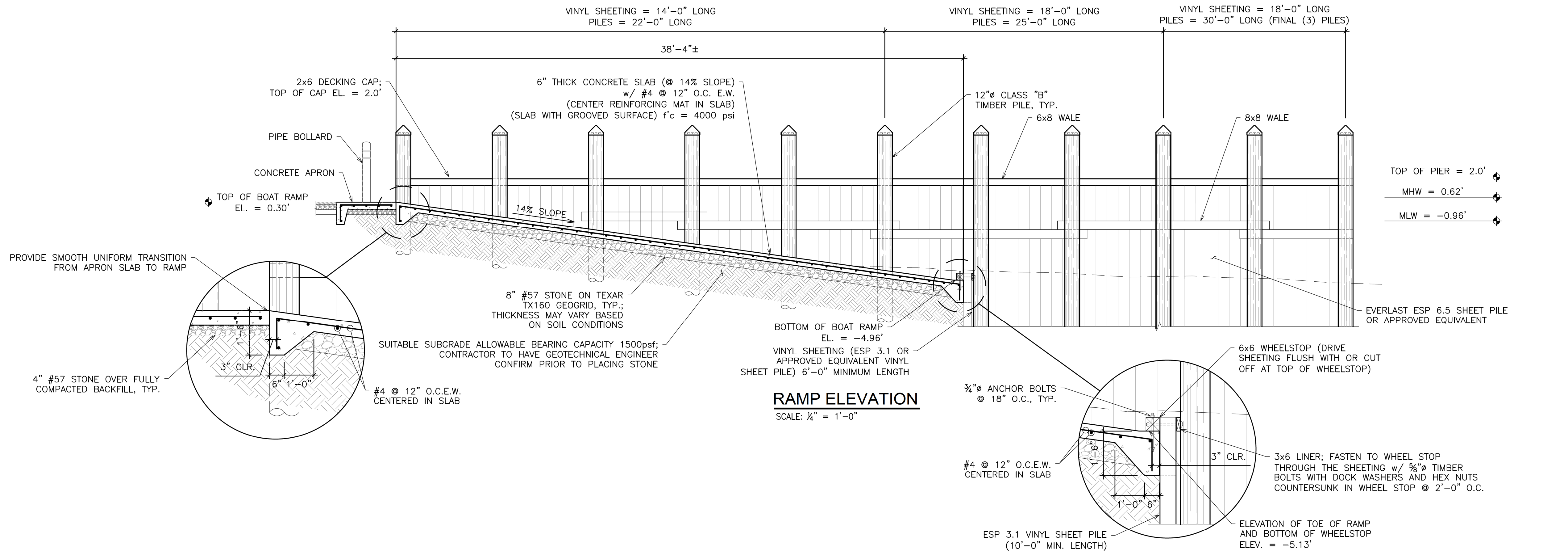
REPLACEMENT OF BOAT RAMP, BULKHEAD, AND REVETMENT IMPROVEMENT
 GEORGE ISLAND LANDING
 WORCESTER COUNTY, MARYLAND

PROPOSED BOAT RAMP PLAN AND DETAILS

SCALE: AS NOTED	SHEET NO.
DESIGN BY: ARM	S2.1
DRAWN BY: TPT	
CHECKED BY: ARM	
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DATE: MARCH 2026	

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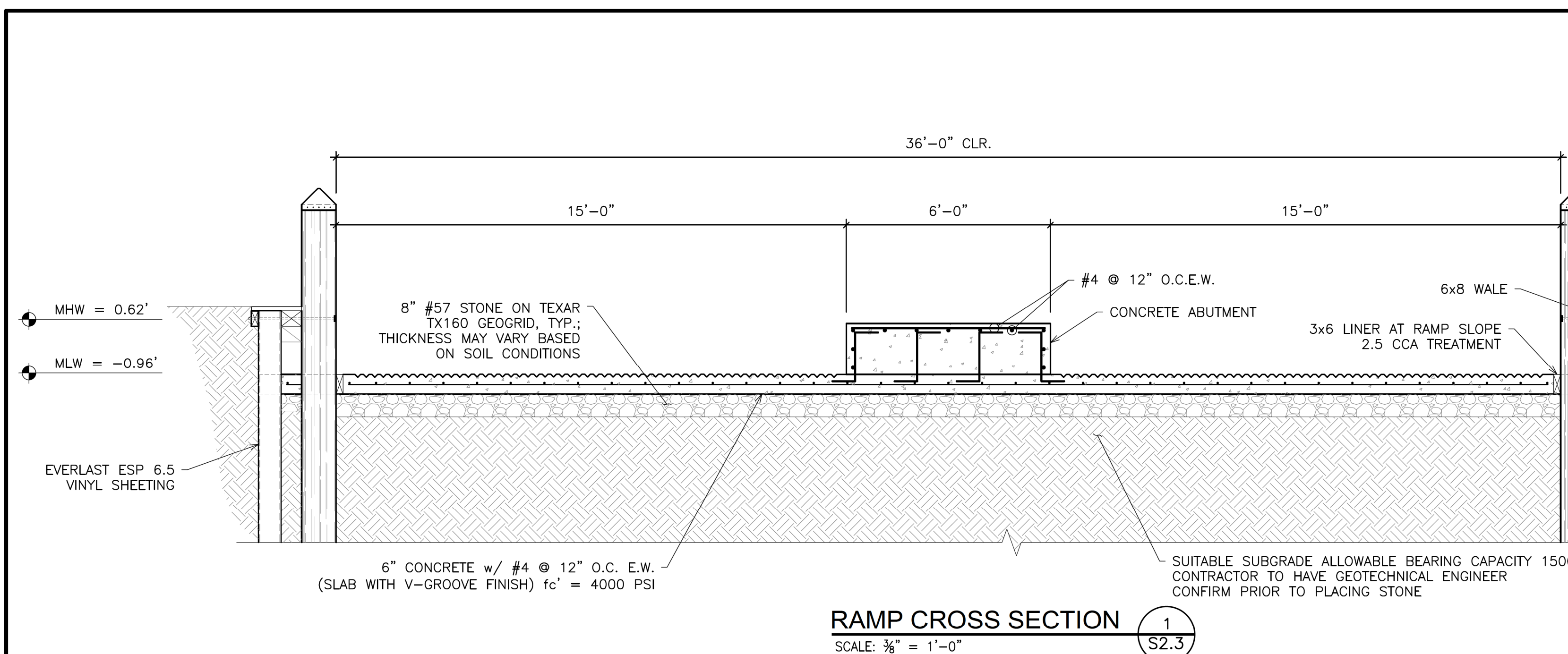
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WORCESTER COUNTY, MARYLAND

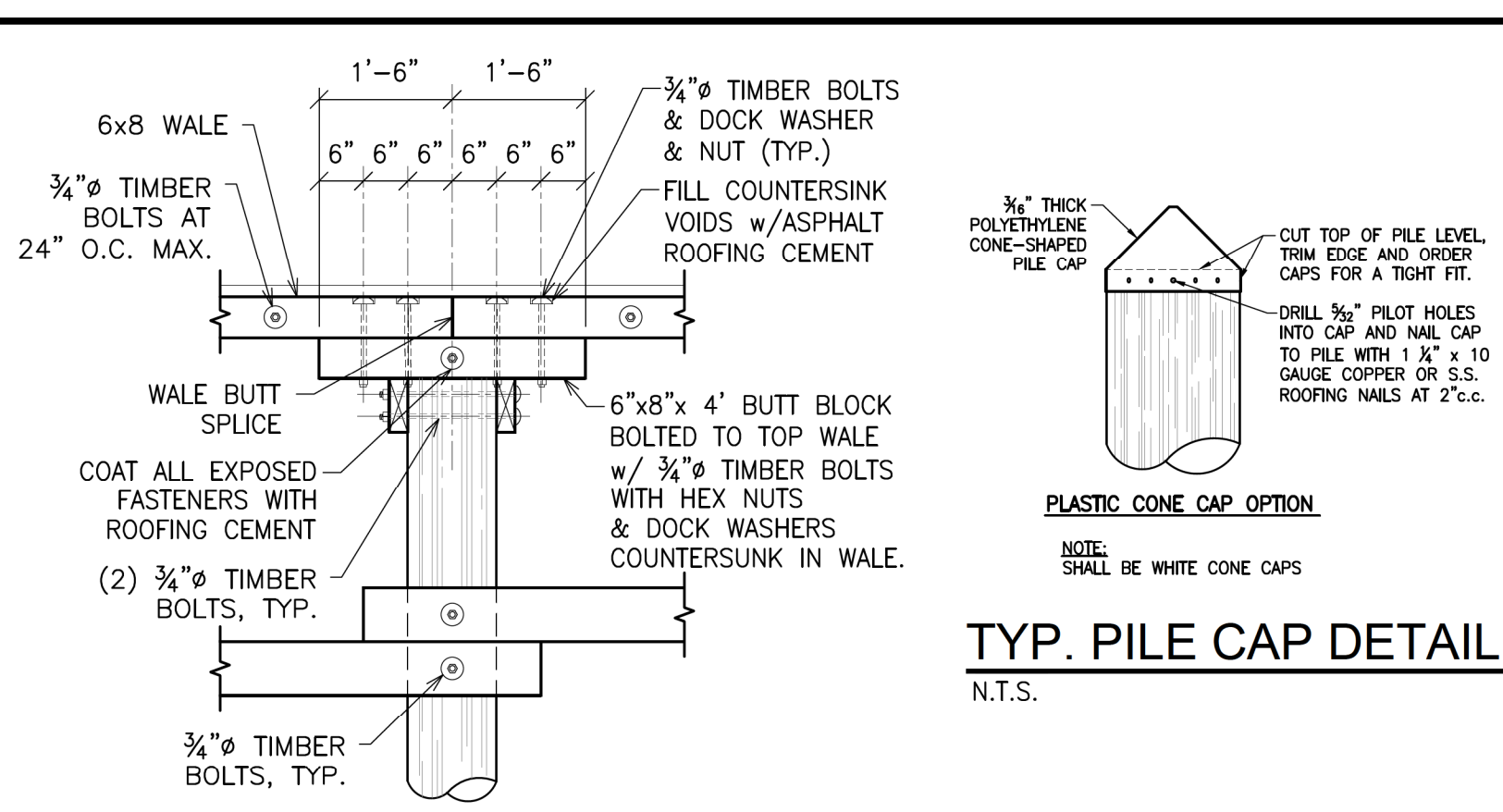
PROPOSED
BOAT RAMP
ELEVATIONS

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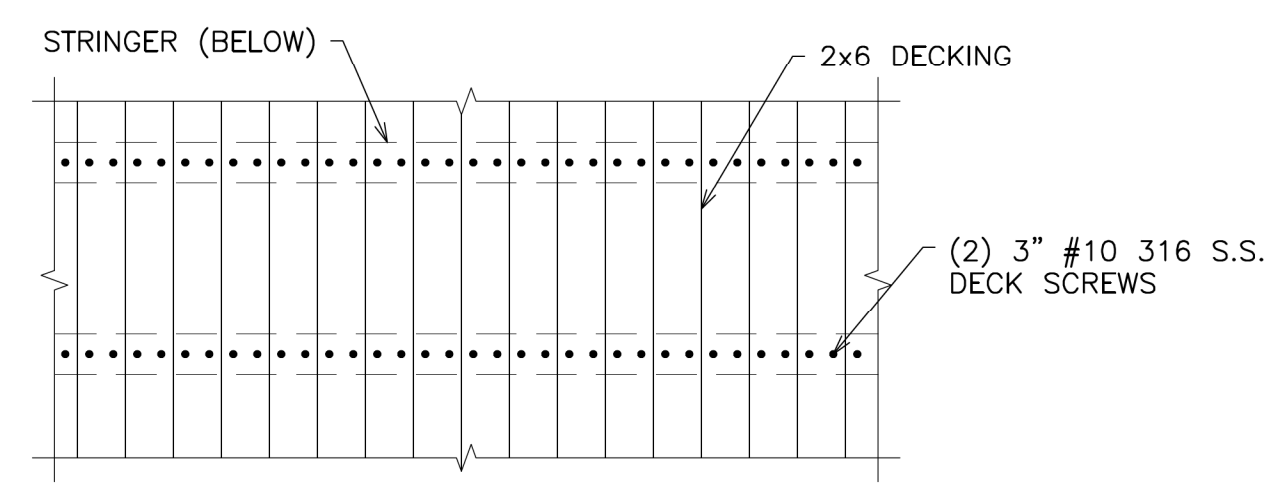
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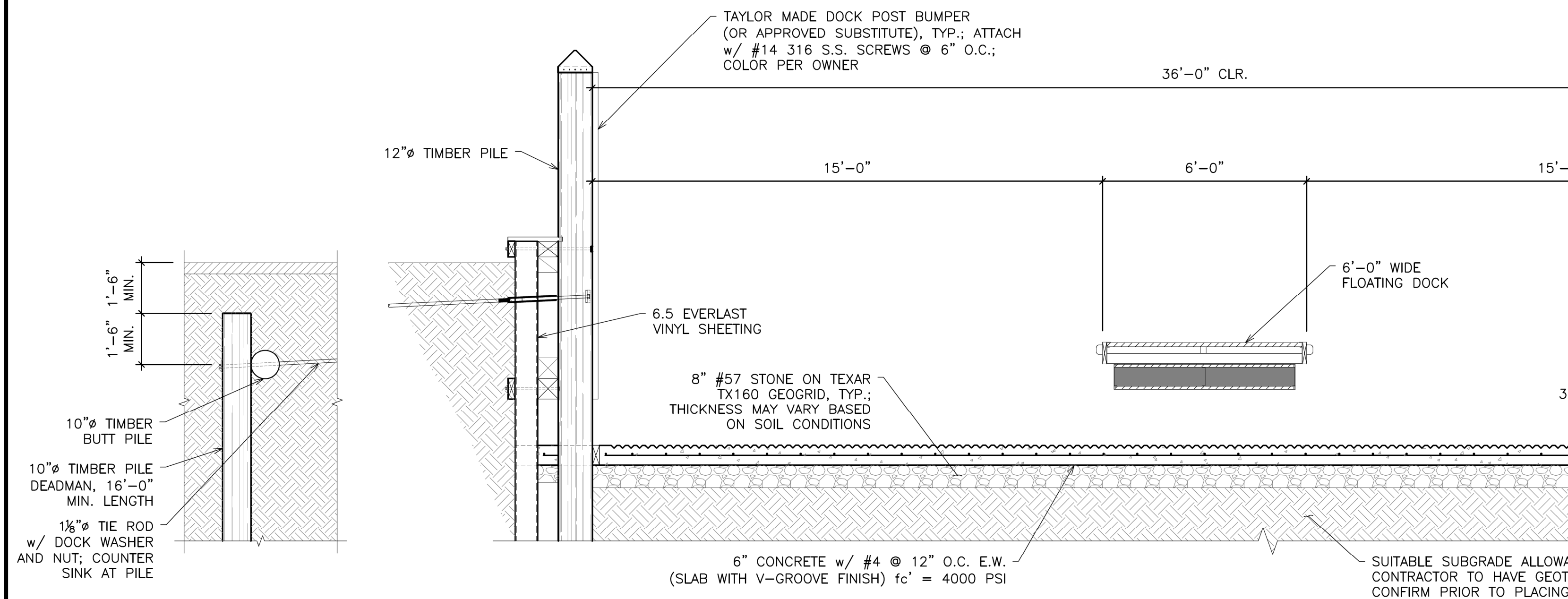
RAMP CROSS SECTION 1
SCALE: 3/8" = 1'-0"



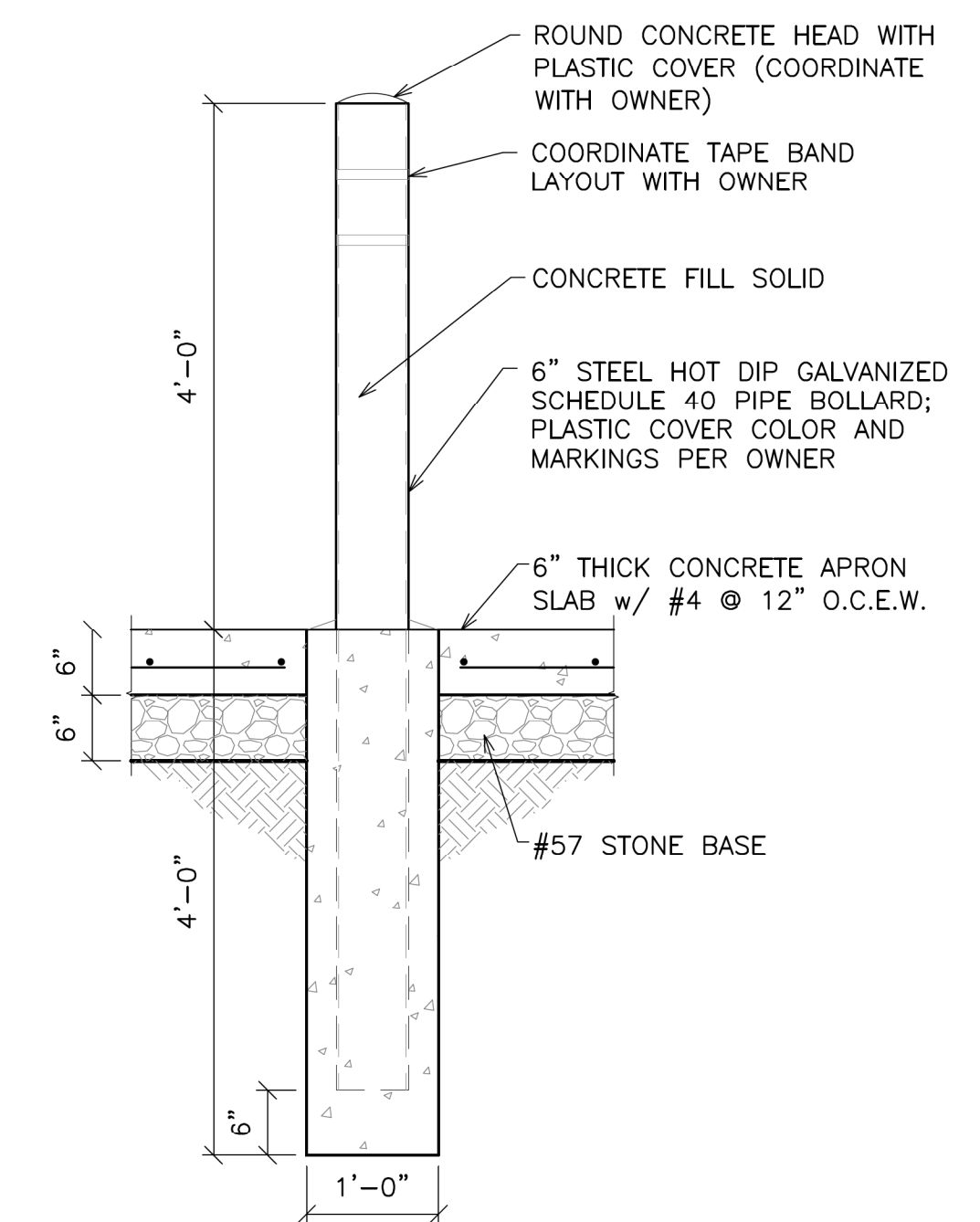
TYP. WALE SPLICE DETAIL
SCALE: 1/2" = 1'-0"



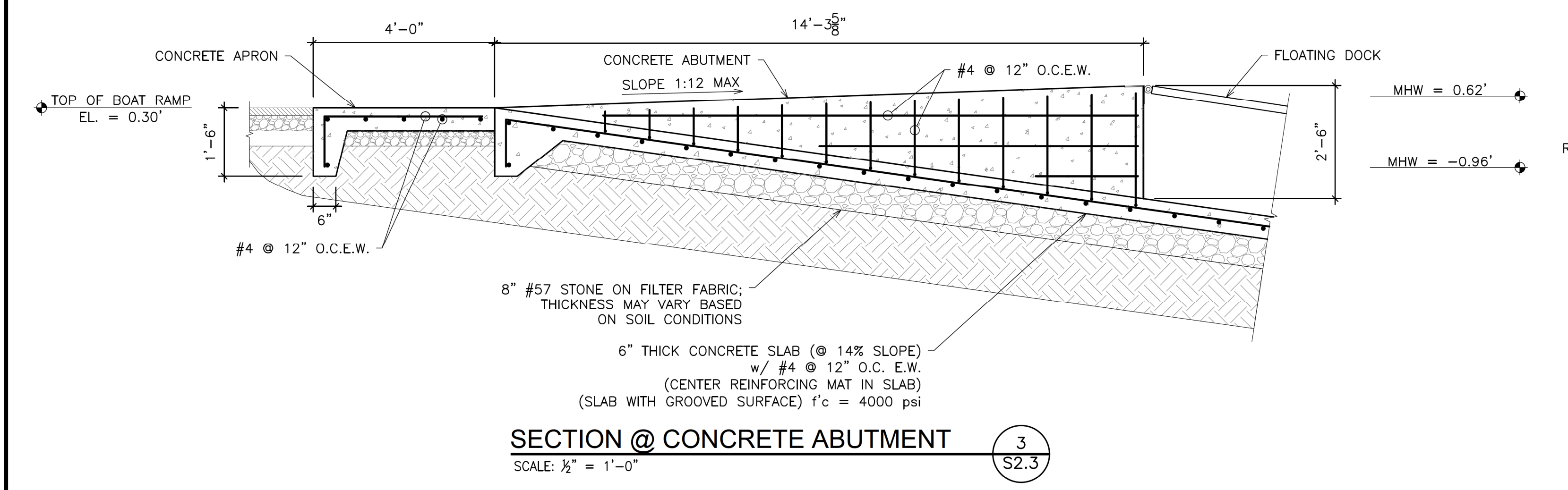
DECK BOARD ATTACHMENT DETAIL
SCALE: 1/2" = 1'-0"



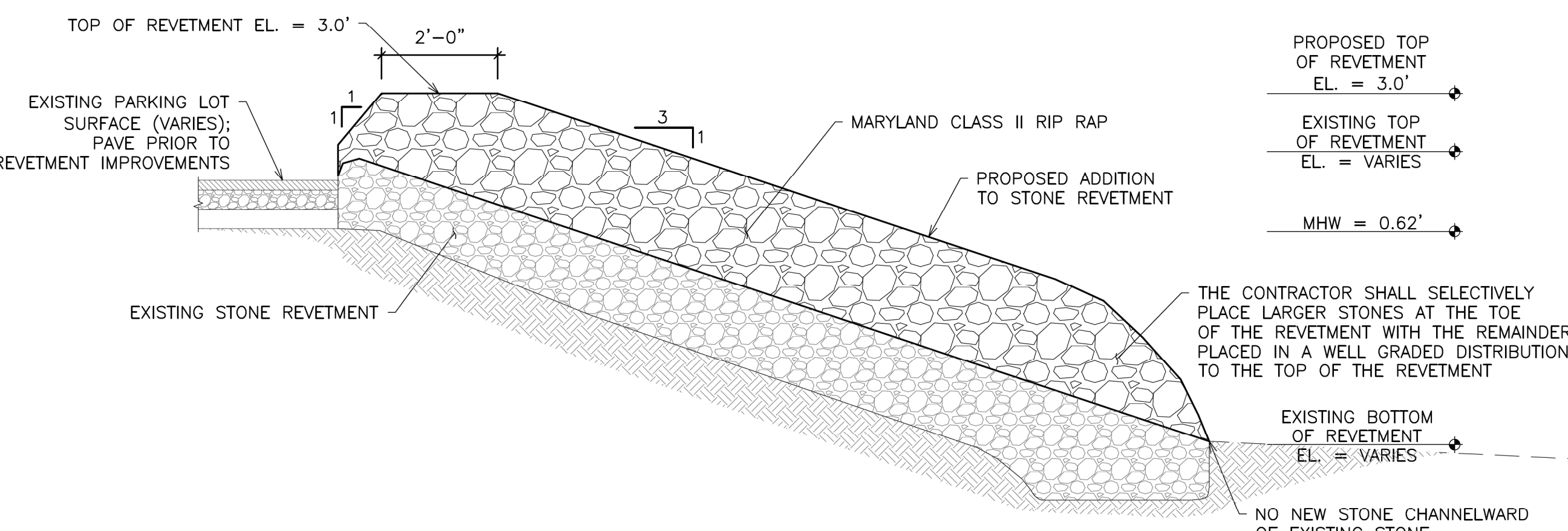
RAMP CROSS SECTION 2
SCALE: 3/8" = 1'-0"



PIPE BOLLARD DETAIL
SCALE: 3/4" = 1'-0"



SECTION @ CONCRETE ABUTMENT 3
SCALE: 1/2" = 1'-0"



TYPICAL REVETMENT SECTION 4
SCALE: 1/2" = 1'-0"

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PROPOSED BOAT RAMP SECTIONS AND DETAILS

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